GENOA CHARTER TOWNSHIP PLANNING COMMISSION PUBLIC HEARING MARCH 9, 2020 6:30 P.M. AGENDA

CALL TO ORDER:

PLEDGE OF ALLEGIANCE:

APPROVAL OF AGENDA:

<u>CALL TO THE PUBLIC:</u> (Note: The Board reserves the right to not begin new business after 10:00 p.m.)

OPEN PUBLIC HEARING #1...Consideration of a special use application, impact assessment and sketch plan for a proposed adult foster care small group home for up to 6 adults located at 3201 E. Coon Lake Road, Howell on the north side of Coon Lake Road between Richardson and Westphal Roads. The request is petitioned by Work Skills, Inc.

- A. Recommendation of Special Use Application
- B. Recommendation of Environmental Impact Assessment (2-3-2020)
- C. Recommendation of Sketch Plan (2-6-2020)

OPEN PUBLIC HEARING #2...Review of a special use application, impact assessment and sketch plan for a proposed holistic veterinary clinic located at 2829 E. Grand River Avenue, Howell on the north side of Grand River Avenue between Char Ann and Meadowview Drive. The request is petitioned by Dr. Matthew Januszewski.

- A. Recommendation of Special Use Application
- B. Recommendation of Environmental Impact Assessment (1-30-2020)
- C. Recommendation of Sketch plan (2-5-2020)

OPEN PUBLIC HEARING #3...Review of a rezoning application, PUD application, PUD agreement, impact assessment and Conceptual PUD Plan for a proposed Non-Residential Planned Unit Development (NRPUD) with a proposed medical complex that includes a hospital, emergency center and medical office building. The rezoning request is from Public Recreation Facilities and Industrial (PRF and IND) to Non-Residential Planned Unit Development (NRPUD) for properties located at 4444 E. Grand River and vacant lot 2 on Parkway Drive. The request is for the following parcels: 4711-09-100-015 and 4711-09-100-020. The request is petitioned by Trinity Health.

- A. Recommendation of Rezoning and PUD Application
- B. Recommendation of PUD Agreement
- C. Recommendation of Impact Assessment (2-19-2020)
- D. Recommendation of Conceptual Plan (2-18-2020)

OPEN PUBLIC HEARING #4...Review of a rezoning application, PUD application, PUD agreement, impact assessment, and conceptual PUD plan for a proposed Redevelopment Planned Unit Development (RDPUD) with a new 37,275 sq. ft. indoor climate controlled storage building, a 7,000 sq. ft. multi-use building and a 2,336 sq. ft. commercial or drive-through restaurant. The rezoning requested is from Office Service District (OSD) to General Commercial District (GCD) with a Redevelopment Planned Unit Development (RDPUD) overlay located at 4525 and 4533 E. Grand River Avenue and 1098 Lawson Drive on the northwest corner of Grand River Avenue and Lawson Drive. The request is for the following parcels: 4711-09-200-005, 010, 014, 015, 016 and 017. The request is petitioned by BMH Realty, LLC.

- A. Recommendation of Rezoning and PUD Application
- B. Recommendation of PUD Agreement
- C. Recommendation of Impact Assessment (2-19-2020)
- D. Recommendation of Conceptual PUD Plan (2-19-2020)

ADMINISTRATIVE BUSINESS:

- Staff Report
 Approval of February 10, 2020 Planning Commission meeting minutes
 Member discussion
 Adjournment



GENOA CHARTER TOWNSHIP Special Land Use Application

This application **must** be accompanied by a site plan review application and the associated submittal requirements. (The Zoning Official may allow a less detailed sketch plan for a change in use.)

requirements. (The Zenning Official may allow a less detailed sketch plan for a change in use.)	
APPLICANT NAME & ADDRESS: Piet W. Lindhout, Lindhout Associates Architects, 10465 Citation Drive, Brighton 4	811
Submit a letter of Authorization from Property Owner if application is signed by Acting Agent.	
APPLICANT PHONE: (810)227-5668 EMAIL: pwl@lindhout.com	
OWNER NAME & ADDRESS: Work Skills Inc. 100 Summit Street, Brighton MI 48116	
SITE ADDRESS: 3201 East Coon Lake RoadPARCEL #(s): 4711-29-100-005	
OWNER PHONE: <u>734-320-0619</u> EMAIL: tinaj@wskills.com	
Location and brief description of site and surroundings: 7 acre site on Coon Lake Road with a single family residence and pole barn on the site. Numerous trees and varied topography are	
present, with similar parcels in the immediate area. The topography and vegetation creates visual and noise buffers between lot	s,
with little awareness of the next door neighbors. There is an old pole barn on the site and an abandoned travel trailer.	_
Proposed Use: A group home for up to 6 adults with developmental disabilities. There will be a 24/7 supervisor on site. Residents have mental and physical disabilities.	es
that make independent living difficult. Group living allows for support and supervision. All occupants work in the community.	
Describe how your request meets the Zoning Ordinance General Review Standards (section 19.03):	
a. Describe how the use will be compatible and in accordance with the goals, objectives, and policies of the Genoa Township Comprehensive Plan and subarea plans, and will promote the Statement of Purpose of the zoning district in which the use is proposed.	;
The group home residential status is compatible with the current residential zoning goals and policies. Disabled individuals make	up
a significant portion of our community and opportunities to live in a home close to work are limited.	
	_
 Describe how the use will be designed, constructed, operated, and maintained to be compatible with, and no significantly alter, the existing or intended character of the general vicinity. 	ot
The proposed improvements include expanding the garage and adding bedrooms. This expansion is compatible with the neighborhood and the	ie
character of the general vicinity. The proposed additions will include all new exterior materials on the existing home. The home is	in
disrepair and is in need of immediate upgrades. At this time it is an eyesore for the neighbors. The pole barn and travel trailer will be removed from the si	
c. How will the use be served adequately by essential public facilities and services such as highways, streets, police and fire protection, drainage structures, water and sewage facilities, refuse disposal and schools?	
The proposed use will not impact essential public services any more than a large family residing on the site. Similar facilities operated by Work Ski	lls

Corporation have not adversely impacts public services.

environment, public hea	alth, safety, or welfare by reason	materials potentially detrimental to the natural n of excessive production of traffic, noise, vibraties, how will the impacts be mitigated?	ion,
There will be no adverse imp	acts to the environment or local re	esidents created by the proposed group home.	
e. Does the use have speci If so, describe how the	fic criteria as listed in the Zonio criteria are met.	ng Ordinance (sections 3.03.02, 7.02.02, & 8.02.0	02)?
Section 3.03.02 (j) requires 1	500 feet from other group homes.	. We comply.	
Section 7.02.02 is not applica	able as we are not zoned Comme	rcial or Service.	
Section 8.02.02 is not applica	able as we are not zoned Industria	al.	
THIS APPLICATION ARI I AGREE TO DESIGN, CO BUILDINGS, STRUCTUF ACCORDANCE WITH TI ORDINANCE, AND SUC THIS PERMIT.	E TRUE AND ACCURATE TO DNSTRUCT AND OPERATE, LES, AND FACILITIES WHIC HE STATED REQUIREMENT H ADDITIONAL LIMITS AND	D DATA ATTACHED TO AND MADE PART OF THE BEST OF MY KNOWLEDGE AND BEIT AND MAINTAIN THESE PREMISES AND THE ARE GOVERNED BY THIS PERMIT IN THE GENOA TOWNSHIP ZONING DISAFEGUARDS AS MAY BE MADE A PART	LIEF. HE
APPLICATION FOR THIS	S SPECIAL LAND USE PERM	STATES THAT THEY ARE THE DESCRIBED ABOVE AND MAKES SUBJECTION. Purchase A	ET TO
ADDRESS: 100 Sun	MIT ST, BRIGHT	on, M1 48116	
	/		
Contact Information - Review	w Letters and Correspondence s	shall be forwarded to the following:	7
Piet W. Lindhout			
Name	of Lindhout Associates Business Affiliation	at pwl@lindhout.com Email	
vanie	Dusiness Affination	Emaii	
			=
	FEE EXCEEDANCE AG	REEMENT	
 Planning Commission mee equired to pay the actual incur 	ting. If additional reviews or mared costs for the additional reviewrent with submittal to the Townderstanding of this policy.	e allocated two (2) consultant reviews and one leetings are necessary, the applicant will be liews. If applicable, additional review fee with board. By signing below, applicant DATE: 24 20 20 ONE: 810-227-5668	



GENOA CHARTER TOWNSHIP APPLICATION Sketch Plan Review

TO THE GENOA TOWNSHIP PLANNING COMMISSION:
APPLICANT NAME & ADDRESS: Piet Lindhout, Lindhout Associates Architects, 10465 Citation Dr. 48116 If applicant is not the owner, a letter of Authorization from Property Owner is needed.
OWNER'S NAME & ADDRESS: Work Skills Corporation, 100 Summit St., Brighton, MI 48116
SITE ADDRESS: 3201 East Coon Lake Rd. PARCEL #(s): 4711-29-100-005
APPLICANT PHONE: () OWNER PHONE: ()
LOCATION AND BRIEF DESCRIPTION OF SITE: North side of Coon Lake Road between
Richardson and Westphal Roads. Single family resident on 7+ acres of rolling topography.
BRIEF STATEMENT OF PROPOSED USE: Proposed use to remain residential. A
group home of up to 6 adult residents with full time supervision is proposed.
THE FOLLOWING IMPROVEMENTS ARE PROPOSED: The garage is proposed to be demolished and
rebuilt to accommodate handicapped accessible vehicles and have an interior
entrance ramp. A 4 bedroom addition is proposed on the east side.
I HEREBY CERTIFY THAT ALL INFORMATION AND DATA ATTACHED TO AND MADE PART OF THIS APPLICATION IS TRUE AND ACCURATE TO THE BEST OF MY KNOWLEDGE AND BELIEF.
BY: Piet W. Lindhout
ADDRESS: 10465 Citation Drive, Brighton, MI 48116
ontact Information - Review Letters and Correspondence shall be forwarded to the following:
Piet W. Lindhout of Lindhout Associates Architects at pwl@lindhout.com Business Affiliation Email Address
Name Business Affiliation Email Address
FEE EXCEEDANCE AGREEMENT Il sketch plans are allocated one (1) consultant review and one (1) Planning Commission meeting. If additional

reviews or meetings are necessary, the applicant will be required to pay the actual incurred costs for the additional reviews. If applicable, additional review fee payment will be required concurrent with submittal for a Land Use Permit. By signing below, applicant indicates agreement and full understanding of this policy.

SIGNATURE:	DATE: 2/6/2020
PRINT NAME: Piet W. Lindhout	PHONE: 810-227-5668

Proposed Group Home Work Skills Corporation 3201 East Coon Lake Rd. Genoa Township, Michigan February 10, 2020

Supplemental Information Relative to Special Use Permit

At this time, there is a need for a supportive living option as there are currently over 40 Livingston County residents being placed in living arrangements out of county, away from work, support services, family, and friends. WSC's Residential Group home will offer personalized 24-hour supervised care to six or fewer residents. There will never be a time where the ratio of residents to staff exceeds six residents to one staff member. Living arrangements include a private bedroom and bathroom for each resident with a common area for meals and recreation. Services will include meal support and assistance with daily living activities such as laundry, managing medications, bathing and dressing. Supports provided will be designed for residents who are relatively independent, communicative and free of complex, debilitating illness. The goal for services will be to improve independent living skills and quality of life. This will be a home licensed by the State of Michigan. The licensing process verifies that the home has passed stringent facility and operational guidelines that conform to state safety laws and regulations, including checks on the quality of the nutrition, care and health of residents as well as continual upkeep to the home. Credentialing and background checks will be completed on all staff members employed at the home.

For reference, this is a typical day in the group home.

Time	Activity	Staff Responsibility
6:30-7:30 am	Wake up, shower & dress for	Provide prompts to wake up, assist with
	the day	showering and dressing as needed.
7:30-8:15 am	Make breakfast, eat and clean	Encourage all residents to participate in
	up	preparing meals and clean up as needed.
		Assistance with eating provided if needed.
		Work on independent living skills.
8:15-8:30 am	Brush teeth, grooming (work	Prompting and assistance to complete
	ready appearance) and prepare	grooming and tooth brushing as needed.
	to go to work	
8:30-9:00 am	Travel to work	Transportation provided to all residents in
		WSC van.
9:00 am-3:00 pm	Work at WSC	Support provided by WSC staff.
3:00-3:30 pm	Travel home	Transportation provided to all residents in
		WSC van.
3:30-6:00 pm	Leisure time/Community outing	Staff to assist with identifying leisure activities,
		transportation to grocery shopping/personal
		shopping, recreational activities, etc.

Proposed Group Home Work Skills Corporation 3201 East Coon Lake Rd. Genoa Township, Michigan February 10, 2020

Time	Activity	Staff Responsibility
6:00-6:30 pm	Prepare Dinner	Encourage all residents to participate in
		preparing meals and clean up as needed.
		Work on independent living skills.
6:30-7:00 pm	Dinner	Assistance with eating provided as needed.
7:00-7:30 pm	Clean up from dinner/pack	Encourage all residents to assist with clean up
	lunch for tomorrow	and pack lunch for work tomorrow.
7:30-9:00 pm	Leisure Activities/Chores	Staff to assist with identifying leisure activities
		or personal chores (cleaning, laundry, etc.)
		Work on independent living skills.
9:00-9:15 pm	Brush teeth, general grooming,	Staff to provide prompts and assist with
	dress for bed	completion of tasks as needed.
9:15 pm+	Bed or leisure activities until bed	Staff to monitor for safety.

Work Skills is a local employer, employment facilitator, and coach to many local developmentally disabled residents. The proposed group home will allow Work Skills to increase support of the disabled and continue the goals and objectives of the Not For Profit Agency.



Planning Commission Genoa Township 2911 Dorr Road Brighton, Michigan 48116

Attention:	Kelly Van Marter, AICP
	Planning Director and Assistant Township Manager
Subject:	Work Skills Corporation – Special Land Use and Sketch Plan Review #1
Location:	3201 East Coon Lake road – north side of East Coon Lake, between Chilson and Westphal
	Roads
Zoning:	CE Country Estate District

Dear Commissioners:

At the Township's request, we have reviewed the submittal from Work Skills Corporation requesting special land use and sketch plan review/approval for an adult foster care small group home at 3201 East Coon Lake Road.

A. Summary

- 1. We are generally of the opinion that the proposal complies with the special land use standards of Section 19.03.
- 2. The applicant must address any comments provided by the Township Engineer or Brighton Area Fire Authority.
- 3. The Township may wish to request supporting documentation confirming that the use requirement of Section 3.03.02(j) is met.
- 4. The applicant must demonstrate that the building height limit for the CE District is met.
- 5. If exterior site lighting is proposed, the applicant must provide details.
- 6. If signage is proposed, we request the applicant provide details.
- 7. Additionally, the applicant should be aware that a sign permit is required from the Township prior to installation of any new signage.

B. Proposal/Process

The applicant seeks approval for an adult foster care small group home for 6 residents and full time supervision/care.

The project includes site improvements (new parking lot, removal of abandoned vehicles, demolition of a pole barn, and a new deck/patio area in the rear yard), as well as additions to each side of the existing residence.

Adult foster care small group homes are allowed with special land use approval in accordance with Table 3.03 of the Genoa Township Zoning Ordinance. Such uses are also subject to the requirements of Section 3.03.02(j).

Procedurally, the Planning Commission is to review the special land use, sketch plan and impact assessment and provide a recommendation on each to the Township Board following a public hearing.

248.586.0505 www.safebuilt.com



Aerial view of site and surroundings (looking north)

C. Special Land Use Review

Section 19.03 of the Zoning Ordinance identifies the review criteria for Special Land Use applications as follows:

1. Master Plan. The Township Master Plan identifies the subject site, as well as the adjacent and surrounding properties, as Agriculture/Country Estate.

The Master Plan states that "this classification is recommended for single family residences on lots no smaller than 5 acres." The existing residence is situated on approximately 7 acres.

The land use classification does not refer to residential care uses, though two of the Plan's Land Use Goals are to:

Accommodate a variety of land uses that are located in a logical pattern and complement community goals, the surrounding land uses, environment, capacity of roads and the sanitary sewer, and public water system capabilities.

Support "aging in place" by providing housing, recreation, and transportation options for seniors.

Aside from the parking lot, the site will retain the appearance of a conventional residential property and, while the "aging in place" goal references the senior population specifically, it could also be viewed as applying to the disabled population.

More specifically, the submittal indicates that several Livingston County residents in need of supportive living have been placed in residences outside of the County due to a lack of options.

2. Compatibility. This area of the Township is generally developed with single-family residences on relatively large lots with ample wooded areas, consistent with both current zoning and the future land use classification. Residential care uses are intended to be compatible with single-family areas.

In this instance, the difference between a special land use and one permitted by right is the fact that the licensee will not also live in the residence.

The applicant has limited the potential intensity of the use by requesting only 6 residents (whereas the Ordinance permits requests of up to 12). As such, there will be fewer residents and employees than could otherwise be allowed.

The project includes exterior backyard improvements that would be typical of a single-family residence, and outdoor usage will be well buffered from neighbors via the space between residences and the existing wooded areas.

The requirement of Section 3.03.02(j) is also intended to ensure such uses remain compatible with their surroundings. As noted in Paragraph D below, the applicant states that the requirement is met, though the Township may wish to request supporting documentation.

- **3. Public Facilities and Services.** The site is served by well and septic systems. As such, we do anticipate adverse impacts on public facilities and services. With that being said, the applicant must address any issues raised by the Township Engineer or Brighton Area Fire Authority.
- **4. Impacts.** As previously noted, residential care uses are intended to blend in with their surroundings. The only noticeable difference will be the presence of a small parking lot, which will be well screened from off-site views via existing wooded areas.

Given the nature of the use and the property, the proposal is not expected to adversely impact adjacent or surrounding properties and/or uses.

5. Mitigation. Should other concerns arise as part of the review process, the Township may require additional improvements to mitigate potential adverse impacts.

D. Use Requirements

As previously noted, adult foster care small group homes are subject to the use requirements of Section 3.03.02(j), as follows:

1. Adult foster care large group home or small group home shall be at least one thousand five hundred (1,500) feet from another group day care home or similar facility. The Township Board shall determine, following review and recommendation by the Planning Commission, that the facility will not result in an excessive concentration of adult care facilities within a neighborhood.

The application form states that there are no other similar facilities within 1,500 feet. The Township may wish to request supporting documentation confirming that this statement is accurate.

E. Sketch Plan Review

1. Dimensional Requirements. The proposal has been reviewed for compliance with the dimensional requirements of the CE District, as follows:

	Min. Lot Req.		Minimum Yard Setbacks (feet)			acks (feet)	Max. Height
	Area	Width	Front	Side	Rear	Parking Lot	(feet)
	(acres)	(feet)	Yard	Yard	Yard		
CE	5	220	75	40	60	N/A	35
Proposed	7	330	163	50 (W)	604	70 front	Information not
				102 (E)		30 side (W)	provided

Genoa Township Planning Commission **Work Skills Corporation** Special Land Use and Sketch Plan Review #1 Page 4

The submittal does not include building elevation drawings depicting the existing/proposed building height(s).

2. Parking. Based upon the number of residents (6) and maximum number of staff (8), Section 14.04 requires 10 parking spaces.

The proposal includes a total of 10 parking spaces – 8 surface spaces and 2 within the proposed garage addition.

3. Landscaping. The site is heavily wooded, and the submittal does not identify any tree removal around the perimeter of the site. (It appears that some tree removal may be necessary for the proposed building additions.)

The plan includes 8 tightly spaced evergreen trees along the westerly side lot line to screen the parking lot from the adjacent property.

- 4. Lighting. The submittal does not identify any exterior site lighting. If lighting is proposed, the applicant must provide details to ensure that adjacent properties are protected from light trespass.
- 5. Signage. No signage is depicted on the plan. If signage is proposed, we request the applicant provide details in accordance with the standards of Article 16.

If the signage is proposed, the applicant must obtain a sign permit from the Township prior to installation.

Should you have any questions concerning this matter, please do not hesitate to contact our office.

Respectfully,

SAFEBUILT STUDIO

Brian V. Borden, AICP

Planning Manager



March 3, 2020

Ms. Kelly Van Marter Genoa Township 2911 Dorr Road Brighton, MI 48116

Re: Work Skills Special Use Sketch Plan Review No. 1

Dear Ms. Van Marter:

Tetra Tech conducted a rezoning plan review of the Work Skills Special Use application. The sketch plan, last dated February 6, 2020, was prepared by Lindhout Associates Architects on behalf of Work Skills, Inc. The application proposes renovating an existing residential building to be used as a group home for up to 6 adults. The property is located on East Coon Lake Road, just east of Westhill Drive. We offer the following comments:

DRAINAGE AND GRADING

1. The Petitioner is proposing 8 new parking spaces to be constructed, which will cause a minor increase in stormwater runoff. The application notes that a swale will be constructed to direct the increased runoff to the lower side of the property to the north. This area is shown on the sketch plan and directing the flow to this location will not adversely affect the neighboring properties.

UTILITIES

1. The application notes that the new use will use the existing well and septic on site. The Petitioner should provide documentation from the County Health Department documenting the suitability of the existing septic system for the increased use.

Contingent on the above comments being addressed, we have no engineering related concerns to the proposed special land use.

Please call or email if you have any questions.

Sincerely,

Gary J. Markstrom, P.E.

Vice President

Shelby Scherdt
Shelby Scherdt
Project Engineer

BRIGHTON AREA FIRE AUTHORITY



615 W. Grand River Ave. Brighton, MI 48116 o: 810-229-6640 f: 810-229-1619

February 26, 2020

Kelly VanMarter Genoa Township 2911 Dorr Road Brighton, MI 48116

RE: Workskills Special Use

3201 E. Coon Lake Road

Howell, MI

Dear Kelly:

The Brighton Area Fire Department has reviewed the above-mentioned site plan. The plans were received for review on February 13, 2020, and the drawings are dated January 29, 2020. The project is based on an existing 7-acre parcel with an existing 1,644 square foot single-family home that will undergo renovation, addition, and change of use to a Group Home for residents with mental and physical disabilities. A 1,845 square foot addition to the East and a 970 square foot addition for a new garage and laundry area to the West. The building will be approximately 3,619 square feet in area. The facility will have 6 total sleeping units and the facility will undergo a complete renovation.

The plan review is based on the requirements of the International Fire Code (IFC) 2018 edition.

- 1. There is no municipal water located near the premise. MHOG water is located approximately 1-mile to the East. The structure will be constructed similar to a single-family dwelling under the State of Michigan building code and the State of Michigan Licensing only requires fire sprinklers in similar facilities with 7 or more residents, however, it is highly recommended that the structure be equipped with a 13R or 13D sprinkler system due to the high-risk nature of the use and its occupant's ability to self-evacuate.
- 2. The building shall include the building address on the building. The address shall be a **minimum of 6"** high letters of contrasting colors and be clearly visible from the street. The location and size shall be verified prior to installation.

IFC 505.1

3. The entry to the access road into the site shall be a minimum of 26-feet wide and narrow to no less than 20-feet wide. With a width of 26' wide, the non-parking side of the drive shall be marked as a fire lane. Include the location of the proposed fire lane signage and include a detail of the fire lane sign in the submittal. Access roads to the site shall be provided and maintained during construction. Access roads shall be constructed to be capable of supporting the imposed load of fire apparatus weighing at least 75,000 pounds.

IFC D 103.6 IFC D 103.1 IFC D 102.1 IFC D 103.3

4. A minimum vertical clearance of 13½-feet shall be maintained along the drive.



BRIGHTON AREA FIRE AUTHORITY

February 26, 2020 Page 2 Workskills Special Use 3201 E. Coon Lake Rd. Site Plan Review

- 5. Due to the length of the dead-end drive, an emergency vehicle turn-around compliant with Appendix D shall be provided.
- 6. The location of a key box (Knox Box) shall be indicated on future submittals. The Knox box will be located adjacent to the front door of the structure.

IFC 506.1

7. Provide names, addresses, phone numbers, emails of owner or owner's agent, contractor, architect, on-site project supervisor.

Additional comments will be given during the building plan review process (specific to the building plans and occupancy). The applicant is reminded that the fire authority must review the fire protection systems submittals (sprinkler & alarm) prior to permit issuance by the Building Department and that the authority will also review the building plans for life safety requirements in conjunction with the Building Department. If you have any questions about the comments on this plan review please contact me at 810-229-6640.

Cordially,

Rick Boisvert, CFPS Fire Marshal February 3, 2020

Genoa Township 2980 Dorr Road Brighton, MI 48116

Re: Special Use and Sketch Plan Approval – Work Skills Corporation E Coon Lake Rd Group Home

IMPACT ASSESSMENT

a. Name and Address of persons responsible for preparation of the impact assessment and a brief statement of their qualifications.

Piet W. Lindhout Tina Jackson
Lindhout Associates Architects Work Skills Coporation
10465 Citation Drive 100 Summit St.
Brighton, MI 48116 Brighton, MI 48116
810-227-5668 734-320-0619

Registered Michigan Architect CEO, Work Skills Inc.

b. **Written Description/Analysis of the Project Site** - including all existing structures, manmade facilities, and natural features.

As shown on the sketch plan submitted, the site is roughly 7 acres, varied topography and vegetation, and occupied by a single family residence. The aerial photograph shows a number of abandoned cars and other equipment, a travel trailer, and a pole barn on the site. There is a low area immediately north of the existing residence that appears to collect moisture during the wetter months.

c. **Impact on Natural Features** - environmental character of site prior to and following development.

Prior to development, the environmental character is as described above in the natural features description. The proposed development would include removal of all remaining abandoned cars, travel trailers and equipment, demolition of the pole barn, and grading of the pole barn site. The environment will improve with these proposed changes.

The proposed additions to the home will be in character with the existing and neighboring residences. New landscaping at the perimeter of the home and the proposed staff parking spaces will enhance the natural environment.

d. **Impact on Stormwater Management** - including measures to control soil erosion and sedimentation during grading and construction operations.

The site plan depicts no new detention basins to be constructed. The proposed parking spaces for staff of the group home will create a minor increase in storm water run-off, which will be handled in accordance to the Township Engineer's and County requirements.

I grassed swale will be constructed directing the run-off to the low area north of the residence.

The site construction documents will also indicate permanent and temporary soil erosion and sedimentation control measures to be used on site. They consist of silt fencing, and a crushed concrete apron at the entry prior to and during construction, as well as hydro-seeding for permanent control.

e. **Impact on Surrounding Land Uses**. Types of proposed uses and man-made facilities. Describe project phasing, indication of how proposed use conforms or conflicts with existing and potential development patterns. Describe increases in light, noise, and air pollution which could negatively impact adjacent properties.

The 3,619 square foot home will be built in a manner consistent with standard residential construction practices and not adversely impact the community. The parking area is screened from neighbors with considerable landscaping on the west side of the parking area.

Work Skills operates a similar Group Home in Howell. Details of this home are attached in a separate document from Work Skills Corporation to better illustrate the proposed operations and character of the home.

As a group home, staff is required to be on site 24/7. This adds to the safety of the home and provides constant monitoring of all occupants and systems.

The surrounding uses will be impacted in a manner consistent with a single family home. Since the occupants do not drive, traffic is limited to staff arrival and departure. Activities such as outdoor barbeques, gardening, and other common residential outdoor activities are anticipated. The light, noise and pollution produced by the proposed use will be consistent with typical residential occupancies.

f. Impact on Public Facilities and Services.

There will be 6 residents on the site.

There will be approximately 2 employees on site during most periods. There will be professional staff visits during the work day, and occasional visits from social workers and other support staff. In a worst case scenario, we anticipate 8 staff to be on site during shift changes that include outside professionals to be present.

There will be no impact on public schools.

Fire trucks, EMS and police will have access to site.

g. Impact on Public Utilities.

The site will be served by on-site septic and well, per the Livingston County Department of Public Health, Environmental Health Division standards.

There will be no extraordinary requirements for water or sewer service.

Electrical Service will be from existing Detroit Edison Service.

Natural Gas will be provided from the existing Consumers Energy Service.

h. Storage and Handling of any Hazardous Materials.

There are no known pollutants or hazardous materials to be permanently housed on site, beyond normal cleaning and maintenance supplies.

i. **Impact on Traffic and Pedestrians**. A description of traffic volume to be generated based on actual counts of similar uses in Michigan.

As noted above, the traffic generated is expected to be 2-4 cars in the morning and afternoons, with a worst case cross over of 8 cars present during shift change of staff. The Trip Generation Guide of the Institute of Transportation Engineers does not list Group Homes in it's database.

The home residents do not drive. The 2 vans housed in the garage are used to transport clients to work and doctors appointments.

Work Skills anticipates very little visitor traffic. Visitors are most often on site on weekends and holidays and are typically in one car at a time. There will be no outside professional visits on weekends and holidays.

j. **Special Provisions**. A description of any deed restrictions, protective covenants, master deed, or association bylaws.

There are no deed restrictions at this time.

k. Source Listing:

Genoa Township Zoning Ordinance Genoa Township Sketch Plan and Special Use Applications I.T.E. Trip Generation Guide, 7th Edition

END OF IMPACT ACCESSMENT



LINDHOUT ASSOCIATES architects aia po

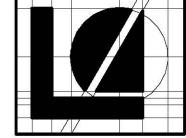
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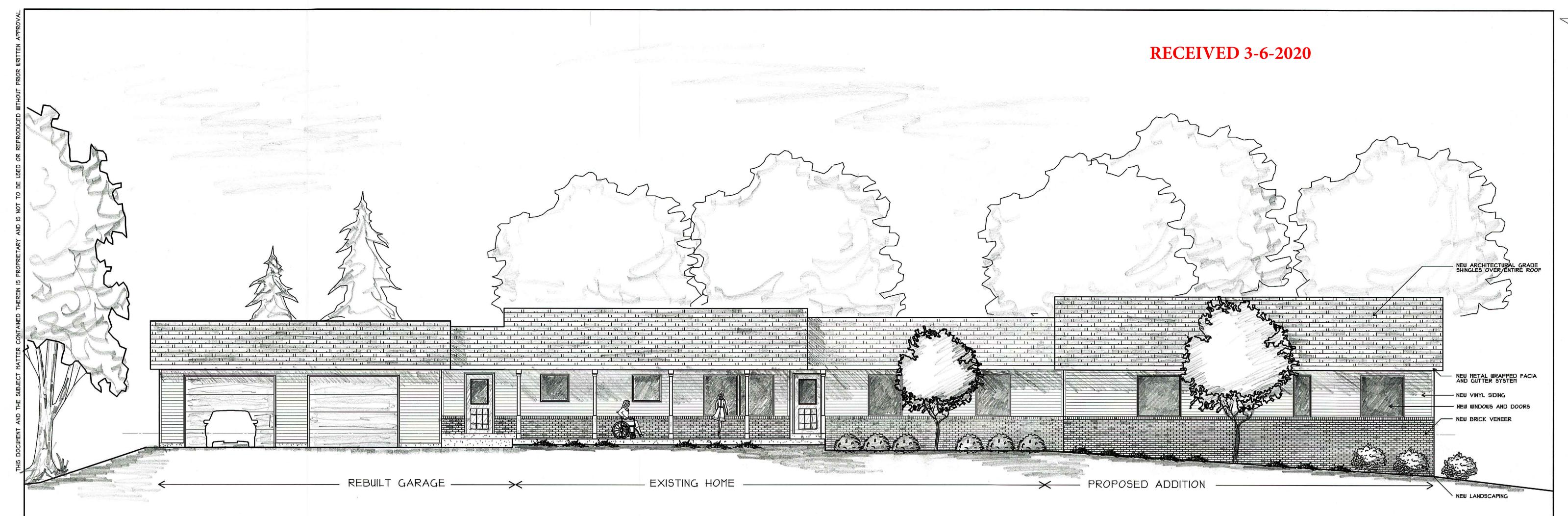
GENOA TOWNSHIP, MICHIGAN SITE SKETCH PLAN

2-6-20 SUBMISSION TO TOWNSHIP app'd: XXX 1-29-20 CLIENT REVIEW issued for

consultant

10465 citation drive, brighton, michigan 48116-9510 www.lindhout.com (810)227-5668 fax: (810)227-5855





PROPOSED COON LAKE ROAD ELEVATION



EXISTING COON LAKE ROAD ELEVATION

PROPOSED FLOOR PLANS



ndhout Associates architects aia pc 5 citation drive, brighton, michigan 48116-9510 lindhout.com (810)227-5668 fax:(810)227-5855

cons

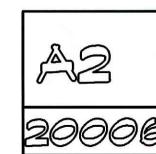
3-2-2020 CLIENT REVIEW date

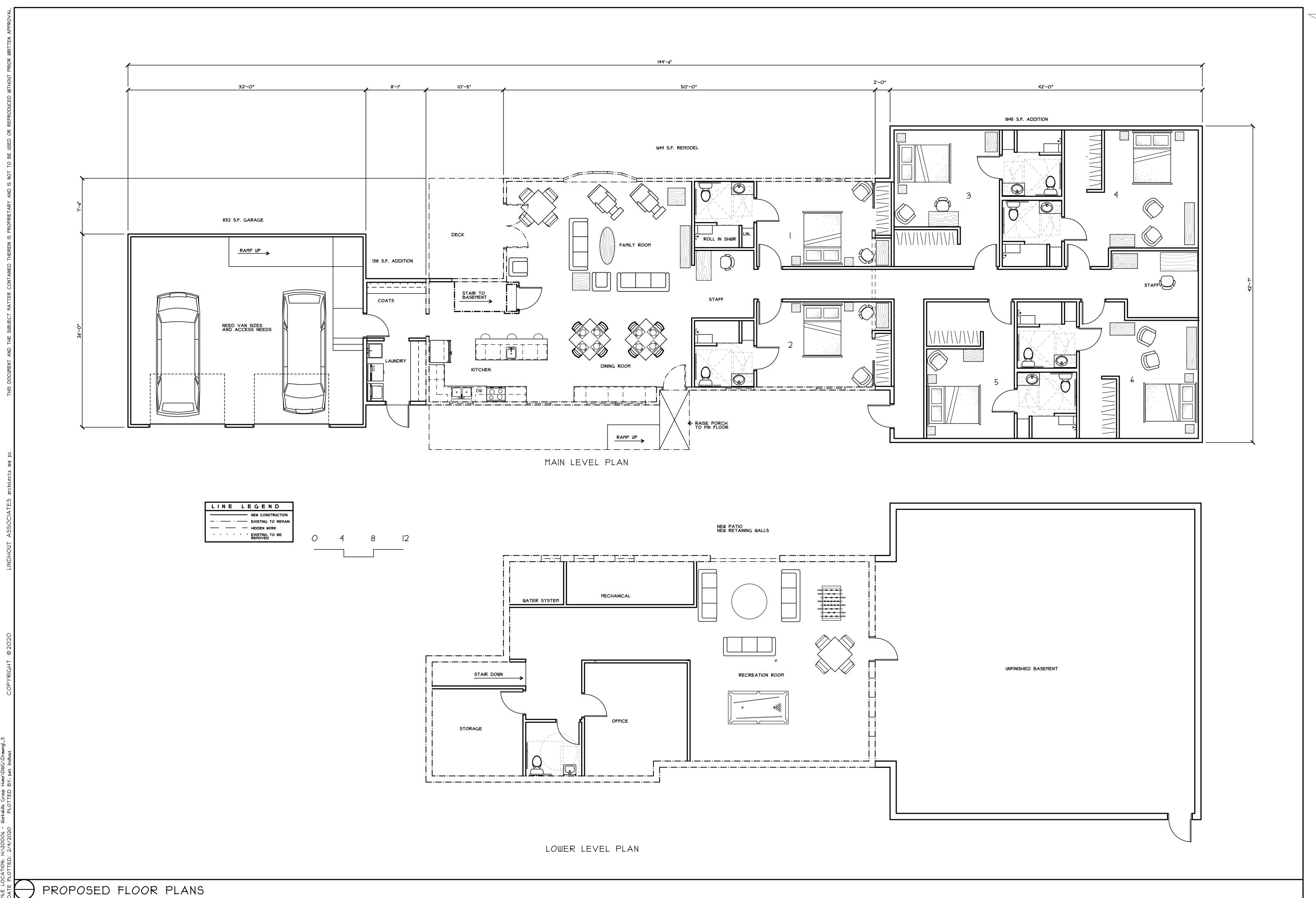
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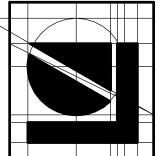
ORKSKILLS, INC.

SNOA TOWNSHIP, MICHIGAN

XTERIOR ELEVATIO







architects aid pc

consultant

2-4-2020 SPECIAL LAND USE / SKETCH PLAN date

ck'd: xxx app'd: xxx 2-4-20

LLS, INC. MICHIGAN LOOR PLANS

JORKSKILLS, INGENOA TOWNSHIP, MICHIGAN

<u></u> A1



GENOA CHARTER TOWNSHIP APPLICATION Sketch Plan Review

TO THE GENOA TOWNSHIP PLANNING COMMISSION:

APPLICANT NAME & ADDRESS: Matthew Januszewski, 5047 Queensway, Howell, MI 48843
If applicant is not the owner, a letter of Authorization from Property Owner is needed.
OWNER'S NAME & ADDRESS:
SITE ADDRESS: 2829 East Grand River Avenue, Howell, MI PARCEL #(s): 4711-06-200-077
APPLICANT PHONE: (517) 304-7224 OWNER PHONE: (517) 304-7224
LOCATION AND BRIEF DESCRIPTION OF SITE: The subject property has an existing 2,088 square-foot
commercial building that sits up on a hill with limited visibility from Grand River Avenue. DisplayMax has occupied
the existing building as a fully-functioning office since 2011. The lot size is 1.43 acres with mature landscaping.
BRIEF STATEMENT OF PROPOSED USE: Integrative Veterinary Services is a holistic veterinary practice
that does NOT offer grooming, boarding or overnight care. We are a specialty practice in business since 2011.
THE FOLLOWING IMPROVEMENTS ARE PROPOSED: There will be no changes to the exterior of the building
(including no changes to parking, landscaping, and lighting). Interior improvements will be painting, replacing
carpet with vinyl plank flooring, updating kitchen counters and backsplash and adding a reception desk.
I HEREBY CERTIFY THAT ALL INFORMATION AND DATA ATTACHED TO AND MADE PART OF THIS APPLICATION IS TRUE AND ACCURATE TO THE BEST OF MY KNOWLEDGE AND BELIEF.
BY: 2
ADDRESS: 5047 Queensway, Howell, MI 48843
ntact Information - Review Letters and Correspondence shall be forwarded to the following:
Matthew Januszewski of Integrative Veterinary Services at Integrative Veterinary Services at Email Address
Name Business Affiliation Email Address

FEE EXCEEDANCE AGREEMENT

All sketch plans are allocated one (1) consultant review and one (1) Planning Commission meeting. If additional reviews or meetings are necessary, the applicant will be required to pay the actual incurred costs for the additional reviews. If applicable, additional review fee payment will be required concurrent with submittal for a Land Use Permit. By signing below, applicant indicates agreement and full understanding of this policy.

SIGNATURE: Matthew Januszewski

_DATE:_01/29/2020

PHONE: 517-304-7224



GENOA CHARTER TOWNSHIP Special Land Use Application

This application **must** be accompanied by a site plan review application and the associated submittal requirements. (The Zoning Official may allow a less detailed sketch plan for a change in use.)

APPLICANT NAME & ADDRESS: Dr. Matthew Januszewski, 5047 Queensway, Howell, MI 48843
Submit a letter of Authorization from Property Owner if application is signed by Acting Agent.
APPLICANT PHONE: (517) 304-7224 EMAIL: mljanuszewski@yahoo.com
OWNER NAME & ADDRESS: Januszewski Investments, LLC, 5047 Queensway, Howell, MI 48843
SITE ADDRESS: 2829 East Grand River Ave., Howell, MI PARCEL #(s): 4711-06-200-077
OWNER PHONE: (517)304-7224 EMAIL: mljanuszewski@yahoo.com
Location and brief description of site and surroundings: Please see attached.
Proposed Use: Please see attached.
Describe how your request meets the Zoning Ordinance General Review Standards (section 19.03):
a. Describe how the use will be compatible and in accordance with the goals, objectives, and policies of the Genoa Township Comprehensive Plan and subarea plans, and will promote the Statement of Purpose of the zoning district in which the use is proposed.
Please see attached.
b. Describe how the use will be designed, constructed, operated, and maintained to be compatible with, and not significantly alter, the existing or intended character of the general vicinity.
Please see attached.
c. How will the use be served adequately by essential public facilities and services such as highways, streets, police and fire protection, drainage structures, water and sewage facilities, refuse disposal and schools?
Please see attached.

environment, public h	y uses, activities, processes, or materials potentially detrimental to the natural ealth, safety, or welfare by reason of excessive production of traffic, noise, vibration, glare, or other such nuisance? If so, how will the impacts be mitigated?
Please see attached.	
e. Does the use have spe If so, describe how the	rific criteria as listed in the Zoning Ordinance (sections 3.03.02, 7.02.02, & 8.02.02)? criteria are met.
Please see attached.	
THIS APPLICATION A I AGREE TO DESIGN, BUILDINGS, STRUCTU ACCORDANCE WITH	IAT ALL INFORMATION AND DATA ATTACHED TO AND MADE PART OF RE TRUE AND ACCURATE TO THE BEST OF MY KNOWLEDGE AND BELIEF. CONSTRUCT AND OPERATE, AND MAINTAIN THESE PREMISES AND THE PREMISES AND THE PREMISES AND THE GES, AND FACILITIES WHICH ARE GOVERNED BY THIS PERMIT IN THE STATED REQUIREMENTS OF THE GENOA TOWNSHIP ZONING CH ADDITIONAL LIMITS AND SAFEGUARDS AS MAY BE MADE A PART OF
APPLICATION FOR TH	PROPERTY OF PROPERTIES DESCRIBED ABOVE AND MAKES IS SPECIAL LAND USE PERMIT
BY: (1)	
ADDRESS: 5047 Queens	way, Howell, MI 48943
Contact Information Pay	ew Letters and Correspondence shall be forwarded to the following:
Matthew Januszewski Name	of Invegrative Veterinary Services at mljanuszewski@yahoo.com Business Affiliation Email
	FEE EXCEEDANCE AGREEMENT
 Planning Commission mequired to pay the actual in payment will be required co 	riew fee schedule, all site plans are allocated two (2) consultant reviews and one ceting. If additional reviews or meetings are necessary, the applicant will be curred costs for the additional reviews. If applicable, additional review fee neurrent with submittal to the Township Board. By signing below, applicant understanding of this policy.
SIGNATURE:	DATE: 01/28/2020
PRINT NAME: Matthew Ja	nuszewski PHONE: 517-304-7224

CONFIDENTIAL

L-4182 R

01/27/2020 01:31 PM

REAL PROPERTY STATEMENT

This form is issued under authority of the General Property Tax Act. (See Section 211.19 on reverse side.) Filing is mandatory. Failure to file is punishable by fine and/or imprisonment.

NOTE: If any information provided in the boxes is incorrect, cross out the incorrect information and write in the correct information.

1. PLEASE FILE THIS STATEMENT ON OR BEFORE: 02/10/2020 Name and Address MAIL THIS STATEMENT TO: JANUSZEWSKI INVESTMENTS LLC **GENOA CHARTER TOWNSHIP** 1122 S MICHIGAN AVE **DEBRA L ROJEWSKI HOWELL** 48843 **2911 DORR RD BRIGHTON MI 48116** TO BE COMPLETED BY THE MICHIGAN STATE TAX COMMISSION County City/Township Assessment Roll Parcel Code Number LIVINGSTON **GENOA CHARTER TOWNSHIP** 4711-06-200-077 Address of Property 2829 E GRAND RIVER Property Description SEC 6 T2N R5E COMM E 1/4 COR TH N01*E 673.34 FT TH N61*W 1371.43 FT TO POB TH N61*W 135.60 FT TH N20*E 462.36 FT TH S62*E 136.47 FT TH S20*W 464.54 FT TO POB CONT 1.44 AC M/L SPLIT 4/92 FR 003, 071, 072 CORR LEGAL 3/98 TO BE COMPLETED BY THE BUYER Sale Price Date of Purchase or Sale Cash Mortgage Assumption* Combination of 1-6 or other Land Contract* Second Mortgage* Explain 3. New Mortgage* 6. Purchase Money Mortgage to Sellers* * Specify Type of Mortgage or Land Contract **FHA** VA **FmHA Land Contract** Wraparound or Blended Rate **MSHDA** Variable Rate Buvdown Purchase Money Mortgage Monthly Payment Down Amount Interest **Authorization Terms** Buydown or Points Paid by seller Balloon Payment Do Not Include Taxes Payment Financed Rate Years or Months if Included in the Sales Price Due Date \$ 61.00 \$ 1-21-25 Secondary Financing Is the buyer a relative of the seller or a business partner with the seller? YES - Explain: Was there any Personal Property (such as furniture, boats, lawnmowers, and appliances that were not built in) NO YES - If yes, describe below. Included in the Sale Price Listed Above? Describe Personal Property Estimated Cash Value of Personal Property Do you Believe the Sale Price Indicates the Actual Market Value of the Property at the Time of Sale? NO - If no, describe below. If you answered NO above, why do you feel the sale price was higher or lower than the price should have been? Did you purchase the Property at Public Auction?.... YES NO If you answered YES above, : Was the auction held by a governmental agency?..... YES NO Was the auction part of a bankruptcy proceeding?..... YES NO CERTIFICATION - I hereby declare that the above is a complete and true statement. Signature of Owner (Buyer): Date:



Planning Commission Genoa Township 2911 Dorr Road Brighton, Michigan 48116

Attention:	Kelly Van Marter, AICP
	Planning Director and Assistant Township Manager
Subject:	Integrative Veterinary Services – Special Land Use and Sketch Plan Review #1
Location:	2829 East Grand River Avenue – north side of Grand River, east of Chilson Road
Zoning:	GCD General Commercial District

Dear Commissioners:

At the Township's request, we have reviewed the submittal from Integrative Veterinary Services requesting special land use and sketch plan review/approval for a new veterinary clinic in the existing office building at 2829 East Grand River Road.

A. Summary

- 1. Provided the use requirements are met to the Township's satisfaction and any comments from the Township Engineer or Brighton Area Fire Authority are addressed, we are of the opinion that the general special land use standards of Section 19.03 are met.
- 2. We are of the opinion that the use requirements of Section 7.02.02(x) are met.
- 3. The existing site generally meets or exceeds current Ordinance standards with respect to dimensional standards, parking and landscaping.
- 4. The Township may wish to request that the applicant provide details of existing light pole fixtures and require upgrades if they do not meet current standards.
- 5. The applicant must obtain a sign permit from the Township prior to installation of any new signage.

B. Proposal/Process

The project entails a new veterinary clinic within an existing 2,088 square foot office building. Veterinary clinics without boarding or overnight care are allowed with special land use approval in accordance with Table 7.02 of the Genoa Township Zoning Ordinance. Such uses are also subject to the requirements of Section 7.02.02(x).

The sketch plan does not propose any exterior site improvements; however, the request for a new special land use on a developed site provides the Township with an opportunity to mitigate existing deficiencies from current Ordinance standards.

Procedurally, the Planning Commission is to review the special land use, sketch plan and impact assessment and provide a recommendation on each to the Township Board following a public hearing.

248.586.0505 www.safebuilt.com



Aerial view of site and surroundings (looking north)

C. Special Land Use Review

Section 19.03 of the Zoning Ordinance identifies the review criteria for Special Land Use applications as follows:

1. Master Plan. The Township Master Plan identifies the subject site, as well as the adjacent properties fronting Grand River, as General Commercial.

This category is intended for , which is intended for "businesses which serve the requirements of the community at large including Genoa Township, Howell, Brighton, and pass-by traffic along Grand River Avenue."

Based on the nature of the proposed use, we are of the opinion that it is compatible with the General Commercial future land use category.

2. Compatibility. The Grand River corridor includes a variety of commercial and service uses. While there are residential uses to the north, the building provides an ample rear yard setback (150+ feet) and the rear of the site is heavily wooded.

Additionally, there are no outdoor activities proposed between the building and residential zoning/uses to the north. The only outdoor impacts will be a small animal walk area adjacent to the building and parking lot.

Provided the use requirements are met, we anticipate the proposed use to be compatible with the existing and planned character of the area.

- **3. Public Facilities and Services.** As a developed property on Grand River, we expect that necessary public facilities and services are in place. However, we defer to the Township Engineer and Brighton Area Fire Authority for any technical comments under this criterion.
- **4. Impacts.** The use requirements of Section 7.02.02(x) are intended to limit impacts of the proposal upon the site and surrounding properties. Provided these requirements are met, the proposal is not expected to adversely impact adjacent or surrounding properties and/or uses.

Genoa Township Planning Commission **Integrative Veterinary Services** Special Land Use and Sketch Plan Review #1 Page 3

5. Mitigation. If any additional concerns arise as part of the review process, the Township may require additional efforts/improvements to mitigate potential adverse impacts.

D. Use Requirements

As previously noted, veterinary clinics are subject to the use requirements of Section 7.02.02(x), as follows:

1. A site plan shall be provided as part of the Land Use Permit application showing a dedicated outdoor animal area. Outdoor animal areas shall consist of properly maintained lawn, special canine grass or other methods with an appropriate drainage system to control surface run-off. The outdoor area surface shall be approved by the Planning Commission following a recommendation by the Township Engineer. The outdoor area must be maintained in a clean, sanitary manner, and adequate odor control measures shall be implemented so that odor will not be discernible beyond the area. Solid pet waste in the outdoor area must be promptly picked up.

The submittal includes the approved site plan from 1998. While this plan does not identify the outdoor animal area, the supplemental material included with the submittal provides photographs, notes and a written description for the required area on the west side of the building.

This area is comprised of an existing lawn and the applicant will provide signage, a bag dispenser and a container for any waste. The submittal notes that this area will be inspected daily with any waste cleaned up and disposed of by staff.

2. Applicants shall submit, at the time of land use application, a proposed site plan and floor plan and written operating procedures including waste and noise management methods. The waste management plan shall detail both indoor and outdoor waste management procedures to ensure animal waste is not discharged to surface or storm water. These procedures shall be followed for the duration of the business and shall be designed to prevent or control animal behavior that may adversely impact surrounding uses, including loud or excessive barking.

The submittal includes a floor plan of the building, as well as a detailed management plan for both pet (indoors and outdoors) and medical waste.

Based on the use description provided, the nature of the business is such that noise generation is not expected to create any offsite impacts.

E. Sketch Plan Review

As previously noted, the request does not include any exterior site improvements.

Based on the materials provided, as well as review of aerial photos and discussion with Township staff, the site has generally been well maintained and existing conditions appear to comply with current standards.

Of note, there is more than adequate parking and the quantity of plantings appears to exceed Ordinance requirements.

Our only question is whether the existing parking lot light poles provide compliant fixtures. The Township may wish to request details of existing fixtures. If they do not meet current Ordinance standards, improvements could be required as part of this review process.

Lastly, the applicant should be aware that a sign permit is required from the Township prior to installation of any new signage.

Genoa Township Planning Commission **Integrative Veterinary Services** Special Land Use and Sketch Plan Review #1 Page 4

Should you have any questions concerning this matter, please do not hesitate to contact our office.

Respectfully,

SAFEBUILT STUDIO

Brian V. Borden, AICP Planning Manager



March 3, 2020

Ms. Kelly Van Marter Genoa Township 2911 Dorr Road Brighton, MI 48116

Re: Integrative Veterinarian Special Use Sketch Plan Review No. 1

Dear Ms. Van Marter:

Tetra Tech conducted a review of the Integrative Veterinarian Special Use application. The application, last dated January 29, 2020, was prepared by Matthew Januszewski. The application proposes using an existing 2,088 square foot office building as a veterinarian office. The property is located on the north side of Grand River Avenue at 2829 Grand River Avenue. We offer the following comments:

GENERAL

1. The Petitioner is not proposing any exterior improvements to the property. Using the parking requirements in the Genoa Township Zoning Standards, converting the use from an office building to a veterinarian office reduces the amount of required parking spaces.

UTILITIES

1. Converting the use of the building from an office building to a veterinarian office increases the anticipated Residential Equivalent Units (REUs) of the site according to the Genoa Township REU Table. The special use application notes that the proposed veterinarian office will experience a lower use than normal veterinarian offices, so the anticipated REUs may be similar to the previous use of the building.

From an engineering viewpoint we have no objections to the proposed special use application as the proposed use will have a reduced impact from the previous use and there are no exterior improvements proposed.

Please call or email if you have any questions.

Sincerely,

Gary J. Markstrom, P.E.

Vice President

Shelby Scherdt
Shelby Scherdt
Project Engineer

BRIGHTON DE P

BRIGHTON AREA FIRE AUTHORITY

615 W. Grand River Ave. Brighton, MI 48116 o: 810-229-6640 f: 810-229-1619

February 26, 2020

Kelly VanMarter Genoa Township 2911 Dorr Road Brighton, MI 48116

RE: Integrative Veterinary Special Use

2829 E. Grand River Avenue

Howell, MI

Dear Kelly:

The Brighton Area Fire Department has reviewed the above-mentioned site plan. The plans were received for review on February 13, 2020, and the submittal is dated January 30, 2020. The project is based on an existing 1.43-acre parcel, with a 2,292 square foot Business-use office building that will undergo a change of occupant and zoning to a veterinary clinic. The plan review is based on the requirements of the International Fire Code (IFC) 2018 edition.

The site location is an existing non-conformance of the currently adopted fire code. There is no increase in the hazard with this change of occupant from a fire safety standpoint, therefore water supply and access are acceptable to the fire authority.

1. The building shall include the building address on the building. The address shall be a **minimum of 6**" high letters of contrasting colors and be clearly visible from the street. The location and size shall be verified prior to installation.

IFC 505.1

2. A key box (Knox Box) shall be provided and located adjacent to the front door of the structure.

IFC 506.1

Additional comments will be given during the building plan review process (specific to the building plans and occupancy). The applicant is reminded that the fire authority must review the fire protection systems submittals (sprinkler & alarm) prior to permit issuance by the Building Department and that the authority will also review the building plans for life safety requirements in conjunction with the Building Department. If you have any questions about the comments on this plan review please contact me at 810-229-6640.

Cordially,

Rick Boisvert, CFPS Fire Marshal

IMPACT ASSESSMENT/IMPACT STATEMENT

for

INTEGRATIVE VETERINARY SERVICES OF SOUTHEAST MICHIGAN

at

2829 E. Grand River Avenue, Howell MI 48843

Applicant:

Dr. Matthew Januszewski 5047 Queensway Howell, MI 48843 517-304-7224

Prepared by:

Dr. Matthew Januszewski

In conjunction with property information provided by:

Advantage Civil Engineering, Inc.

110 E. Grand River Ave.

Howell, MI 48843

January 30, 2020

Introduction

This impact assessment has been prepared pursuant to Article 18-Site Plan Review and Impact Assessment of Special Land Use for the Township of Genoa, Livingston County, Michigan. The assessment addresses the impact of the proposed change of use for 2829 E. Grand River Avenue, Howell, MI. The existing 2,088 square foot commercial building has been used DisplayMax, providing instore retail services since 2011, to Integrative Veterinary Services of Southeast Michigan, a specialty, holistic veterinary practice.

a. **Preparer.** Name(s) and address(es) of person(s) responsible for preparation:

Dr. Matthew Januszewski, 5047 Queensway, Howell, MI 48843. Owner of Integrative Veterinary Services of Southeast Michigan. This impact assessment has been prepared in conjunction with property information provided by Advantage Civil Engineering, 110 E. Grand River Ave., Howell, MI 48843 from the impact assessment for Great Lakes Software at 2829 East Grand River Ave., Howell, MI 48843 on January 28, 1998.

- b. **Location.** Map(s)and a written description/analysis of the project site:
 - -The existing building is located at 2829 Grand River Avenue, Howell, MI 48843 is proposed to become the office for Integrative Veterinary Services of Southeast Michigan. The building is 2,088 square feet and the lot is 1.43 acres. The area surrounding the building consists of a parking lot and landscaped areas: There is a small storage shed on the west side of the building, used for storage of outdoor tools and a flag pole south of the building.
 - The building sits on top of a hill with limited visibility from Grand River Avenue. The 1.43-acre site has several large trees which also block visibility from Grand River Avenue. The site is relatively flat for about 120' from the existing building to the road, then there is a modest slope for 40', then flat to the right of way line. The soils are predominately loams. The site is mostly lawn with an area of brush in the front and an area of woods and brush in the rear of the property. There is landscaping around the building, including red maples, green ashes, arborvitaes and a row of spruces along the east and west property line.
 - -Neighboring businesses include Advance Auto Parts to the west and Cruz'n Quick Lube to the east. Bob Maxey Ford, the Elks Lodge, and Uncle Joe's Used Cars are across Grand River Avenue. There is a small residential area north of the site which is separated by the large back yard, a tree line and brush.
 - -An aerial photograph is attached for your review.

c. Impact on natural features:

-This is not applicable as the proposed project is for use of existing building with no exterior changes

d. Impact on stormwater management.

-In accordance with Section 7.02.02(x), I have provided a site plan and photographs of the proposed dedicated outdoor animal walk area. This area is a maintained grassed area. There will be a small sign indicating the dedicated animal walk area, as well as an outdoor poop bag dispenser and a metal collection container with a lid. The feces will be promptly picked up and the collection container will be emptied daily. Fecal waste collected inside and outside the facility will be disposed of in the sewer system (by flushing down the toilet) or secured in a closed bag and deposited in the trash. The trash will then be disposed of in a waste container outside the building, which will be emptied weekly by a waste collector. These arrangements will be made with a waste collection service contracted by Integrative Veterinary Services. The city sewer and water systems providing services to the building are in a closed system preventing risk of exposure to animal waste in the proposed outdoor animal walk area. Additionally, as a result of my proposed business plan detailing no grooming, boarding, or overnight stays of animals, animal waste should be kept at a minimum.

-The storm drains for this property are on the other side of the property, all the way towards the front of the property (near Grand River). The proposed dog walk area is on the west side of the building, the storm drains are on the east side, south of the building. A photograph of the storm drain location in relation to the proposed animal walk area is included.

e. Impact on surrounding land uses.

-The existing building is used as an office, which is compatible with our business plan. Our typical hours of operation are: Monday and Tuesday: 9am to 6pm; Wednesday and Thursday: 9am to 5pm; Friday 9am to 4pm; Saturday (one Saturday per month only): 9am-1pm. Interior lighting will follow these hours as will exterior lighting with the addition of an hour before and an hour after for employees entering and exiting before and after normal business hours.
-Exterior Lighting: A site plan detailing the existing landscaping and the site lighting detail as well as a photograph of the parking lot lighting is attached for your review. There are no changes in the site lighting proposed with this use.

- -Signs: Approval and permit from Genoa Township will be obtained prior to installation of signage. Sign regulations as outlined in Article 16 of the Township Zoning Ordinance will be followed.
- -The landscape will not be affected by the interior upgrades to the existing building. Consequently, Integrative Veterinary Services will make no alterations to the existing landscaping.
- -The proposed business is consistent with the development of adjacent properties. Growth along the Grand River Avenue corridor has consisted of a mixture of medical, office, and commercial uses.
- -Potential for air pollution in the form of fecal odor and waste nuisance can be managed: Dedicated Outdoor Animal Walk Area: Located on the West side of the building (see photo). There will be a sign indicated the animal walk area as well as an outdoor poop bag dispenser with a steel collection container with a lid (see photo). The container will be emptied daily and the property will be inspected daily for any feces not picked up by owners. The fecal waste will be secured in a closed bag, deposited into a larger trash bag and then deposited into the trash. The trash will then be disposed of in a waste

container outside the building which will be emptied weekly by a waste collection service. The city water and sewer systems providing services to the building are in a closed system preventing risk of exposure to animal waste in the proposed outdoor animal walk area. Additionally, as a result of our proposed business plan detailing no grooming, boarding or overnight stays of animals, animal waste will be kept to a minimum. We have used this system at our current location (with a MUCH smaller yard area) in the City of Howell for eight years now. We have never had an issue with odor, dead grass or messes.

Any indoor feces will be immediately tied up in a plastic bag and then placed into another plastic garbage bag OR flushed down the toilet followed by immediate clean-up and disinfection of the affected area.

-Noise will not have an impact on surrounding properties:

Indoor noise will not be an issue since our patients rarely become agitated as they are with their owner, on leashes and rarely encounter other patients. Our patients are not held in cages or kennels at any time. They are escorted from the reception area into a small, private exam room accompanied by their owner. We play calming music throughout the building as well as diffuse calming oils to create a serene environment. We also use pet treats to reward our patients. If there is any indoor animal barking, the geographical isolation and large size of the lot would prevent anyone from being able to hear the noise.

Outdoor barking is also a rarity since dogs are coming from their car on a leash accompanied by their owner. Due to the small volume of clients that we see per hour, encountering another dog is rare. Again, our geographically isolated building (hardly visible from Grand River), large lot size and trees/shrubs on property lines creates a natural noise barrier.

f. Impact on public facilities and services.

- The anticipated number of employees will be 6-7. There will be 1 full- time employee and 5-6 part-time employees with 4 to 5 people working daily. We schedule 1-3 clients per hour at separate intervals of time so there will only be a few client cars coming and going from the lot at a time. We do not have walk-ins nor emergency appointments since we are a specialty-only practice. This is a negligible amount; therefore, there will be no noticeable impact on public facilities and services

There are no anticipated impacts on public schools nor an increased need for police and fire protection.

g. Impact on public utilities.

-- The property is presently supplied by municipal sewer and water systems. There will be minimal impact on public utilities from the proposed business.

h. Storage and handling of any hazardous materials.

-This is not applicable as there is none.

i. Traffic Impact Study.

- -The business is located in the site on Grand River Avenue, west of Char-Ann Drive. Grand River Avenue at this location is a four-lane road with a left turn center lane. Traffic counts conducted by the Livingston County Road Commission for this area of Grand River Avenue indicate a 24-hour traffic count of approximately 19,900.
- -Traffic generated from this business in this site based on the proposed use is estimated to be 18-27 trips per day. The traffic generated from this business will slightly increase the local traffic on Grand River Avenue. However, the existing level of service will not be significantly impacted by the proposed project.

j. Historic and Cultural Resources.

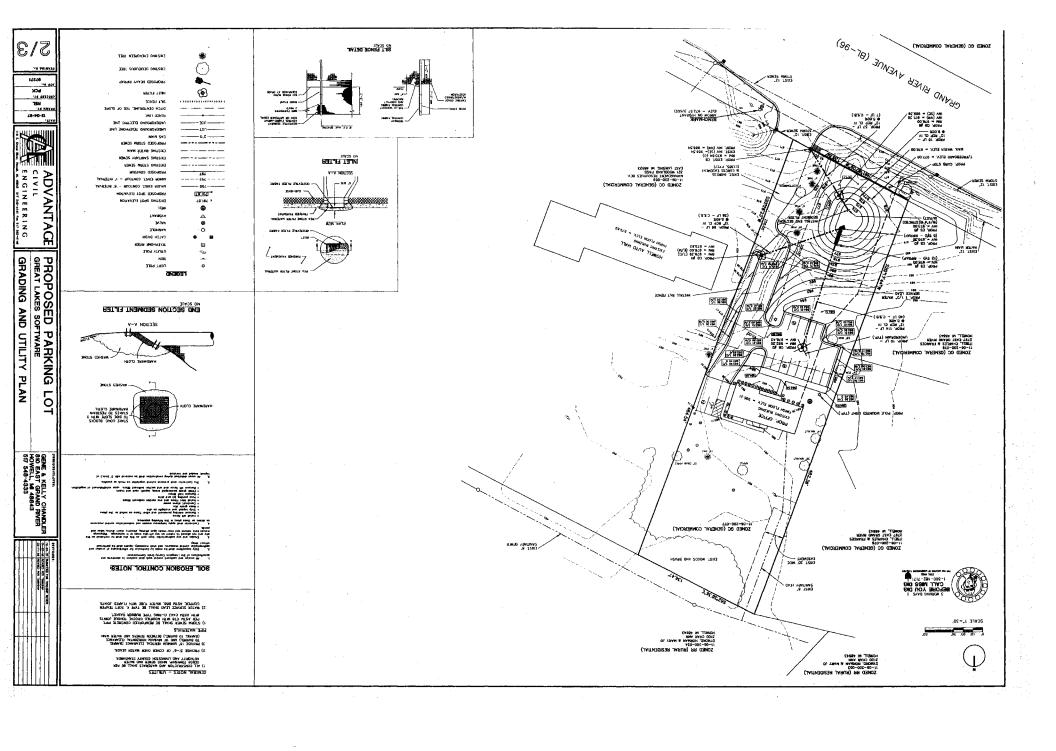
-This is not applicable as the proposal does not involve the alteration or demolition of a structure.

k. Special Provisions.

-There is an existing ingress and egress easement in the front.

I. A list of all sources shall be provided:

- Livingston County Road Commission. Transportation Data Management System. 2015 data. http://livingston.ms2soft.com/tcds/tsearch.asp?loc=Livingston&mod=.



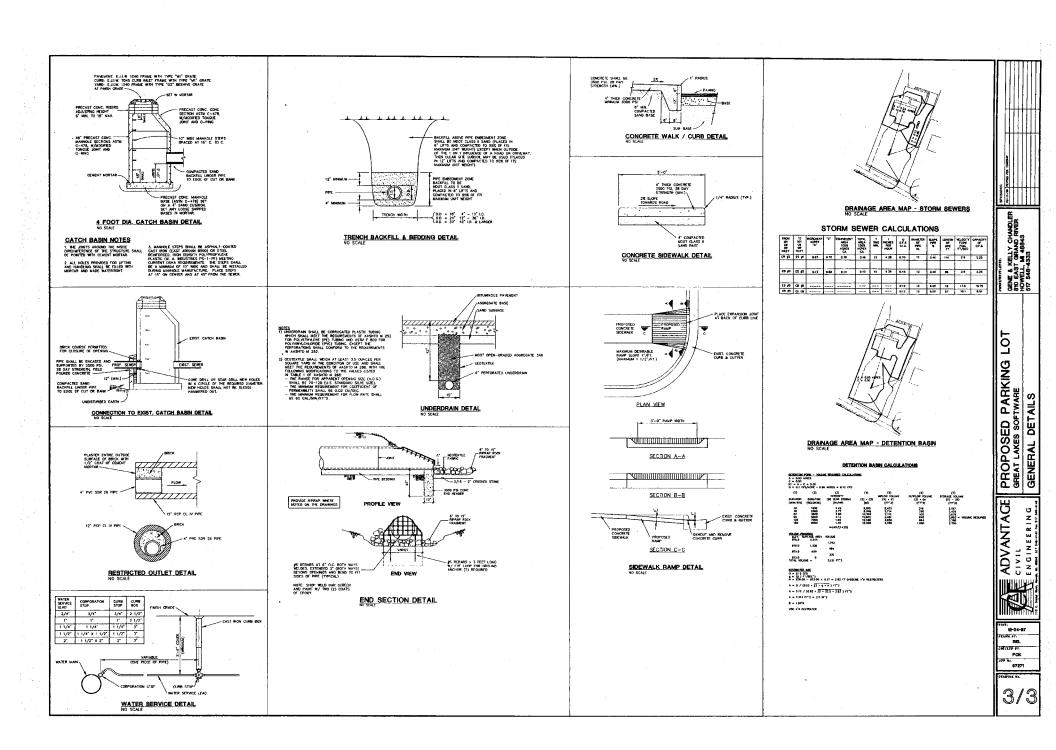




Photo Addendum

Subject Property Address: 2829 E Grand River Ave







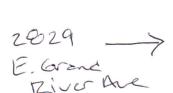






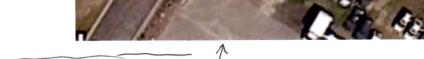


home





Distance to residential area showing tree line



Grand River Ave N





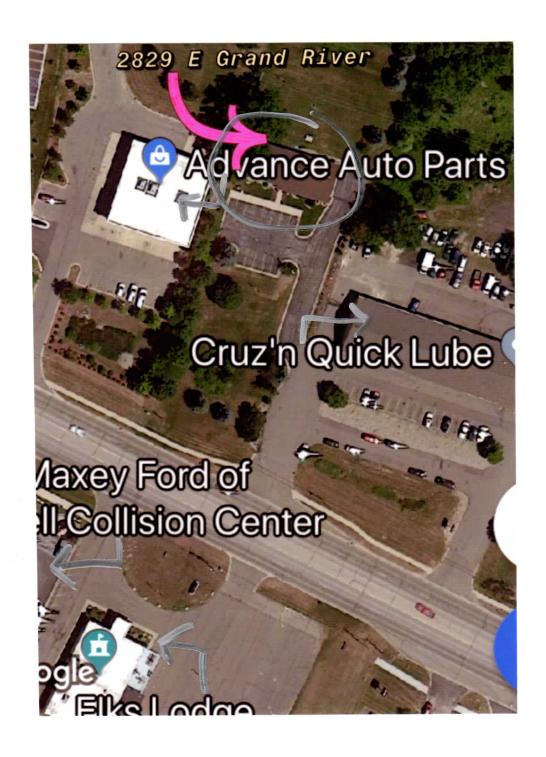
Dog elimination area. West of building, off of the parking lot.



View of Grand River from dog walking area

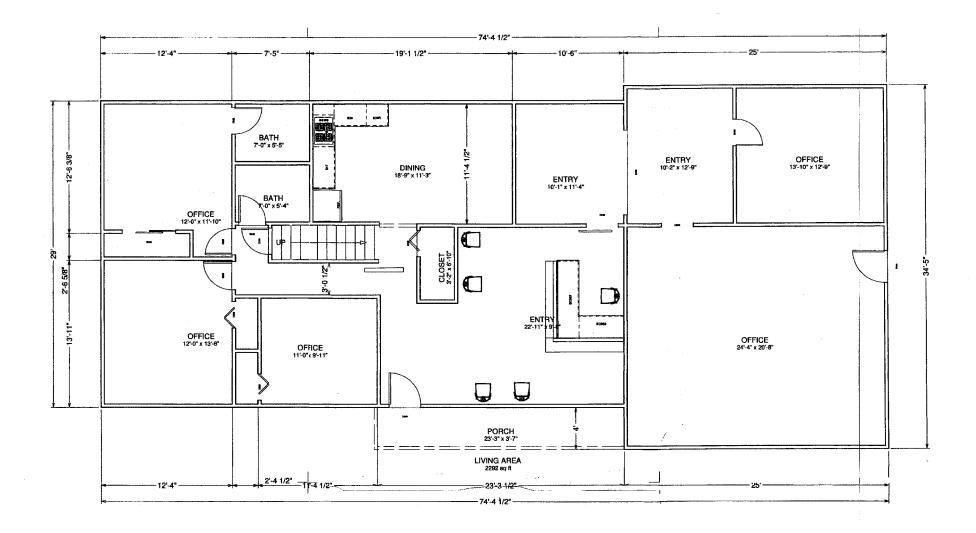


Neighboring Businesses



24

Frank er Avenue





GENOA CHARTER TOWNSHIP APPLICATION Planned Unit Development (PUD)

APPLICANT NAME: Trinity Health - Michigan d/b/a St. Joseph Mercy Livingston
APPLICANT EMAIL: Rebecca.selter@stjoeshealth.org
APPLICANT ADDRESS & PHONE: 620 Byron Road, Howell, MI 48843 (517) 545-6864
OWNER'S NAME: Trinity Health - Michigan d/b/a St. Joseph Mercy Livingston
OWNER ADDRESS & PHONE: 620 Byron Road, Howell, MI 48843 (517) 545-6864
TAX CODE(S): Parcel1: 11-09-100-015 Parcel2: 11-09-100-020
QUALIFYING CONDITIONS (To be filled out by applicant)
1. A PUD zoning classification may be initiated only by a petition.
2. It is desired and requested that the foregoing property be rezoned to the following type of PUD designation:
Residential Planned Unit Development (RPUD) Planned Industrial District (PID) Mixed Use Planned Unit Development (MUPUD) Redevelopment Planned Unit Development (RDPUD) Non-residential Planned Unit Development (NRPUD) Town Center Planned Unit Development (TCPUD)
3. The planned unit development site shall be under the control of one owner or group of owners and shall be capable of being planned and developed as one integral unit.
EXPLAIN Trinity Health - Michigan d/b/a St. Joseph Mercy Livingston (THSJM) and the current
owners of the parcels have signed a Purchase Agreement for Parcel 1 and Parcel 2. See Attachments
2 and 3 for Letters of Authorization for approvals from the current owners to submit Rezoning, PUD, and
Site Plan Applications. See Attachments 4,5 for memos outlining critical dates for purchase of the parcels.

- 4. The site shall have a minimum area of twenty (20) acres of contiguous land, provided such minimum may be reduced by the Township Board as follows:
 - A. The minimum area requirement may be reduced to five (5) acres for sites served by both public water and public sewer.
 - B. The minimum lot area may be waived for sites zoned for commercial use (NSD, GCD or RCD) where the site is occupied by a nonconforming commercial, office or industrial building, all buildings on such site are proposed to be removed and a new use permitted within the underlying zoning district is to be established. The Township Board shall only permit the PUD on the smaller site where it finds that the flexibility in dimensional standards is necessary to allow for innovative design in redeveloping the site and an existing blighted situation will be eliminated. A parallel plan shall be provided showing how the site could be redeveloped without the use of the PUD to allow the Planning Commission to evaluate whether the modifications to dimensional standards are the

minimum necessary to allow redevelopment of the site, while still meeting the spirit and intent of the ordinance.

- C. The PUD site plan shall provide one or more of the following benefits not possible under the standards of another zoning district, as determined by the Planning Commission:
 - preservation of significant natural or historic features
 - a complementary mixture of uses or a variety of housing types
 - common open space for passive or active recreational use
 - mitigation to offset impacts
 - redevelopment of a nonconforming site where creative design can address unique site constraints.
- D. The site shall be served by public sewer and water. The Township may approve a residential PUD that is not served by public sewer or water, provided all lots shall be at least one (1) acre in area and the requirements of the County Health Department shall be met.

Size of property is 23.86 acres.
DESCRIBE BELOW HOW THE REQUESTED PUD DESIGNATION COMPLIES WITH AFOREMENTIONED MINIMUM LOT SIZE REQUIREMENTS.
The proposed development includes two parcels and meets the 20 acre minimum requirement:
Parcel 1 is 21.86 acres and Parcel 2 is 2 (two) acres, for a total of 23.86 acres.
STANDARDS FOR REZONING TO PLANNED UNIT DEVELOPMENT (RESPOND HERE OR WITHIN THE IMPACT STATEMENT)
 How would the PUD be consistent with the goals, policies and future land use map of the Genoa Township Master Plan, including any subarea or corridor studies. If conditions have changed since the Master Plan was adopted, the consistency with recent development trends in the area;
See Attachment 1, Impact Statement section(s): Impact on Genoa Township Master Plan
2. The compatibility of all the potential uses in the PUD with surrounding uses and zoning in terms of land suitability, impacts on the environment, density, nature of use, traffic impacts, aesthetics, infrastructure and potential influence on property values;
See Attachment 1, Impact Statement section(s): Impact on surrounding land used,
Impact on on natural features, Impact on storm-water management.
See Attachment 9: Traffic Study.
3. The capacity of infrastructure and services sufficient to accommodate the uses permitted in the requested district without compromising the "health, safety and welfare" of the Township;
See Attachment 1, Impact Statement section(s): Impact on public facilities and services,
Impact on public utilities.

 See Attachment 1. Impact Statement eaction(e): Demand for the proposed developme. 	
See Attachment 1, Impact Statement section(s): Demand for the proposed developme	nt.
AFFIDAVIT	
The undersigned says that they are the OWNEZ (owner, lessee, or other specified interest) involved in this petition and that the foregoing answers and statements herein contained and the informat herewith submitted are in all respects true and correct to the best of his/her knowledge and belief. BY: JOHN F. O'MALLEY ADDRESS: 620 BYIZON Rd. Howell, Michigan 48843	ion
ADDRESS: 620 BYKON Rd. Howell, Michigan 48843	-
Contact Information - Review Letters and Correspondence shall be forwarded to the following:	
Rebecca Selter, AIA of SJMHS Planning Design & Construction at Rebecca selter@stjoeshealth.org Name Business Affiliation E-mail	
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FEE EXCEEDANCE AGREEMENT	
FEE EXCEEDANCE AGREEMENT As stated on the site plan review fee schedule, all site plans are allocated two (2) consultant reviews and one	(1)
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GENOA CHARTER TOWNSHIP Application for Re-Zoning

Trinity Health – Michigan d/b/a
PPLICANT NAME: St. Joseph Mercy Livingston ADDRESS: 620 Byron Road, Howell, MI 48843
WNER NAME: Trinity Health - Michigan d/b/a St. Joseph Mercy Livingston ADDRESS: 620 Byron Road, Howell, MI 48843
ARCEL #(s): Parcel1: 11-09-100-015 Parcel2: 11-09-100-020 PRIMARY PHONE: (517) 545-6864
MAIL 1: Rebecca.selter@stjoeshealth.org EMAIL 2: brandon.woodle@smithgroup.com
e, the undersigned, do hereby respectfully make application to and petition the Township Board to the township Zoning Ordinance and change the zoning map of the township of Genoa as reinafter requested, and in support of this application, the following facts are shown:
REQUIRED SUBMITTAL INFORMATION
 A legal description and street address of the subject property, together with a map identifying the subject property in relation to surrounding properties; The name, signature and address of the owner of the subject property, a statement of the applicant's interest in the subject property if not the owner in fee simple title, and proof of consent from the property owner; It is desired and requested that the foregoing property be rezoned from:
Parcel 1: PRF, Parcel 2: Industrial to Non-Residential Planned Unit Development.
 A site plan illustrating existing conditions on the site and adjacent properties; such as woodlands, wetlands, soil conditions, steep slope, drainage patterns, views, existing buildings, sight distance limitations, relationship to other developed sites. and access points in the vicinity; A conceptual plan demonstrating that the site could be developed with representative uses permitted in the requested zoning district meeting requirements for setbacks, wetland buffers access spacing, any requested service drives and other site design factors; A written environmental impact assessment, a map of existing site features as described in Article 18 describing site features and anticipated impacts created by the host of uses permitted in the requested zoning district; A written description of how the requested rezoning meets Sec. 22.04 "Criteria for Amendment of the Official Zoning Map." The property in question shall be staked prior to the Planning Commission Public Hearing.
DESCRIBE HOW YOUR REQUESTED RE-ZONING MEETS THE ZONING ORDINANCE CRITERIA FOR AMENDING THE OFFICIAL ZONING MAP:
1. How is the rezoning consistent with the goals, policies and future land use map of the Genoa Township Master Plan, including any subareas or corridor studies. If not consistent, describe how conditions have changed since the Master Plan was adopted?
See Attachment 1, Impact Statement section(s): Impact on Genoa Township Master Plan.
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2. Are the site's physical, geological, hydrological and other environmental features suitable for the host of uses permitted in the proposed zoning district?
See Attachment 1, Impact Statement section(s): Impact on natural features,
Impact on storm-water management.
 Do you have any evidence that a reasonable return on investment cannot be received by developing the property with one (1) of the uses permitted under the current zoning? No evidence.
4. How would all the potential uses allowed in the proposed zoning district be compatible with surrounding uses and zoning in terms of views, noise, air quality, the environment, density, traffic impacts, drainage and potential influence on property values?
See Attachment 1, Impact Statement section(s): Impact on surrounding land used
and Impact on stormwater management.
5. Are infrastructure capacity (streets, sanitary sewer, water, and drainage) and services (police and
fire protection, etc.) sufficient to accommodate the uses permitted in the requested district? See Attachment 1, Impact Statement section: Impact on public facilities and
services. See Attachment 9: Traffic Study.
6. Is there a demonstrated demand in Genoa Township or the surrounding area for the types of use permitted in the requested zoning district? If yes, explain how this site is better suited for the zoning than others which may be planned or zoned to accommodate the demand.
See Attachment 1, Impact Statement section: Demand for the proposed development.
7. If you have a particular use in mind, is another zoning district more appropriate? Why should the Township re-zone the land rather than amend the list of uses allowed in another zoning district t accommodate your intended use?
The proposed re-zoning is consistent with the Genoa Township Master Plan, per the
recommendation on page 5-6 that says: "Any development with another, more intense
quasi-public land use such as a medical center or hospital, should be designed as a PLID"

8. Describe any deed restrictions which could potentially affect the use of the property. None.
C. AFFIDAVIT
The undersigned says that they are the <u>Owver</u> (owner, lessee, or other specified interest) involved in this petition and that the foregoing answers and statements herein contained and the information herewith submitted are in all respects true and correct to the best of his/her knowledge and belief.
ADDRESS: 620 BYROW Rd. Howell Michigan 48843
ADDRESS: 620 BYROW Rd. Howell Michigan 48843
SIGNATURE TO SIGNATURE
The following contact should also receive review letters and correspondence:
Name: Brandon Woodle Email: brandon.woodle@smithgroup.com
Business Affiliation: Owner's Consultant, Landscape Architect
FEE EXCEEDANCE AGREEMENT
As stated on the site plan review fee schedule, all site plans are allocated two (2) consultant reviews and one (1) Planning Commission meeting. If additional reviews or meetings are necessary, the applicant will be required to pay the actual incurred costs for the additional reviews. If applicable, additional review fee payment will be required concurrent with submittal to the Township Board. By signing below, applicant indicates agreement and full understanding of this policy.
PROJECT NAME: SJML New Hospital and Medical Office Building
PROJECT LOCATON & DESCRIPTION: Parcel 1: 4444 E. Grand River Ave., Howell, MI
Parcel 2: Vacant Parkway Dr., Howell, MI
SIGNATURE: DATE: 1/27/2020
PRINT NAME: JOHN F. O'MATTEY PHONE: 517-545-6864
PRINT NAME: JOHN F. O'MAHEY PHONE: 517-545-6864 ST JOSEPH MERCY LIVINGSTON HOSPITAL COMPANY NAME & ADDRESS: 620 BYRON Rd. Howell Michigan 118843



GENOA CHARTER TOWNSHIP Application for Site Plan Review

TO THE GENOA TOWNSHIP PLANNING COMMISSION AND TOWNSHIP BOARD: Trinity Health – Michigan d/b/a St. Joseph Mercy Livingston
APPLICANT NAME & ADDRESS: 620 Byron Road, Howell, MI 48843
If applicant is not the owner, a letter of Authorization from Property Owner is needed. Trinity Health – Michigan d/b/a St. Joseph Mercy Livingston
OWNER'S NAME & ADDRESS: 620 Byron Road, Howell, MI 48843
Parcel 1: 4444 E. Grand River Ave., Howell, MI Parcel 1: 11-09-100-01
SITE ADDRESS: Parcel 2: Vacant Parkway Dr., Howell, MI PARCEL #(s): Parcel2: 11-09-100-020
APPLICANT PHONE: (517) 545-6864 OWNER PHONE: (517) 545-6864
OWNER EMAIL: Rebecca.selter@stjoeshealth.org
LOCATION AND BRIEF DESCRIPTION OF SITE:
The project site consists of 2 parcels, with the larger, 21.86-acre parcel fronting on the south side of Grand River
Avenue and west side of Parkway Drive. It is currently occupied by the Grand River Golf Range and Putt-Putt, with a
small building, parking lot and putt-putt course located on the north side and the golf range occupying the remainder of the site. The smaller, 2-acre parcel fronting on the east side of Parkway Drive is vacant land, with a 20' sanitary
sewer easement, a 12' DTE easement and a variable width storm easement currently being used for detention along
the east and north sides.
BRIEF STATEMENT OF PROPOSED USE:
This project proposes development of a new hospital and Medical Office Building. The hospital will
replace the current facility in Howell, MI with expanded capacity to serve the growing population of
Genoa Township. The proposed project includes development of two parcels, with a majority of the
infrastructure built on Parcel 1, the larger parcel, and for Parcel 2 to be used for surface stormwater
detention.
THE FOLLOWING BUILDINGS ARE PROPOSED:
This project proposes one building containing a hospital and a Medical Office Building with
approximately 245,000 GSF. The two uses (hospital and MOB) are separated into two volumes with a
central connection containing common space. The hospital portion of the building will be 4 stories,
approximately 200,000 GSF, and contain 72 beds. The MOB will be 2 stories and occupy approximately 45,000 GSF of the building.
I HEREBY CERTIFY THAT ALL INFORMATION AND DATA ATTACHED TO AND MADE PART OF THIS APPLICATION IS TRUE AND ACCURATE TO THE BEST OF MY KNOWLEDGE AND BELIEF. BY: John F. O'Malley
ADDRESS: 620 Byron Rd. Howell MI, 48843

Contact Information - Review Letter 1.) Brandon Woodle, PLA of Name	s and Correspondence shall be forwarded to the Owner's consultant, Landscape Architect at Business Affiliation	brandon.woodle@
מות ותו	EXCEEDANCE AGREEMENT	
As stated on the site plan review fee so one (1) Planning Commission meeting will be required to pay the actual incur	chedule, all site plans are allocated two (2) con . If additional reviews or meetings are necessared costs for the additional reviews. If applicant with submittal to the Township Board. By s	ary, the applicant ble, additional review
PRINT NAME: JOHN F. ON	DATE: 1/27/20 ALICY PHONE: 517-543	020 5-6864



Planning Commission Genoa Township 2911 Dorr Road Brighton, Michigan 48116

Attention:	Kelly Van Marter, AICP	
	Planning Director and Assistant Township Manager	
Subject:	St. Joseph Mercy Livingston – PUD Rezoning and Conceptual Plan Review #2	
Location:	South side of Grand River Avenue, east of Latson Road	
Zoning:	PRF Public and Recreation Facilities District and IND Industrial District	

Dear Commissioners:

At the Township's request, we have reviewed the revised submittal from St. Joseph Mercy requesting PUD rezoning and conceptual plan review for approximately 23.86 acres of land. The subject site is currently comprised of two parcels – a 21.86-acre parcel that is currently occupied by a driving range with access to/from Grand River and an undeveloped 2-acre parcel along Parkway Drive. The latter is intended to accommodate the stormwater detention for development of the former.

A. Summary

1. PUD Qualifying Conditions:

- a. In general, the standards of Section 10.02 are met.
- b. The applicant must address any technical comments provided regarding sewer and water.

2. Rezoning Criteria:

- a. In general, the standards of Section 22.04 are met.
- b. The inclusion of outdoor storage and a helipad may impact the surrounding area. We request the applicant provide additional details to ensure compatibility.
- c. The applicant must address any technical comments provided regarding the capacity of Township services and infrastructure.

3. Conceptual PUD Plan:

- a. Uses: hospitals, outdoor storage, accessory fuel storage and heliports typically require special land use review/approval; however, the applicant seeks to have each allowed as a by-right use. These deviations are subject to Township approval, though the applicant must comply with the conditions applicable to each of these uses (whether special land use review is waived or not).
- b. Traffic: the traffic impact study includes recommended improvements necessary to accommodate the proposal. We defer technical review of traffic circulation, including the traffic impact study, to the Township Engineer.
- c. Site design: additional details are needed with respect to site lighting, a Township entranceway feature, and project signage. Additional landscaping around the stormwater detention areas and inclusion of site amenities (such as bicycle racks) are also required. The applicant notes that full details will be provided with a final PUD plan submittal.
- d. Architecture: the proposed building does not provide peaked rooflines and the predominant material is not brick, per Ordinance standards. Deviations from the material standards would be needed, as proposed.
- e. Utilities: utility plans are subject to technical review by the Township Engineer and/or Utilities Director.

4. PUD Agreement:

- a. Dimensional deviations are sought for building height, rooftop equipment and loading in the Parkway Drive front yard.
- b. Reference to vehicular connections with Parkway Drive in Section 3.2 should be removed.
- c. Some of the signage details in Section 5.2 need to be completed.

248.586.0505 www.safebuilt.com



Aerial view of site and surroundings (looking north)

B. Proposal/Process

The request is to establish a Non-Residential Planned Unit Development (NRPUD) for a large medical complex that includes a hospital, emergency center and medical office building.

Procedurally, following the required public hearing, the Planning Commission is to provide recommendations to the Township Board on the PUD rezoning, conceptual PUD plan, Environmental Impact Statement and draft PUD Agreement.

C. Qualifying Conditions

The proposal has been reviewed for compliance with the PUD Qualifying Conditions (Section 10.02), as follows:

- **1. Single Ownership.** The application forms included with the submittal identify the owner and applicant as the same party Trinity Health doing business as St. Joseph Mercy Livingston.
- **2. Initiated by Petition.** The request has been properly initiated via applications for rezoning, PUD and site plan review.
- **3. Minimum Site Area.** The total land area is identified as 23.86 acres. As previously noted, the main development parcel contains 21.86 acres, while the separate 2-acre parcel is needed for stormwater management.
- **4. Benefits.** Benefits of the proposal include a complementary mix of medical uses, as well as improvements to the site's access to/from Grand Avenue via a new traffic signal and re-striping to accommodate/mitigate the traffic impacts anticipated by the proposal.
- **5. Sewer and Water.** The property has access to public sewer and water, though we defer to the Township Engineer and/or Utilities Director for technical comments under this criterion.

D. Rezoning Criteria

The proposal has been reviewed for compliance with the Criteria for Amendment of the Official Zoning Map (Section 22.04), as follows:

1. Consistency with the goals, policies and future land use map of the Genoa Township Master Plan, including any subarea or corridor studies. If conditions have changed since the Master Plan was adopted, the consistency with recent development trends in the area.

The Township Master Plan and Future Land Use map identify the main parcel as General Commercial and the smaller parcel as Research and Development.

Establishment of a NRPUD is generally consistent with these classifications. Additionally, the Plan addresses redevelopment of private recreation sites, as follows:

Any development with another, more intense quasi-public land use, such as a medical center or hospital, should be designed as a PUD to properly integrate the development within the community. Such facilities should be designed to provide a campus type design, with coordinated access and circulation, consistent building design and preserved open space and natural features.

As such, we are of the opinion that NRPUD rezoning for a large medical complex on this site is consistent with the Township Master Plan.

2. Compatibility of the site's physical, geological, hydrological and other environmental features with the host of uses permitted in the proposed zoning district.

In general, the subject site does not contain sensitive environmental features that will impact future development of the site. The majority of the property was previously developed with an outdoor recreational use.

The Impact Assessment does identify a small wetland area on parcel 2 that was created as part of a stormwater management plan. Per communication from the Department of Environment, Great Lakes and Energy (EGLE), the additional stormwater improvements proposed do not require a permit.

Given this information, it does not appear that the Township's wetland protection regulations (Section 13.02) will apply to the stormwater management plan; however, this aspect of the project is subject to technical review by the Township Engineer.

3. The ability of the site to be reasonably developed with one (1) of the uses permitted under the current zoning.

As referenced above, the Master Plan anticipates redevelopment of private recreation sites and specifically identifies potential redevelopment of a hospital complex under the PUD option. As such, we do not believe another PRF use would be more reasonable or appropriate.

4. The compatibility of all the potential uses allowed in the proposed zoning district with surrounding uses and zoning in terms of land suitability, impacts on the environment, density, nature of use, traffic impacts, aesthetics, infrastructure and potential influence on property values.

NRPUD allows consideration of broad host of commercial, service, office, recreational and industrial/research uses. The proposal limits the host of uses to a large medical complex including a hospital, emergency center and medical offices, as well as outdoor storage and a helipad.

In general, we anticipate that a large medical complex will be compatible with the surrounding area, though we request the applicant provide additional information regarding the outdoor storage and helipad for the Township's consideration under this criterion. (This aspect is discussed in greater detail under paragraph E(3) of this review letter below.)

5. The capacity of Township infrastructure and services sufficient to accommodate the uses permitted in the requested district without compromising the "health, safety and welfare" of the Township.

The applicant must address any comments provided by the Township Engineer, Utilities Director and/or Brighton Area Fire Authority related to this criterion.

6. The apparent demand for the types of uses permitted in the requested zoning district in the Township in relation to the amount of land in the Township currently zoned to accommodate the demand.

As previously noted, the Master Plan anticipates redevelopment of private recreational sites, and specifically identifies hospitals via the PUD option as a desired land use/approach.

The submittal materials also references population trends in the Township as generating an increased demand for healthcare.

7. Where a rezoning is reasonable given the above criteria, a determination the requested zoning district is more appropriate than another district or amending the list of permitted or Special Land Uses within a district.

Based on the Master Plan excerpt noted above, we are of the opinion that NRPUD rezoning for a large medical complex is reasonable, and that a text amendment is not a more appropriate approach.

8. The request has not previously been submitted within the past one (1) year, unless conditions have changed or new information has been provided.

No such requests have been submitted in the past year.

E. Conceptual PUD Plan

The proposal has been reviewed for compliance with the NRPUD standards (Section 10.03.05), as follows:

- **1. Size of uses.** The Ordinance places size limitations on commercial and retail uses. Given the nature of the project as a medical complex, these standards do not apply.
- **2. Open space.** At least 25% of the site must be retained as open space. Calculations provided on Sheet CS100 identify an open space ratio of 37% for the main development parcel and 63% for the smaller parcel with the stormwater detention pond.

The notes indicate that 50% of detention/retention areas were included as open space, in accordance with this standard.

The open space also includes a small outdoor seating area in a courtyard at the rear of the building, which is a benefit described under this criterion.

3. Permitted uses. The NRPUD allows the host of permitted and special land uses in the NSD, OS, GCD, RCD and PRF, as well as low intensity industrial uses.

Hospitals, outdoor storage, accessory fuel storage and heliports are all identified as special land uses; however, the applicant requests that the entire project be allowed by-right via the draft PUD Agreement. This would be a deviation from the NRPUD requirements that is subject to Township approval.

Section 7.02.02(n) identifies the use requirements applicable to hospitals. These standards will be applied to the final PUD site plan (if NRPUD is granted), though they appear to be met by the concept plan. Furthermore, outdoor storage, accessory fuel storage and the heliport are subject to the use conditions of Sections 7.02.02(d), 13.07, and 6.02.02(n), respectively.

Information demonstrating compliance with these conditions must be included with the final PUD site plan submittal (if NRPUD is granted).

- **4. Traffic circulation.** The submittal includes a detailed traffic impact study, as required. We defer technical review of the study to the Township Engineer; however, in summary, the study recommends:
 - driveway improvements at the intersection with Grand River;
 - installation of a traffic signal at the driveway intersection with Grand River;
 - optimization to and coordination of signal timings along Grand River; and
 - improved pavement markings on Parkway Drive as part of this project.

The concept plan includes one main driveway along Grand River and proposes two connections to Parkway Drive; however, a revised Sheet CS101 eliminates the connections to Parkway Drive.

The intersection with Grand River exceeds the 600-foot minimum spacing from the signalized intersections to the east and west.

The concept plan also provided pedestrian walkways, crosswalks and connections along Grand River and throughout the site, as required.

5. Site design. Per this standard, the concept plan includes an ample greenbelt along the roadways, landscaping both around and within parking areas, tree-lined drives, and an outdoor seating area.

Additional information is needed with respect to site lighting (ornamental poles/fixtures are required) and additional landscape screening is needed for the stormwater detention areas.

This standard also requires a Township entranceway landmark; the details of which should be discussed with Township officials. We also suggest the applicant incorporate site amenities outlined under this standard, such as bicycle racks.

The applicant indicates that such details will be provide with a final PUD plan submittal.

6. Architecture. The submittal includes a color rendering/perspective of each building façade (Sheet A-100). This drawing includes the proposed building materials and calculations for each façade.

The Ordinance requires peaked rooflines and that the predominant material be brick; however, the proposed building has a flat roof and "masonry" is noted as the predominant material on only 2 of the 5 facades.

A note on the calculations states that "materials shall be high quality, natural in appearance, attractive, durable, expressive, and inviting," which is generally reflective of the NRPUD architectural standards.

In our opinion, the varied materials, building lines and heights, and extensive windows will result in a quality design, though material deviations are subject to Township approval.

7. Utilities. The submittal includes conceptual utility plans. We defer technical review to the Township Engineer and Utilities Director.

8. Additional Considerations:

- The draft PUD Agreement notes deviations sought for rooftop equipment and loading/unloading in the secondary front yard (Parkway Drive).
- Section 11.01.05 allows hospital buildings up to 60 feet in height, though the proposal is for a 63'-8" tall building. This is now included in the table of deviations.
- The reference to vehicular connections with Parkway Drive in Section 3.2 should be removed.
- There are placeholders for details on signage in Section 5.2 that need to be completed.

Should you have any questions concerning this matter, please do not hesitate to contact our office. I can be reached by phone at (248) 586-0505, or via e-mail at bborden@safebuilt.com.

Respectfully,

SAFEBUILT STUDIO

Brian V. Borden, AICP Planning Manager



March 4, 2020

Ms. Kelly Van Marter Genoa Township 2911 Dorr Road Brighton, MI 48116

Re: St. Joseph Mercy Livingston Hospital PUD Site Plan Review No. 2

Dear Ms. Van Marter:

Tetra Tech conducted a second site plan review of the St. Joseph Mercy Livingston Hospital PUD Conceptual plans last dated February 18, 2020. The plans were prepared by Smithgroup on behalf of Trinity Health. The development includes two parcels located on the south side of Grand River Avenue. The larger of the two parcels is 21.86 acres and is located in the southwest quadrant of the Grand River Avenue and Parkway Drive intersection and the smaller, 2-acre parcel is located on the east side of Parkway Drive, across from the larger parcel. The petitioner is proposing a new hospital, emergency center, and medical office building. We offer the following comments:

TRAFFIC/ROADWAYS

- The provided traffic study recommends a traffic signal at the proposed site driveway off Grand River Avenue. These improvements should be shown on the final site plan. The Petitioner no longer plans on having the two access drives off Parkway Drive. An updated traffic study should be provided with the final site plan submittal to reflect these changes. The impact assessment will also need to be revised, as it still refers to access drives from Parkway Drive.
- 2. The traffic study must be reviewed by MDOT and their approval submitted with the final site plan.
- 3. The final site plan must include more details on the proposed drives and parking lots, including dimensioning, details, and pavement cross sections.

DRAINAGE AND GRADING

- 1. The final site plan will require more detail for the proposed storm management systems such as detention basin sizing calculations, storm sewer sizing calculations, and storm structure sizes and details.
- 2. The final site plan must include proposed grading for the site.
- 3. Approval from MDOT to discharge to Grand River Avenue must be submitted with the final site plan.

Ms. Kelly Van Marter

Re: St. Joseph Mercy Livingston Hospital PUD SPR 2

March 4, 2020

Page 2

UTILITIES

- 1. All proposed water main must be at least 20 feet from the building footprint, per the MHOG design standards. Given the essential use of the structure, we recommend the water main be no closer than 40 feet from the proposed building foundations. The petitioner should also consider routing water main further from the building, in a more direct route, rather than following the outline of the building. These changes can be made on the final site plan.
- 2. Tetra Tech is currently conducting a water and sanitary sewer utility study and will provide a recommendation on where to locate connections to the existing utilities as part of the study. Once the study is complete the petitioner should revise their plans and incorporate the recommendations in the final site plan submittal. After final site plan approval, the petitioner will be required to submit water and sanitary sewer construction plans to MHOG for review and approval.
- 3. The Petitioner is expecting to have sustained summer demands of 50,000 gallons per day based on observed flows from similar facilities. This will account for approximately 229 residential equivalency units (REUs) for the proposed facility. This was determined with the assumption that one REU accounts for 218 gallons per day as noted in the Genoa Township REU Table.

The above comments should be addressed in the final site plan submittal.

Please call or email if you have any questions.

Sincerely,

Gary J. Markstrom, P.E.

Vice President

Shelby Scherdt Project Engineer

helby Schordt



BRIGHTON AREA FIRE AUTHORITY

615 W. Grand River Ave. Brighton, MI 48116 o: 810-229-6640 f: 810-229-1619

February 26, 2020

(See February 4, 2020 review for specific comments)

Kelly VanMarter Genoa Township 2911 Dorr Road Brighton, MI 48116

RE: St. Joseph Mercy Livingston Hospital

4444 E. Grand River Ave.

Genoa Twp., MI

Dear Kelly:

The Brighton Area Fire Department has reviewed the above-mentioned site plan. The plans were received for review on February 20, 2020 and the drawings are dated February 18, 2020. The project is based on a proposed rezoning of two existing parcels per the PUD requirements. It calls for two parcels along Grand River and Parkway Dr. to be redeveloped. The area totals 23.86-acres, 21.86 of which will be utilized to construct a 245,000 square foot hospital and medical office building, along with associated parking and access. The remaining two-acres will be used for stormwater retention for the site. The structure will include a 200,000 square foot, four-story hospital with an attached 45,000 square foot two-story medical office wing (MOB).

The plan review is based on the requirements of the International Fire Code (IFC) 2018 edition.

All Previous comments are under evaluation by the engineers for a re-design of the site layout, fire protection, access, and building-specific comments. BAFA has had numerous conversations and is working toward the final site plan submittal. All items identified in the February 4, 2020 letter have been acknowledged and will be addressed. It should be noted that a cross-connection of the water main and access drives onto Parkway have been eliminated form discussion due to the private nature of the roadway.

Additional comments will be given during the final site and building plan review process (specific to the building plans and occupancy). The applicant is reminded that the fire authority must review the fire protection systems submittals (sprinkler & alarm) prior to permit issuance by the Building Department and that the authority will also review the building plans for life safety requirements in conjunction with the Building Department.

If you have any questions about the comments on this plan review please contact me at 810-229-6640.

Cordially,

Rick Boisvert, CFPS Fire Marshal

AGREEMENT

PLANNED UNIT DEVELOPMENT AGREEMENT

that was repealed in by the MZEA

THIS PLANNED UNIT DEVELOPMENT AGREEMENT (the "Agreement") is made as of _______, 2020, between TRINITY HEALTH – MICHIGAN dba SAINT JOSEPH MERCY HEALTH SYSTEM, a Michigan nonprofit corporation, whose address is 5301 E. Huron River Drive, P.O. Box 992, Ann Arbor, Michigan 48106 (the "Owner") and GENOA CHARTER TOWNSHIP, a Michigan charter township, whose address is 2911 Door Road, Brighton, Michigan 48116 (the "Township").

RECITALS

- A. The Owner owns that certain real property located in the Township of Genoa, County of Livingston, State of Michigan, more particularly described on <u>Schedule A</u> attached hereto (the "<u>Property</u>"), which consists of Parcel 1 with a zoning designation of Public and Recreational Facilities (PRF) District and Parcel 2 with a zoning designation of Industrial (IND) District.
- B. The Owner desires to develop the Property, as a "Non-Residential Planned Unit Development" under Article 10 of the zoning ordinance of the Township (as amended, the "Ordinance") and under the "Conceptual Site Plan" in the form required by the Ordinance and attached hereto as Schedule B which identifies the location and general configuration of each such uses (the "Conceptual Plan").
- C. The Township has determined that the Conceptual Plan (a) properly achieves the purposes of Article 10 of the Ordinance, including the encouragement of innovation in land use, the preservation of open space in areas adjacent to Grand River Avenue in order to achieve compatibility with adjacent land uses, the promotion of efficient provision of public services and utilities, the reduction of adverse traffic impacts, and the provision of adequate employment, (b) is consistent with the Master Plan of the Township, and (c) promotes the public health, safety, and welfare of the Township. The Township acknowledges and has determined that the Grand River Corridor Plan of the Township is not applicable to the Property.
- D. In accord with the Ordinance and the requirements of Act No. 184 of the Michigan Public Acts of 1943, as amended (the "Act") and Act No. 110 of 2006 of the Michigan Public Acts of 2006 (the "MZEA"), the Planning Commission and Township Board have approved the requested rezoning of the Property and the Conceptual Plan.

NOW, THEREFORE, in consideration of the foregoing and the remaining terms, conditions and covenants hereinafter set forth, the Township and the Owner agree as follows:

/	has
_	recommended
	approval

has

and subject to
Township
approval and
compliance with
minimum lot sizes
in the underlying
zoning.

ARTICLE 1

THE ZONING AND CONCEPTUAL PLAN

- 1.1 Representations of Township. The Township warrants and represents to the Owner that (a) the Township has taken all actions necessary to rezone the Property to the Non-Residential PUD (NR-PUD) District, including, without limitation, all required public hearings, notices to nearby landowners, and all other actions required by the Ordinance, the MZEA, and the Act and that no that further action to rezone the Property to the Non-Residential PUD (NR-PUD) District is required; (b) the Township has taken all actions necessary to approve the Conceptual Plan as a Non-Residential PUD, including, without limitation, all required public hearings, notices to nearby landowners, and all other actions required by the Ordinance, the MZEA, and the Act and that no further action to approve the Conceptual Plan as a Non-Residential PUD is required; (c) all uses contemplated by the Conceptual Plan are authorized by the Ordinance; and, (d) the Owner may rely upon this Agreement for purposes of future development of the Property in accord with the rezoning and Conceptual Plan.
- 1.2 Amendment of Conceptual Plan. The Owner and the Township may only amend the Conceptual Plan as permitted by, and in accord with the procedure therefor set forth in, the Ordinance; provided, however, that the Owner may adjust the size or shape of the various parcels of the Property dedicated to differing uses so long as any such adjustment does not alter the land use designation for any area of the Property or increase the intensity and/or density of use.
- 1.3 <u>Site Plan Approvals</u>. Except as otherwise specifically provided in this Agreement, the Owner shall develop the Property only in accord with the Conceptual Plan and subject to site plan approval in accord with the process therefor established in the Ordinance. Each site plan shall superimpose the approved plan of development upon the Conceptual Plan to clearly illustrate the final plan for each portion of the Property. In the event a portion of the Property is submitted for site plan approval and such approval is denied, the party submitting such site plan shall be entitled to appeal such decision to the Township Zoning Board of Appeals as provided by law, and all parties shall agree to proceed expeditiously to final resolution.
- 1.4 <u>Final PUD Site Plan</u>. Within the two (2) year limit prescribed by the Ordinance, the Owner shall submit a "Final PUD Site Plan" to the Township.

ARTICLE 2

LAND USE AUTHORIZATION

When developed, the Property will be

- 2.1 Zoning. The zoning designation of the Property shall be changed to Non-Residential PUD (NR-PUD) District under Article 10 of the Ordinance.
 - 2.2 Phased Development. The Owner plans to develop the Property in one phase.
- 2.3 <u>Detailed Development</u>. The Owner may, in its discretion, develop the Property in accord with the Conceptual Plan and subject to the obligation to coordinate development of the Property as a whole pursuant to this Agreement. In connection with the discretionary development of the Property, the Owner shall submit to the Township for review and approval a plan in the form required by the Ordinance, including impact assessments required by the Township (including an updated Traffic Impact Study which meets the requirements of Section 18.07.09 of the Ordinance, showing how traffic will differ from that originally projected, a Natural Features Plan, and a Pollution Incident Prevention Plan). The Township shall

review each of such plans within a reasonable time. Unless the Conceptual Plan is materially altered at the request of the Owner, site plan and other review requirements with respect to any such plan shall be as in effect as of the date of this Agreement, and shall not be subject to any subsequent amendments of the Ordinance which are inconsistent with this Agreement.

- 2.4 Permitted and Special Use. The Township acknowledges the Owner's plan to develop the Property as one building as a hospital, emergency center, and for medical offices, together with a heliport, outdoor storage, storm water detention pond, parking, vehicular and pedestrian access, and associated improvements as shown on the Conceptual Plan. The Township further acknowledges the Owner's plan to develop the hospital and emergency services portion of the building as a 4-story building with approximately 200,000 gross square feet; and, proposes to develop the medical office portion of the building as a 2-story building with approximately 45,000 gross square feet. Notwithstanding the requirement of a permit for any special land use, variance, fee, or other submission under the Ordinance, the Township agrees that such uses shall not require any special land use permit, variance, fee, or other submission or additional application forms beyond those normally required for general site plan approval.
- 2.5 <u>Open Space</u>. The Owner shall develop a minimum of 25% of the Property as open space, defined as undisturbed areas of key natural features, lake and pond areas, retention and/or detention ponds, landscaped areas, plazas, and residential uses. <u>Schedule B</u> shows the calculation and location of open space as shown on the Conceptual Plan.

ARTICLE 3

ACCESS TO AND WITHIN THE PROPERTY

may

- Traffic Impact Study. The Owner shall submit to the Township for review and approval a traffic impact study as required by Article 18.07.09 of the Ordinance. Such study shall evaluate the impact of the planned development of the Property at each access point and existing adjacent major intersections, specifically including any intersections where traffic from the Property will comprise at least five percent (5%) of the existing intersection capacity. Such study shall also include proposals to mitigate any adverse impact of such development identified by the study and describe timing and responsibility for funding any improvements designed to implement any such proposals. At such time as the Township and Livingston County Road Commission determine that a signal is warranted at the main entry to the Property, the Owner shall fund the installation of said traffic signal and such other traffic mitigation measures as are required by its development of the Property. However, nothing in this Agreement shall require the Owner to pay more than its pro-rata share of improvements as determined by the proportion of total traffic increase which is generated by developments on the Property.
- 3.2 Access to the Property. Except those entrances generally shown on the Conceptual Plan along Parkway Drive, access to the Property shall be limited to one major entrance from Grand River Avenue and a secondary entrance at the existing intersection of Grand River Avenue and Parkway Drive. Both access points along Grand River Avenue shall be full movement, with Grand River Avenue ingress and egress from both directions. The Township will only consider additional access points along Grand River Avenue if spaced a safe distance apart and a traffic impact study submitted by the Owner demonstrates that overall traffic operations and safety will be improved. The Owner shall determine the specific location of permitted access points utilizing safety standards established and approved by the Township and the Livingston County Road Commission. The Owner shall also locate access points such distances from existing signalized intersections to ensure sound traffic operations if the one of the access points is signalized. The site design for the Property shall direct traffic flow to use the main access point to public thorough fares. Stacking or queuing depth at access points shall be sufficient to accommodate expected peak hour volumes to minimize conflict with inbound or internal circulation.

Subject to the approval of the Township, Livingston County Road Commission and if necessary, the Michigan Department of Transportation,

- 3.3 <u>Internal Roadways</u>. The internal system of vehicular thoroughfares is shown on the Conceptual Plan. The Owner shall plan and establish the internal system of vehicular thoroughfares throughout the Property in connection with development of portions of the Property as it occurs. The Owner shall design internal roads in such a way as to permit vehicular access between and among users of the Property, as ultimately developed, with the view and intent of minimizing the number of traffic movements onto adjoining public roads. The Township shall review and approve the precise locations and design of the overall system of internal thoroughfares as each site plan for development of portions of the Property is submitted to the Township for approval, taking into consideration the uncertainty of the development of the then undeveloped portions of the Property.
- 3.4 <u>Pedestrian Walkways</u>. The Owner shall construct an eight-foot-wide pedestrian walkway along Grand River Avenue. The Owner shall also construct pedestrian walkways between each building and the parking area(s) that serve each such building, as the Property is developed. The pedestrian walkway will conform to the walkway plan shown conceptually in the Final PUD Site Plan. The Township and Owner agree that the purpose of any such pedestrian walkways is to serve the business needs of the Owner and other occupants of buildings on the Property, and not to encourage recreational pedestrian use by the general public.

ARTICLE 4

SITE IMPROVEMENTS

4.1	Coordination of S	ite Improvements.	The Owner shall	coordinate the	construction of site
improvemen	its within the Property	as a whole to ensur	re that site improv	ements are integ	grated and mutually
supportive a	mong the respective	portions of phases	of development	of the Property,	including utilities,
landscaping,	and site lighting.	paragraph 2.2	says they are doir	ng this in one ph	ase

- 4.2 <u>General Guidelines</u>. The Owner shall observe the following guidelines in connection with the construction of site improvements on the Property: may
 - 4.2.1 Landscape islands and tree-lined drives shall be located within parking lots to improve traffic operations and reviews, in accordance with the Ordinance.
 - 4.2.2 Parking shall be provided by Owner constructing 750 parking spaces and allowing space for an additional 200 parking spaces which shall be considered "banked". The Township shall not require the installation of the parking areas noted as "banked" on the Conceptual Plan and the Owner may, at any time and in its sole discretion, install any portion of or all of such parking areas noted as "banked" on the Conceptual Plan. Loading and unloading may occur in the front yard.

4.2.3 Any storm water control devices visible from public streets shall be designed to have a naturalistic appearance, such as variable shape, natural arrangement of landscape materials.

- 4.2.4 Outlets into the stormwater system on Parcel 2 will be improved to properly restore their functionality without further approval required by the Township
- 4.2.5 Landscaping shall demonstrate consistency in design and materials.
- 4.2.6 Site lighting shall be of a uniform type and color throughout.

Parkway Drive

4.3 <u>Maintenance</u>. The Owner shall be responsible for ongoing maintenance and repair or replacement of site design elements, such as open spaces, signage, landscaping, lighting, and pavement markings. The Owner shall be responsible for maintenance and repair or replacement of facilities serving the entire Property such as stormwater control devices and any roadways within the Property.

ARTICLE 5

DESIGN OF BUILDINGS AND SIGNAGE

- 5.1 <u>Building</u>. The Owner shall develop buildings on the Property that incorporate varying building lines, natural earth tone construction materials, and other elements that are aesthetically pleasing. The Owner shall follow the following general guidelines:
 - 5.1.1 building designs shall be quality, thoughtful solutions with scale, materiality, lines, and details to promote attractive and approachable structures;
 - 5.1.2 each site plan shall include a narrative or illustration(s) that demonstrate the design of new buildings will be consistent with, or complement, architecture of the other sites; and,
 - 5.1.3 the exterior materials shall be high quality, natural in appearance, attractive, durable, expressive, and inviting; cladding may include but shall not be limited to decorative masonry, stone, high performance glazing, and premium metal panels.

5.1.4 TBD

5.2 <u>Signage</u> . Freestanding signs within the PUD shall be ground-mounted (monument) sign	gns.
All freestanding signs shall have a base constructed of materials that coordinate with and are consist	tent
with the building, and with other freestanding signs within the PUD. All freestanding lighted signs sl	hall
be internally lit. The following signs shall be permitted on the Property:	

5.2.1	one (1) monument sign at the at the corner of Parkway Drive and Grand River Avenue of up to square feet (SF) in area with a height of feet;
5.2.2	one (1) monument sign at the Grand River Avenue main drive entrance to the Property, of up to square feet (SF) in area with a height of feet;
5.2.3	the building shall be entitled to three (3) wall signs which together do not exceed square feet ();
5.2.4	one (1) non-commercial sign, if needed by Owner, shall not exceed square feet (SF);
5.2.5	() directional signs shall not exceed square feet (SF) with a height of feet;
5.2.6	One (1) monument sign on south end of Property boundary;
5.2.7	the size, materials, and other specifications of each such wall sign shall be

approved by the Township;

- 5.2.8 all other signs, including directional and wayfinding signs, shall conform to the size, materials, and other specifications of the Ordinance; and
- 5.2.9 directional and wayfinding signs shall be provided throughout the site as shown on a master sign and pavement marking plan to be approved with the Site Plan, with the number of signs not restricted to one per approved driveway, but as determined by the requirements of way finding and safety, and approved by the Township.
- 5.3 <u>Building Height</u>. The Owner shall be permitted to erect hospital and emergency services portion of the building as a 4-story building with general roof height of approximately 63 feet, 8 inches above finished floor (63' 8"AFF) measured as indicated in the Ordinance. The owner shall be permitted to erect a stair tower as part of the hospital portion of the building to an approximate roof height of 80 feet 0 inches (80' 0" AFF) measured as indicated in the Ordinance. The Owner shall be permitted to erect the medical office portion of the building as a 2-story building with general roof height of approximately 33 feet, 4 inches above finished floor (33' 4" AFF) measured as indicated in the Ordinance. Roof mounted equipment may exceed 10 feet and 15% of the total roof area on the entire building. All other buildings on the Property shall meet the specific height limitations required by the Ordinance for the respective type of building.
- 5.4 <u>Deviations</u>. The Township approves the deviations from the Township regulations as identified on <u>Schedule C</u> attached hereto.

ARTICLE 6

UTILITIES

- 6.1 <u>Water</u>. Each building on the Property must connect to the community water system. The Owner shall submit a master water plan for the Property as an attachment to the Final PUD Site Plan for approval by the Township and its Engineers. The Owner shall install the internal water main for each developed portion of the Property prior to or concurrent with the site plan approval for such development.
- 6.2 <u>Sanitary Sewer</u>. Each building constructed on the Property shall, as developed, be connected to and be served by the public sanitary sewer. The Township represents that there has been reserved for the Owner adequate municipal wastewater treatment capacity to service the approved use of the Property, and the adequacy of wastewater treatment capacity shall not limit the type of use or density of the development based on the Concept Plan.
- 6.3 <u>Electrical</u>. All electrical lines serving the buildings on the Property shall be underground. Public utility lines in existing or future easements shall be permitted overhead so long as the buildings are serviced from underground.
- 6.4 <u>Fees.</u> Fees, charges, and costs for utilities shall be as set forth on attached <u>Schedule D</u>, which may be amended on a district-wide basis from time to time.

ARTICLE 7

MISCELLANEOUS

, unless required to be underground by the utility company or the Michigan Public Service Commision or otherwise required by law, 7.1 <u>Notices.</u> All notices and communications required or permitted to be given under this Agreement shall be in writing and hand delivered, or via email or facsimile, or mailed by certified or registered mail, postage prepaid, or delivered by FedEx or similar overnight delivery service, addressed to the parties at their addresses recited below:

If to Owner:	
Phone: (
Dawda, Mann, Mulcahy & Sadler, PLC Dawda Mann Building 39533 Woodward Ave., Suite 200 Bloomfield Hills, MI 48304 Attn: Edward C. Dawda/Tyler D. Tennent Phone: (248) 642-3700 Fax: (248) 642-7791 If to Township:	Zoning Administrator in accordance with the Township's Zoning ordinances, such as sections 10.11
Phone: ()	and 18.10

The parties may designate a change of address by notice given in accordance with the provisions of this Section at least ten (10) days before such change of address is to become effective.

- Amendment. This Agreement may not be modified replaced, amended or terminated 7.2 without prior written consent of the parties to this Agreement. The Owner and the Township shall be entitled to modify, replace, or amend this Agreement without the consent of any other person or entity, regardless of whether such person or entity now or hereafter has any interest in any part of the Property, including subsequent purchasers, or their tenants, mortgagees, or others. This Agreement shall allow, and shall be interpreted to allow, the Owner flexibility in the construction, development, design, and use of the Property, including, but not limited to, (i) agreed upon deviations from the strict application of the existing Ordinance, (ii) rules, policies, and regulations relating to building height and stories, access, building length, setbacks, sidewalks, parking, recreation, open space, road dimensions, landscaping, land divisions and combinations, type of use, mix of use, building architecture and materials, and signage, and (iii) modification of the size, number, location and configuration of the building within the Property. Minor modifications regarding deviations; building layout, location, and size; and, the configuration, architectural design and materials of the building shall be permitted and approved by the Township Supervisor, without action by the Township Board or the need to amend this Agreement, to allow the Owner flexibility and as may be reasonably necessary.
- 7.3 <u>Permits and Authorizations</u>. The Township shall grant to Owner, and to its contractors and subcontractors, all Township permits and authorizations reasonably necessary to bring all utilities to the Property including electricity, telephone, gas, cable television, water, storm, and sanitary sewer, and shall grant to owner any such other approvals and authorizations reasonably necessary to develop and improve

and is otherwise in compliance with State, County and local laws

the Property in accordance with this Agreement, and in accordance any subsequent approvals of any governmental authority, provided that the Owner has first made all requisite applications for permits—and paid all required fees. Any applications for permits from the Township will be processed in the customary manner and shall not be unreasonably delayed or withheld. The Township will cooperate with Owner in connection with such applications for any necessary county, state, federal or utility company approvals, permits or authorizations.

- 7.4 Enforcement. Notwithstanding anything to the contrary contained in the Ordinance or allowed by applicable law, in the event there is a failure to timely perform any obligation or undertaking required by this Agreement, the Township shall serve written notice upon Owner setting forth such deficiency and a demand that the deficiency be cured within a reasonable time following the notice. If the deficiency set forth in the notice is not cured within a reasonable time, the Township shall serve written notice upon the Owner setting forth the deficiency and the date, time and place for a hearing before the Township Board, for the purpose of allowing the Owner an opportunity to be heard as to the reasons for the deficiency, and what actions will be taken to correct the deficiency. At any such hearing, the time for curing and the hearing itself may be extended and/or continued to a date certain. If, following the hearing described above, the Township Board determines that the obligation has not been fulfilled or has not been corrected within the time specified in the notice, or if an emergency situation exists as determined by the Township in its discretion, the Township shall have the power and authority, but not the obligation, to take any or all actions authorized under the Ordinance and state law.
- 7.5 <u>Binding Effect</u>. This Agreement shall be binding upon and inure to the benefit of the parties hereto and their respective legal representatives, successors, and assigns. The rights and obligations under this Agreement shall run with the Property and shall bind and inure to the benefit of any successors-in-interest of the Owners in the Property.
- 7.6 <u>Controlling Effect</u>. In the event of any direct conflict between the terms and provisions of this Agreement and the Conceptual Plan, and the provisions of the Ordinance, or other Township ordinances, rules, or regulations, the provisions of this Agreement and the Conceptual Plan shall control.
- 7.7 <u>Conditions Reasonable</u>. The Owner acknowledges that the conditions imposed by this Agreement upon the development of the Property are reasonable and necessary to ensure that public services and facilities affected by the proposed land use or activity will be capable of accommodating increased service and facility loads caused by the land use or activity, to protect the natural environment and conserve natural resources and energy, to ensure compatibility with adjacent used of land, and to promote the use of land in a socially and economically desirable manner. The Owner further acknowledges that the conditions meet all of the requirements of the Act and of the MZEA.
- 7.8 <u>Governing Law.</u> This Agreement shall be governed by and construed in accordance with the laws of the State of Michigan, without giving effect to principles of conflicts of law.
- 7.9 <u>Counterparts</u>. This Agreement may be executed in one or more counterparts, each of which shall constitute an original, and all of which shall constitute one and the same agreement.
- 7.10 <u>Plans and Drawings</u>. The plans and drawings listed in <u>Schedule E</u> are attached to and made a part of this Agreement.

[Signatures Appear on the Following Page]

This Agreement has been executed by a duly authorized representative of the Owner as of the date first set forth above.

WITNESSES:		TRINITY HEALTH-MICHIGAN dba SAINT JOSEPH MERCY HEALTH SYSTEM
		By:
STATE OF MICHIGAN)) ss.	
COUNTY OF)	
The foregoing instrument was acl O'Malley, President, St. Joseph M Health-Michigan dba Saint Josep	knowledged befor Mercy Livingston l h Mercy Health S	e me this day of, 2020, by John Hospital & Brighton Health Center, on behalf of Trinity system.
		Notary Public
		County, Michigan My commission expires:
WITNESSES:		GENOA CHARTER TOWNSHIP By: Name: Bill Rogers Title: Supervisor
		•
		By:
STATE OF MICHIGAN)) ss.	
COUNTY OF)	
The foregoing instrument was ac Rogers and Paulette A. Skolarus, Michigan charter township, on be	, respectively the S	re me this day of, 2020, by Bill Supervisor and Clerk of the Genoa Charter Township, a ship.
		Notary Public County, Michigan My commission expires:
		My commission expires:

Drafted by and when recorded return to:

Edward C. Dawda/Tyler D. Tennent Dawda Mann Dawda Mann Building 39533 Woodward Avenue, Suite 200 Bloomfield Hills, Michigan 48304

SCHEDULE A

Legal Description of Property

Parcel 1 (DAV LLC):

Real property located in Genoa Township, County of Livingston, State of Michigan, more particularly described as follows:

Part of the East ½ of the Northwest ¼ of Section 9, T2N-R5E, Genoa Township, Livingston County, Michigan, described as follows: Commencing at the Northwest corner of said Section 9; thence North 1 degree 45' 40" East 549.30 feet along the centerline of Latson Road and the Section line; thence South 60 degrees 55' 25" East 587.60 feet along the centerline of Grand River Avenue; thence South 60 degrees 50' 00" East 1618.14 feet along the centerline of Grand River Avenue to the point of beginning of the land to be described; running thence South 60 degrees 50' 00" East 378.78 feet along the centerline of Grand River Avenue; thence South 29 degrees 16' 00" West 202.51 feet; thence South 1 degree 31' 50" West 937.31 feet; thence South 89 degrees 51' 00" west 884.69 feet along the Northerly Right-of-Way line of I-96 Rest Area; thence North 1 degree 32' 25" East 999.96 feet; thence South 88 degrees 25' 30" East 475.63 feet; thence North 29 degrees 16' 00" East 360.25 feet to the point of beginning. Subject to easements of Grand River Avenue and easement of record.

Tax Parcel No. 4711-09-100-015 Commonly known as 4444 E. Grand River, Howell, Michigan

[Legal description continues on next page]

Parcel 2 (Chapman):

Real property located in the Township of Genoa, County of Livingston, State of Michigan, more particularly described as follows:

PARCEL 2:

A part of the Northwest ¼ of Section 9, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan, described as follows: Commencing at the Northwest corner of said Section 9; thence North 01 degree 45 minutes 40 seconds East along the centerline of Latson Road and the West line of said Section 4, 549.50 feet to the centerline of Grand River Avenue (100 feet wide); thence South 60 degrees 55 minutes 25 seconds East along said centerline, 587.60 feet; thence South 60 degrees 50 minutes 00 seconds East along said centerline 2375.70 feet to the North-South ¼ line of said Section 9; thence South 01 degree 31 minutes 50 seconds West along said North-South ¼ line 215.00 feet to the point of beginning of the parcel to be described; thence continuing South 01 degree 31 minutes 50 seconds West along said North-South ¼ line, 240.00 feet; thence North 88 degrees 28 minutes 10 seconds West 363.81 feet to the Easterly line of a 66 foot wide private road easement for ingress and egress; thence North 01 degree minutes 50 seconds East along said Easterly easement line, 240.00 feet; thence South 88 degrees 28 minutes 10 seconds East, J3.81 feet too the point of beginning.

Including the use of a 66 foot wide, non-exclusive, private road easement and cul-de-sac being part of the Northwest 1/4 of Section 9, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan, described as: Commencing at the Northwest corner of said Section 9; thence North 01 degrees 45 minutes 40 seconds East along the centerline of Latson Road and the West line of Section 4, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan, 549.30 feet to the centerline of Grand River Avenue (100 feet wide); thence South 60 degrees 55 minutes 25 seconds East along said centerline, 587.60 feet; thence South 60 degrees 50 minutes 00 seconds East along said centerline, 1996.92 feet to the point of beginning of the easement to be described; thence continuing South 60 degrees 50 minutes 00 seconds East along said centerline, 66.00 feet; thence South 29 degrees 16 minutes 00 seconds West along the Easterly line of the easement being described, 137.69 feet; thence Southwesterly along said Easterly line, 95.37 feet along the arc of a curve left, which has a central angle of 27 degrees 44 minutes 10 seconds, a radius of 197.00 feet, and a long chord bearing South 15 degrees 23 minutes 53 seconds West 94.44 feet; thence South 01 degree 31 minutes 50 seconds West along said Easterly line, 486.03 feet; thence Southeasterly, 83.64 feet along a curve left, which has a central angle of 63 degrees 53 minutes 46 seconds, a radius of 75.00 feet, and a long chord bearing South 30 degrees 25 minutes 03 seconds East 79.37 feet; thence 319.26 feet along the arc of a 75 foot radius cul-de-sac on a curve right, which has a central angle of 243 degrees 53 minutes 46 seconds, a radius of 75.00 feet and a long chord bearing South 59 degrees 34 minutes 57 seconds West 127.28 feet; thence North 01 degree 31 minutes 50 seconds East 685.66 feet; thence North 29 degrees 16 minutes 00 seconds East, 202.51 feet to the centerline of Grand River Avenue and the point of beginning.

Tax Parcel No. 4711-09-100-020 Commonly known as: Vacant Land

SCHEDULE B

Conceptual Plan



SCHEDULE C

Deviations

Ordinance Standard	Deviation	Ordinance Reference	Agreement Reference
Maximum building height of 60' 0"	Exceeds 60' 0".	11.01.05	5.3
Roof-mounted equipment not to exceed a height of 10'	Exceeds 10'.	11.01.05(c)(2)	5.3
Roof -mounted equipment shall not occupy more than 15% of the total roof area	Exceeds 15%.	11.01.05(c)(2)	5.3
Loading/unloading areas shall not be in front yard.	Loading/ unloading in front yard. Parkway Drive is a private road which makes Parcel 1 a corner lot with front yard along both Grand River Ave. and Parkway Drive.	14.08.03	4.2.3
Non-commercial signs shall not exceed two (2) square feet in area.	Non-commercial sign exceeds two (2) square feet in area.	16.03.10	5.2
No more than one (1) directional sign shall be permitted per approved driveway, with a maximum sign area of four (4) square feet per sign, and a maximum height of three (3) feet.	directional signs which will exceed maximum sign area of four (4) square feet per sign, and a height of three (3) feet.	16.07.03	5.2
One wall sign shall be allowed per business with its own public entrance, unless Planning Commission allows additional signs.	Three (3) wall signs will exist because the lot is on the corner of Grand River Ave and Parkway Drive.	16.1 (footnotes to Table 16.1)	5.2
Wall signs shall not exceed an area of ten percent (10%) of the façade of a building, unless Planning Commission allows additional area.	Wall signs will be in excess of ten percent (10%) of the building façade.	16.1 (footnotes to Table 16.1)	5.2

One monument sign is	Two (2) monument	16.1 (footnotes to Table	5.2
allowed, unless more	signs.	16.1)	
are approved by the			
Planning Commission.			
Monument signs may	Monument signs will	16.1 (footnotes to Table	5.2
not exceed six (6) feet	exceed six (6) feet in	16.1)	
in height.	height.		
1,025 parking spaces	950 parking spaces	14.04	4.2.3
required.	provided with 750 built		
	and 200 banked.		

SCHEDULE D

Fees, Charges, and Costs of Utilities

To be determined.

SCHEDULE E

Other Plans and Drawings



Conceptual Main Entrance Drive



Conceptual Main Entrance

Attachment 1

ENVIRONMENTAL IMPACT ASSESSMENT

ST. JOSEPH MERCY LIVINGSTON

February 19, 2020 Concept PUD Submittal

a. Names and addresses of persons responsible:

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Gary J. Markstrom, Professional Engineer and Genoa Township consultant

King & MacGregor Environmental, Inc.

2520 Woodmeadow SE Grand Rapids, MI 49546 Woody Held, PLA and Wetland Specialist

Testing Engineers & Consultants, Inc.

1343 Rochester Road
PO Box 249
Troy, MI 48099-0249
Carey J. Suhan, PE, Geotechnical & Environmental Services

b. Map(s) and written description/analysis of the project site:

The project site consists of 2 parcels, with the larger, 21.86-acre parcel fronting on the south side of Grand River Avenue and west side of Parkway Drive. A detention area is located south of the larger parcel on MDOT property, a 30' DTE easement is located along the west edge of the property, and a 15' wide public utility easement is on the north side of this parcel. It is currently occupied by the Grand River Golf Range and Putt-Putt, with a small building, parking lot and putt-putt course located on the north side and the golf range occupying the remainder of the site. The smaller, 2-acre parcel fronting on the east side of Parkway Drive is vacant land, with a 20' sanitary sewer easement, a 12' DTE easement and a variable width storm easement currently being used for detention along the east and north sides. See attached ALTA/NSPS Land Title Survey with topographic information and legal descriptions for Parcels 1 and 2 and aerial photo.

c. Impact on natural features:

The existing topography on the larger parcel is primarily gently sloping to the east, with the southeast portion of the site utilized for detention. A few landscaped berms with steeper slopes (greater than 20%) are in the golf range and putt-putt areas. The smaller parcel also slopes to the east to another detention area, as previously noted. Based upon the geological data and borings in the area, existing soils are a variable profile of clay and sand. Clay consistencies may range from plastic to extremely stiff and relative density of sand varying from loose to compact. No endangered species habitats were observed during the course of the 2019 survey of the subject property. There are no significant trees on the smaller parcel, however, there are large trees (8" cal. and over) identified on the survey along all of the large parcel property borders and in the putt-putt golf area. The property is outside of the 100-year flood area (Zone X unshaded) on the Federal Emergency Management Agency (FEMA) Flood Insurance Rate Map (FIRM). Zone X unshaded is defined as areas of minimal flooding (see attached FIRMETTE). A pre-application meeting was conducted on the project site with a Water Resources Division representative from the Department of Environment, Great Lakes, and Energy (EGLE). Based upon an in-office review of the project plans and this on-site review, it was determined that the wetlands present on Parcel 2 (the smaller parcel) will not require a permit for the proposed project, as they were incidentally created as a result of construction of a storm water basin constructed in upland. (See attached EGLE Preapplication Meeting Letter). Considering the conditions described above, the site is suitable for the proposed development.

In order to develop the site, regrading will be required to accommodate the planned roads, parking lots, walkways, buildings and utilities needed for the new hospital and medical office building. New soils and aggregates will be imported from off-site to build the development. Some of the existing large trees will need to be removed and replaced with new trees, as required by the ordinance.

d. Impact on storm-water management:

The proposed development will follow all MDOT and Livingston County Drain Commissioner requirements for stormwater detention and discharge. Soil erosion control measures will be followed during construction, per Appendix K: Soil Erosion and Sedimentation Control Plan Check List from Livingston County Drain Commissioner (see attached).

e. Impact on surrounding land used:

A new hospital, emergency center and medical office building are planned to benefit the community by providing state-of-the-art healthcare facilities. This development complements the existing adjacent commercial areas and is conveniently located to nearby residential areas, providing access from the I-96 expressway to the south and East Grand River Avenue to the north. In addition, the DTE easement provides a wide buffer to the residential area to the west. Site improvements include a loop road, drop-off areas, parking lots, helipad, loading area, generators, oxygen tanks, mobile MRI pad for trucks and ambulance access. Although exterior lighting will be required for the roadways and walks to provide safe access for staff and visitors,

full cut-off fixtures are planned to decrease unnecessary light pollution. Lighting will also be required for the safe use of the helipad, currently planned to only pick up patients and take them to another facility if needed. Although the generators, helicopters, vehicles and ambulances planned for the site will increase the noise and air pollution, this is not expected to be a problem in this area, due to the current noise level and air pollution from I-96 and East Grand River and their proximity to the site. In addition, truck deliveries, helipad and generators are planned for the east side of the site, located furthest from the high-density residential development west of the DTE utility corridor.

f. Impact on public facilities and services:

Based on projections for the hospital and MOB, approximately 602 employees and xxx patients and visitors are expected to visit/work at the facility daily. This development is not expected to impact the public schools. Police and fire protection will be needed for both the hospital and MOB.

- g. Impact on public utilities: New storm sewers, above ground and underground detention facilities are planned on-site to control site storm drainage and follow the Livingston County Drain Commissioner storm water management guidelines Soil erosion control measures will be followed during construction to control runoff, per Appendix K: Soil Erosion and Sedimentation Control Plan Check List from Livingston County Drain Commissioner (see attached). Impacts on water and sanitary sewer facilities will be completed following Tetra Tech water and sanitary sewer utility study. Sewer and water facilities will be designed in accordance with the MHOG design standards
- h. Storage and handling of any hazardous materials: Oxygen tanks will be installed on a concrete pad as required by code and are located near the truck docks on the east side of the site. Below grade fuel tanks will be installed to the proper setbacks from the building outside of the loading dock, with depth below grade and venting as required by code. It is the policy of Saint Joseph Mercy Health System (SJMHS) to ensure the safe handling and disposal of medical waste throughout the organization including the proposed development. SJMHS enforces a strict medical waste policy and plan, including obtaining certificates from regulatory agencies, oversight from SJMHS Safety Program personnel to ensure safe practices for the transportation, packaging, and storage of medical waste.
- i. Impact on traffic and pedestrians: The proposed development will improve pedestrian connectivity by constructing an eight (8) foot wide sidewalk along Grand River Avenue in accordance with the Zoning Ordinance. Additional interior sidewalks will connect the building to Grand River Avenue and provide necessary interior circulation with safe, accessible routes. A traffic study (see attachments) was conducted to analyze the vehicular impact of this development on surrounding public roads. Based on the results of the traffic study (see attachments), the following are proposed to provide acceptable traffic operations:
 - a. Install a traffic signal at the proposed site driveway to I-96 BL aligned with Arundell Avenue.

- b. Optimize the network signal timings and coordinate all traffic signals, including the proposed signal along I-96 BL.
- c. Install pavement markings on Parkway Drive in accordance with MMUTCD guidelines.

The proposed improvements above will be submitted to MDOT for review and approval, as Grand River Avenue is a MDOT right-of-way.

j. Special Provisions:

All easements are shown on ALTA/NSPS Land Title Survey (see attached).

k. Demand for the proposed development:

The population of Genoa Township has grown over the past decade and as population grows the demand for healthcare increases. Additionally, the proposed new hospital will replace the facility in Howell and provide a new state-of-the-art hospital and MOB for the residents of Genoa Township.

I. Impact on Genoa Township Master Plan and zoning:

The proposed hospital and MOB development is consistent with the Genoa Township Master Plan, as it is located within the area designated as a Primary Growth Area. Additionally, rezoning to a Planned Unit Development is in accordance with the recommendation on page 5-6 of the Genoa Township Master Plan that specifies re-zoning new hospital developments to PUD.

m. A list of all source material:

Appendix K: Soil Erosion and Sedimentation Control Plan Check List Flood Insurance Rate Map (FIRM) from Federal Emergency Management Agency (FEMA) SmithGroup ALTA/NSPS Land Title Survey dated 10/09/2019 EGLE Preapplication Meeting Letter dated 11/22/2019





SJMHS Hospital Traffic Impact Study

Genoa Township, MI January, 2020

29777 Telegraph Road | Suite 1640 Southfield, Michigan 48034 248.663.1379 www.bergmannpc.com



TRAFFIC IMPACT STUDY

SJMHS HOSPITAL – GENOA TOWNSHIP, MI

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Executive Summary

This Traffic Impact Study (TIS) was completed for the proposed St. Joseph Mercy Health Systems (SJMHS) Hospital and Medical Office Building (MOB) in Genoa Township, Livingston County, Michigan. The project site is located along the south side of Grand River Avenue (I-96 BL) approximately ½ mile east of Latson Road and is currently occupied by the Grand River Golf Range & Putt Putt facility. The purpose of this study is to identify the traffic related impacts, if any, of the proposed project on the adjacent road network. This study was conducted in accordance with accepted traffic engineering practice and guidelines published by the Institute of Transportation Engineers (ITE). Township Ordinance as well as Michigan Department of Transportation (MDOT) and Livingston County Road Commission (LCRC) standards were referenced as applicable.

The proposed development plans include construction of an approximately 200,000 square foot (SF) Hospital facility to accommodate approximately 500 employees and 72 beds in addition to a 50,000 SF MOB. Access for the site is currently provided via a single driveway to I-96 BL aligned with Arundell Avenue. The existing access location is not proposed to change as part of the development plans but will be reconstructed and widened to accommodate separate left turn and right turn / shared through lanes exiting the site. Additionally, multiple access points are proposed to Parkway Drive which also currently provides access to I-96 BL. Traffic volumes that are expected to be generated by the development were forecast based on the rates and equations published by ITE in *Trip Generation*, as summarized below:

Land Use	ITE	Amount	Units	Average	AM	Peak I	lour	PM	Peak H	lour
Land Ose	Code	Amount	Units	Daily	In	Out	Total	In	Out	Total
Hospital	610	502	Employees	2,675	171	63	234	57	153	210
Medical Office Building	720	50,000	SF	1,833	94	27	121	48	124	172
TOTAL				4,508	265	90	355	105	277	382

The overall conclusions of this Traffic Impact Study are as follows:

- 1. All study intersection approaches and movements currently operate acceptably at a LOS D or better during both peak hours with the exception of the following:
 - a. The signalized intersection of I-96 BL & Latson Road which currently operates at a LOS E during the PM peak hour with several individual movements currently operating at a LOS E or F during both peak hours.
 - b. The STOP controlled approaches of Arundell Avenue and the Grand River Golf Range & Putt Putt drive with I-96 BL which currently operate at a LOS F during both peak hours.
 - c. The STOP controlled left turn movement from Parkway Drive to I-96 BL which currently operates at a LOS E during both peak hours.
- 2. Ambient traffic growth of 1.0% was applied to establish 2024 background traffic volumes *without the proposed development*. Several background developments were also identified in the study area and included in this study.



3. Background (2024) conditions analyses indicate that several movements will experience degraded operations including the intersection of I-96 BL & Latson Road, and the left-turn movements from Latson Road to the EB and WB I-96 on-ramps.

Future (2024) conditions analyses indicate that most study intersection approaches and movements will continue to operate acceptably; however, there are several locations with undesirable or failing conditions that are expected to worsen in the future, if those movements operating at a LOS E or F are not improved under background conditions.

- 4. Egress left turns from the site and Parkway Drive to I-96 BL will not operate acceptably under STOP control with the proposed development. In accordance with MMUTCD requirements a traffic signal is warranted at the site driveway with the proposed development.
- 5. With the improvements outlined below, all study network intersections and site driveways will operate acceptably, or in a manner similar or improved compared to background conditions during the peak hours.

Based on the results of this study, the following should be considered to provide acceptable traffic operations due to existing deficiencies and other development projects, *regardless of the proposed Hospital and MOB:*

1. Install permissive-protected left-turn phasing at the intersections of Latson Road with the EB and WB I-96 ramps.

Based on the results of this study, the following should be considered to provide acceptable traffic operations with all development projects, including the proposed Hospital and MOB:

- 1. Install a traffic signal at the proposed site driveway to I-96 BL aligned with Arundell Avenue.
- 2. Optimize the network signal timings and coordinate all traffic signals, including the proposed signal along I-96 BL.
- 3. Install pavement markings on Parkway Drive in accordance with MMUTCD guidelines.

This report is intended for use by the Township, MDOT, and LCRC to guide decisions related to development project approvals, access permitting, and identifying future roadway improvement needs. The methodologies, analyses, results, and recommendations relevant to this study are described in detail herein. The opinions, findings, and conclusions expressed in this report are those of Bergmann and not necessarily those of the Owner, Genoa Township, MDOT, and/or LCRC.

Prepared By:

Steven J. Russo, PE Bergmann Associates





Project Overview

This report presents the methodologies, analyses, results, and recommendations of a Traffic Impact Study (TIS) for the proposed St. Joseph Mercy Health Systems (SJMHS) Hospital and Medical Office Building (MOB) in Genoa Township, Livingston County, Michigan. The project site is located along the south side of Grand River Avenue (I-96 BL) approximately ½ mile east of Latson Road and is currently occupied by the Grand River Golf Range & Putt Putt facility as shown on **Figure 1**.

The proposed development plans include construction of an approximately 200,000 square foot (SF) Hospital facility to accommodate 500 employees and 72 beds in addition to a 50,000 SF MOB. This project is planned to be built out by 2024, and assumed to be fully occupied by that time for the purposes of this study. Access for the site is currently provided via a single driveway to I-96 BL aligned with Arundell Avenue. The existing access location is not proposed to change as part of the development plans but will be reconstructed and widened to accommodate separate left turn and right turn / shared through lanes exiting the site. Additionally, multiple access points are proposed to Parkway Drive which also currently provides access to I-96 BL.

The study section of I-96 BL and all I-96 ramp terminals are under the jurisdiction of the Michigan Department of Transportation (MDOT). All other study roadways are under the jurisdiction of the Livingston County Road Commission (LCRC), except Parkway Drive which is private. In accordance with Section 1.2.4 of the MDOT *Geometric Design Guidance* a TIS is required for permitting of site access. Additionally, although no access is proposed to LCRC right-of-way, the potential impacts of the project at study intersections including I-96 BL & Latson Road may require LCRC involvement. The project is also subject to review by Genoa Township and their consultants through their site plan approval process.

The purpose of this study is to identify the traffic related impacts, if any, of the proposed project on the adjacent road network. This study therefore includes analysis of the proposed site access points as well as key off-site intersections surrounding the site. Analysis of the site access point will determine appropriate lane configurations as well as traffic control to safely and efficiently process site traffic. Key off-site intersections were analyzed to determine if new site-generated traffic passing through these locations would require improvements to mitigate any impacted traffic operations.

The scope of this study was developed based on Bergmann's knowledge of the study area, understanding of the development program, accepted traffic engineering practice and information published by the Institute of Transportation Engineers (ITE). Additionally, Bergmann solicited input regarding the proposed scope of work from MDOT and LCRC. The study analyses were completed using Synchro and SimTraffic, Version 10 traffic analysis software and in accordance with the methodologies and practices published by ITE and the applicable requirements of MDOT, Genoa Township, and LCRC. This report is intended for use by MDOT, the Township and LCRC to guide decisions related to development project approvals, access permitting, and identifying future roadway improvement needs.



Background Data

Existing Road Network

Vehicle transportation for the proposed development will be provided via I-96 BL and Latson Road. Regional transportation is provided by I-96 which has interchanges with I-96 BL ½ mile east of the subject site and Latson Road ½ mile south of I-96 BL. The study intersections are identified below and further details on the study network are summarized in **Table 1**.

Study Intersections

- I-96 BL & Latson Road (signalized);
- I-96 BL & Lawson Drive (signalized);
- I-96 BL & WB I-96 off-ramp (signalized);
- I-96 BL & Arundell Avenue / Site Access (unsignalized);
- I-96 BL & Parkway Drive (unsignalized);
- Latson Road & Grand Oaks Drive (signalized);
- Latson Road & I-96 WB Ramps (signalized);
- Latson Road & I-96 EB Ramps (signalized);

Table 1: Roadway Summary

Roadway Data	I-96 BL	Latson Road	Grand Oaks Dr.	Arundell Ave.	Parkway Dr.
Functional Class	Principal Arterial	Minor Arterial	Local	Local	Local
Direction	E-W	N-S	N-S / E-W	N-S	N-S
Speed Limit (mph)	50	50	45	25	25
Jurisdiction	MDOT	LCRC	LCRC	Private	Private
Cross Section	5-Lane	5-Lane	2-Lane	2-Lane	2-Lane
AADT	31,000	22,000	6,240	1,400	1,000
AM Peak Hour Volume	2,083	1,490	532	32	116
PM Peak Hour Volume	3,170	2,228	624	138	91

Existing Traffic Counts

Existing traffic volume data were collected at the study intersections by Bergmann subconsultant Traffic Data Collection, LLC (TDC). Turning movement counts were initially collected on Thursday, November 7th, 2019 during typical weekday AM (7:00 AM to 9:00 AM) and PM (4:00 PM to 6:00 PM) peak periods. On the morning of November 7th, adverse weather and roadway conditions resulted in several crashes along EB I-96 near Howell, including a fatal crash. As a result, EB I-96 between Pinckney Road and Latson Road was closed for several hours of the morning with traffic detoured to Pinckney Road and I-96 BL. As this closure has direct impacts on the study intersections, AM peak hour traffic counts were recollected on Wednesday, November 13th, 2019 and were utilized in this study.

Data were collected in 15-minute intervals to establish the current peak hour traffic volumes and peak hour factors (PHF). Counts were collected while schools were in session and major weather events, holidays, and other local special events were avoided. During collection of the manual intersection turning movement counts, pedestrian data and commercial truck percentages were recorded and used in the traffic analysis. In order to establish baseline 'Existing Conditions' volumes for this study, peak



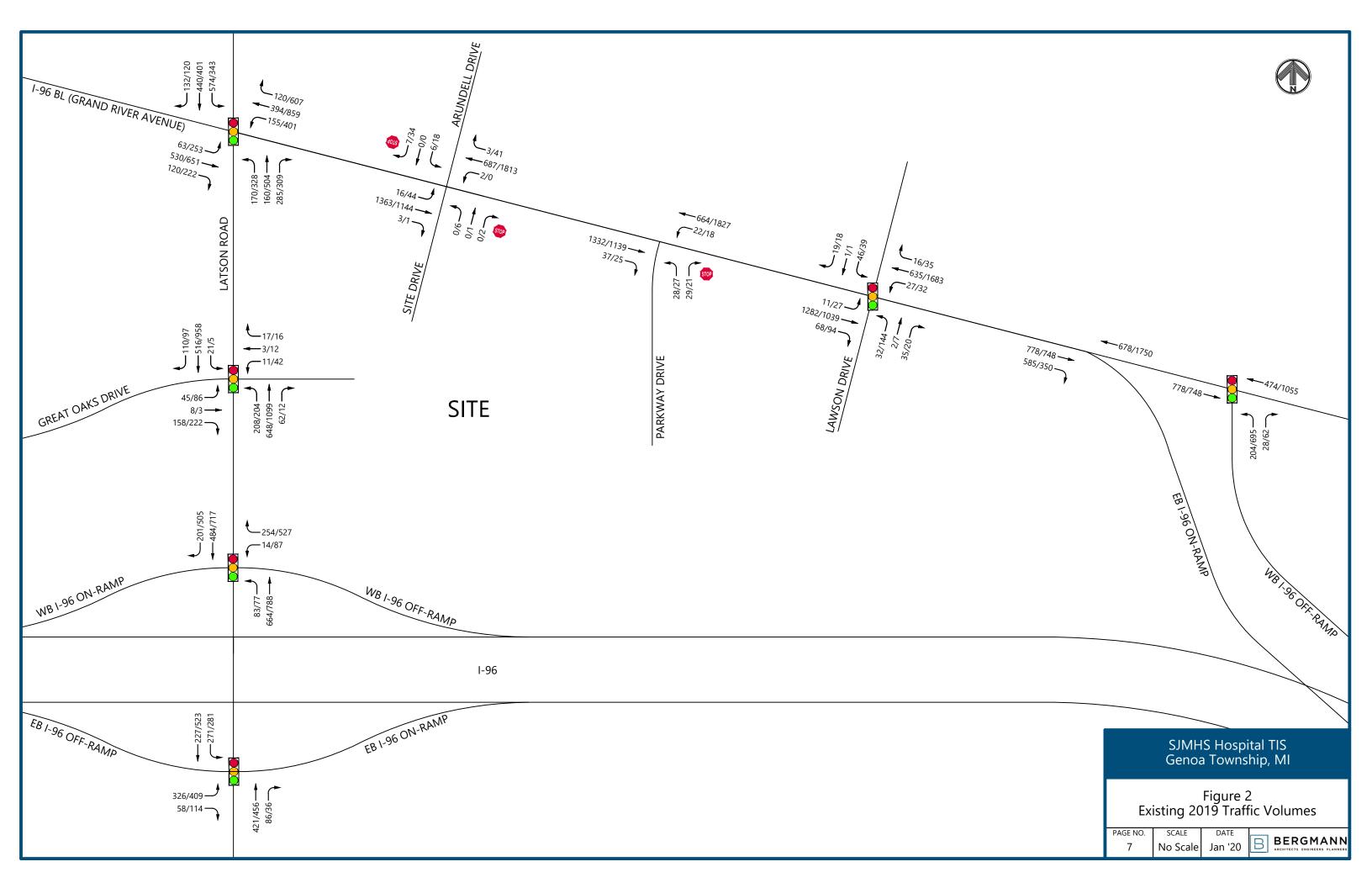
hour volumes for each individual intersection were identified and utilized based on the data collected. Specific traffic generators were also identified as sink / source locations and modeled as dummy nodes between each study intersection, and thru traffic volumes were balanced upward across the network.

Seasonal adjustment factors published by the Southeast Michigan Council of Governments (SEMCOG) for the SEMCOG region indicate adjustment factors below 1.0 (downward seasonal adjustment) for traffic counts collected on a Tuesday, Wednesday, or Thursday in the month of November. To validate the current traffic counts, a traffic volume comparison was conducted along Latson Road during the peak hours to determine if any seasonal volume adjustments are necessary.

Traffic volume data from Tuesday, September 12th, 2017 and Thursday, August 1st, 2019 were obtained from the Versa Development TIS completed by F&V for the study intersections along Latson Road and compared with the count data collected. The results of the comparison are summarized in **Table 2** below and indicate that current traffic volumes are similar with the previous traffic volumes collected in August and September. Based on these results, no adjustments to existing traffic volumes were determined necessary to account for seasonal adjustments. Upward balancing of the network also accounts conservatively for some minor variations in relative traffic volumes. The traffic volume data are included in **Appendix A** and the resulting 2019 existing traffic volumes are shown on **Figure 2**.

Table 2: Traffic Volume Comparison

		A	M Peak Hou	ır	P	M Peak Hou	ır
Intersection	Approach	Vers	a TIS	SJMHS TIS	Vers	a TIS	SJMHS TIS
		09/12/17	08/01/19	11/13/19	09/12/17	08/01/19	11/07/19
	NB	723		615	1117		1141
I-96 BL &	SB	1150		1146	801		864
_	EB	711		713	1172		1126
Latson Road	WB	682		669	1921		1867
	Total	3266		3143	5011		4998
Latson Road	NB		730	747		841	865
& I-96 WB	SB		743	685		1190	1222
	WB		217	268		547	614
Ramps	Total		1690	1700		2578	2701
Latson Road	NB		436	507		524	492
	SB		506	498		724	804
& I-96 EB	EB		414	384		468	523
Ramps	Total		1356	1389		1716	1819



Existing Conditions Analysis

Existing 2019 Traffic Conditions

Existing peak hour vehicle delays and Levels of Service (LOS) were calculated at the study intersections based on the existing lane configurations and traffic control, the existing traffic volumes shown on **Figure 2**, and the methodologies presented in the *Highway Capacity Manual*, 6th Edition (HCM6).

Typically, LOS D is considered acceptable, with LOS A representing minimal delay, LOS F indicating failing conditions, and LOS E representing conditions where demands are approaching capacity. Simulations of the study network were also observed using SimTraffic, in order to identify potential issues related to vehicle queuing, traffic flow between intersections, and the overall study network.

At the intersection of I-96 BL & Latson Road, all approaches have dynamic no turn on red restrictions where a dynamic regulatory sign displays a "NO TURN ON RED" message for right turning vehicles during the protected phase of the conflicting left turn movement only. As SimTraffic cannot replicate a dynamic no turn on red, right turns on red were assumed prohibited at all times for all approaches as a conservative approach. The HCM6 methodology calculations assume a right-turn-on-red flow rate of zero vehicles which also conservatively accounts for the dynamic no turn on red restrictions.

The results of the analysis of existing conditions are presented in **Appendix B**, summarized in **Table 3** and described in further detail below. The results of the existing conditions analysis indicate that all study intersection approaches and movements currently operate acceptably at a LOS D or better during both peak hours with the exception of the following:

- The signalized intersection of I-96 BL & Latson Road which currently operates at a LOS E during the PM peak hour. Additionally, several individual movements currently operate at a LOS E or F during both peak hours.
- The STOP controlled approaches of Arundell Avenue and the Grand River Golf Range & Putt Putt drive with I-96 BL which currently operate at a LOS F during both peak hours.
- The STOP controlled left turn movement from Parkway Drive to I-96 BL which currently operates at a LOS E during both peak hours.

Review of network simulations indicates generally acceptable traffic operations during the AM peak hour with vehicle queues at signalized intersections processed during each signal cycle. During the PM peak hour, a long vehicle queue is observed for the WB right-turn movement at the intersection of I-96 BL & Latson Road which frequently exceed available storage length. Additionally, moderate vehicle queues and cycle failures are observed for several other movements at the intersection. At all other study intersections acceptable traffic operations are observed and vehicle queues are processed during each signal cycle. At the STOP controlled intersections of Arundell Avenue and Parkway Drive, the adjacent signalized intersections along I-96 BL provide gaps in traffic along I-96 BL to facilitate egress movements. As a result, moderate vehicle queues are observed with 95th percentile queue lengths for these STOP controlled approaches calculated to be 89 feet (4 vehicles) or less.



Table 3: Existing 2019 Traffic Conditions

		ΑI	И Peak Н	lour			P۱	/I Peak H	lour	
Intersection	Арр	roach	4	Î	~	Арр	roach	4	Ť	~
1. I-96 BL & Latson Road	EB	32.7 C	43.8 D	33.7 C	22.6 C	EB	42.7 D	45.7 D	47.0 D	26.7 C
	WB	32.4	55.3	29.1	13.7	WB	65.2	55.0	50.3	93.0
Signalized		C 56.3	44.6	C 36.8	B 74.3		52.0	D 89.5	D 41.2	F 30.0
	NB	E	D	D	E	NB	D	F	D	C 25.1
<u> </u>	SB	36.0 D	43.2 D	29.9 C	25.0 C	SB	74.0 E	133.3 F	37.8 D	25.1 C
	0١	erall	38.5	LOS	D	Ov	erall	58.4	LOS	Е
2. I-96 BL & Lawson Drive	EB	4.9 A	2.6 A	5.0 A	2.7 A	ЕВ	5.1 A	19.1 B	4.8 A	3.4 A
	WB	0.6 A	2.5 A	0.5 A	0.5 A	WB	10.2 B	7.4 A	10.2 B	10.2 B
Signalized	NB	39.6	40.2	39	9.2	NB	46.1	47.9	3!	5.9
		D 41.9	D 43.5		D 3.4		D 37.3	D 38.0		D 5.8
	SB	D	D)	SB	D	D		D _
3. I-96 BL & WB I-96 Off-Ramp	01	erall 0.4	6.2	LOS 0.4	Α	OV	erall 22.2	10.8	22.2	В
3. 1-96 BL & WB 1-96 OH-Ramp	EB	Α		Α		EB	С		С	
Signalized	WB	3.7 A		3.7 A		WB	12.5 B		12.5 B	
	NB	39.2 D	39.6 D		36.8 D	NB	32.9 C	33.5 C		26.0 C
	O۱	erall	7.3	LOS	Α	Ov	erall	21.0	LOS	С
4. Latson Road & Grand Oaks Drive / Ascension Medical Drive	EB	36.4 D	30.3 C		3.0 O	EB	31.9 C	26.8 C		3.9 C
Ascension Medical Drive	WB	31.7	37.9	28	3.4	WB	31.8	37.1	24	1.0
Signalized	NB	2.4	D 8.1	0.8	0.7	NB	3.6	D 14.7	1.6	1.5
		A 14.2	A 10.9	A 14.3	A 14.3		A 24.7	B 13.4	A 24.8	A 24.7
•	SB	В	В	В	В	SB	С	В	С	С
5 L L D L D L D C L L D D	٥١	erall	11.3	LOS	B	Ov	erall	16.0	LOS	B
5. Latson Road & I-96 WB Ramps	WB	36.3 D	30.5 C		36.6 D	WB	33.2 C	24.8 C		34.6 C
Signalized	NB	0.5 A	2.1 A	0.3 A		NB	1.1 A	6.8 A	0.5 A	
	SB	13.2 B		13.2 B	13.1 B	SB	21.5 C		19.2 B	24.6 C
•	O۱	erall	10.8	LOS	В	Ov	erall	17.5	LOS	В
6. Latson Road & I-96 EB Ramps	EB	34.3 C	34.9 C		31.1 C	EB	32.6 C	33.2 C		30.5 C
<u>.</u>	NB	4.6		4.6	4.2	NB	5.6		5.6	4.9
Signalized	SB	A 2.3	4.1	0.1	Α	SB	1.3	3.3	0.3	Α
		A rerall	A 11.6	LOS	В		A erall	A 11.6	LOS	В
7. I-96 BL & Arundell Avenue / Site		0.1	9.4				0.8	20.6		
Drive	EB	Α	Α	Fr	ee	EB	Α	С	Fr	ee
Minor STOP	WB	0.0 A	13.4 B		ee	WB	0.0 A	0.0 A		ee
STOP	NB	0.0 A		0.0 A		NB	630.7 F		630.7 F	
	SB	40.9 E		5.3 F	11.4 B	SB	871.0 F		71.8 F	23.5 C
8. I-96 BL & Parkway Drive	EB			ee		EB			ee	ŭ
	WB	0.5	14.1	Fr	ee	WB	0.1	11.7	Fr	ee
Minor STOP STOP	NB	31.5	B 46.5		17.0	NB	A 29.5	B 41.8		13.7
3101	IND	D	E		С	ND	D	E		В

Background Conditions

Background 2024 Traffic Volumes

Traffic impact studies typically include an evaluation of traffic operations in the future as they would be without the proposed development. This "background" condition serves to identify any mitigation that may be required regardless of the project, and as a baseline for comparison of future buildout conditions. This scenario is comprised of existing traffic conditions plus ambient traffic growth plus traffic from approved developments in the study area that have yet to be constructed. At the time of this study the following developments were identified within the study area and immediate vicinity that have yet to be constructed or are currently under construction:

- 1. Versa Mixed-Use Development
- 2. Westbury Phase II Residential Development

The vehicle trips that would be generated by the background developments were assigned to the study intersections based on the respective traffic study completed for each development. Where a traffic study was not completed for the development or the traffic study did not include the same intersections as this study, the number of vehicle trips was forecast based on data published by ITE in *Trip Generation*, 10th Edition and assigned to the study road network based on existing traffic patterns.

In addition to background developments, an ambient growth factor is applied to existing traffic volumes to account for future projects in the study area and population increases, as well as growth in regular traffic volumes due to development projects outside the study area. The recent construction of the I-96 & Latson Road interchange has resulted in significant changes in traffic patterns throughout the study area. As a result, historical traffic volumes do not provide an accurate representation of traffic growth in the area. Therefore, publicly available data from the Southeast Michigan Council of Governments (SEMCOG), including population and employment forecasts for Genoa Township were referenced.

Table 4: Community Annual Growth Summary

Community Measure Growth

Community	Measure	Growth
Genoa Township	Employment	0.38%
Genoa Township	Population	1.54%
Howell	Employment	0.35%
nowell	Population	0.57%
Prighton	Employment	0.47%
Brighton	Population	0.69%
Prighton Township	Employment	0.63%
Brighton Township	Population	1.64%
AVERAG	GE	0.78%

The SEMCOG data indicates annual population and employment growths ranging from 0.35% to 1.64% between 2015 and 2045 as shown in **Table 4**. Therefore, an ambient background growth rate of 1.0% per year was utilized for this study and was agreed upon by MDOT. The ambient growth rate and trips from the background developments were applied to the existing 2019 traffic volumes to forecast the



future 2024 background traffic volumes **without the proposed development.** The resultant background traffic volumes are summarized on **Figure 3**.

Background 2024 Traffic Conditions

Background peak hour vehicle delays and LOS were calculated at the study intersections based on existing lane configurations and traffic control, the background traffic volumes shown on **Figure 3**, and the methodologies presented in the *HCM*. The results of the analysis of background conditions are presented in **Appendix C**, summarized in **Table 5**, and described in further detail below.

The results of the background conditions analysis indicate that all approaches and movements at the study intersections will continue to operate acceptably at a LOS D or better during both peak hours with the exception of the following:

- The signalized intersection of I-96 BL & Latson Road which will continue to operate at an overall LOS E during the PM peak hour. Additionally, several individual movements will continue to operate at a LOS E or F during both peak hours.
- The NB left-turn movement at the signalized intersection of Latson Road & WB I-96 ramps which will operate at a LOS F during the PM peak hour.
- The STOP controlled approaches of Arundell Avenue and the Grand River Golf Range & Putt Putt drive with I-96 BL which will continue to operate at a LOS F during both peak hours.
- The STOP controlled left turn movement from Parkway Drive to I-96 BL which will operate at a LOS F during both peak hours.

Review of network simulations continues to indicate acceptable traffic operations during the AM peak hour with vehicle queues at signalized intersections typically processed during each signal cycle. During the PM peak hour, long vehicle queues and frequent cycle failures occur with increased frequency at the intersection of I-96 BL & Latson Road, as compared to existing conditions. Specifically, a long vehicle queue is observed for the WB right-turn movement which exceeds available storage length throughout the majority of the peak hour. It should be noted that these operations reflect modeled prohibition of all right turns on red at this intersection. The dynamic case signs allow for additional right turn demand processing and would result in shorter queues for actual field conditions.

Additionally, long vehicle queues are observed for the left-turn movements from Latson Road to the EB and WB I-96 on-ramps which exceed available storage length and spill back into the through lanes along Latson Road. At all other signalized study intersections acceptable traffic operations are observed and vehicle queues are processed during each signal cycle. At the STOP controlled intersections of Arundell Avenue and Parkway Drive, gaps in the I-96 BL traffic stream during the PM peak hour to facilitate egress movements. As a result, long vehicle queues are observed with average vehicle queue lengths for Arundell Avenue and Parkway Drive calculated to be 193 feet (8 vehicles) and 115 feet (5 vehicles), respectively.

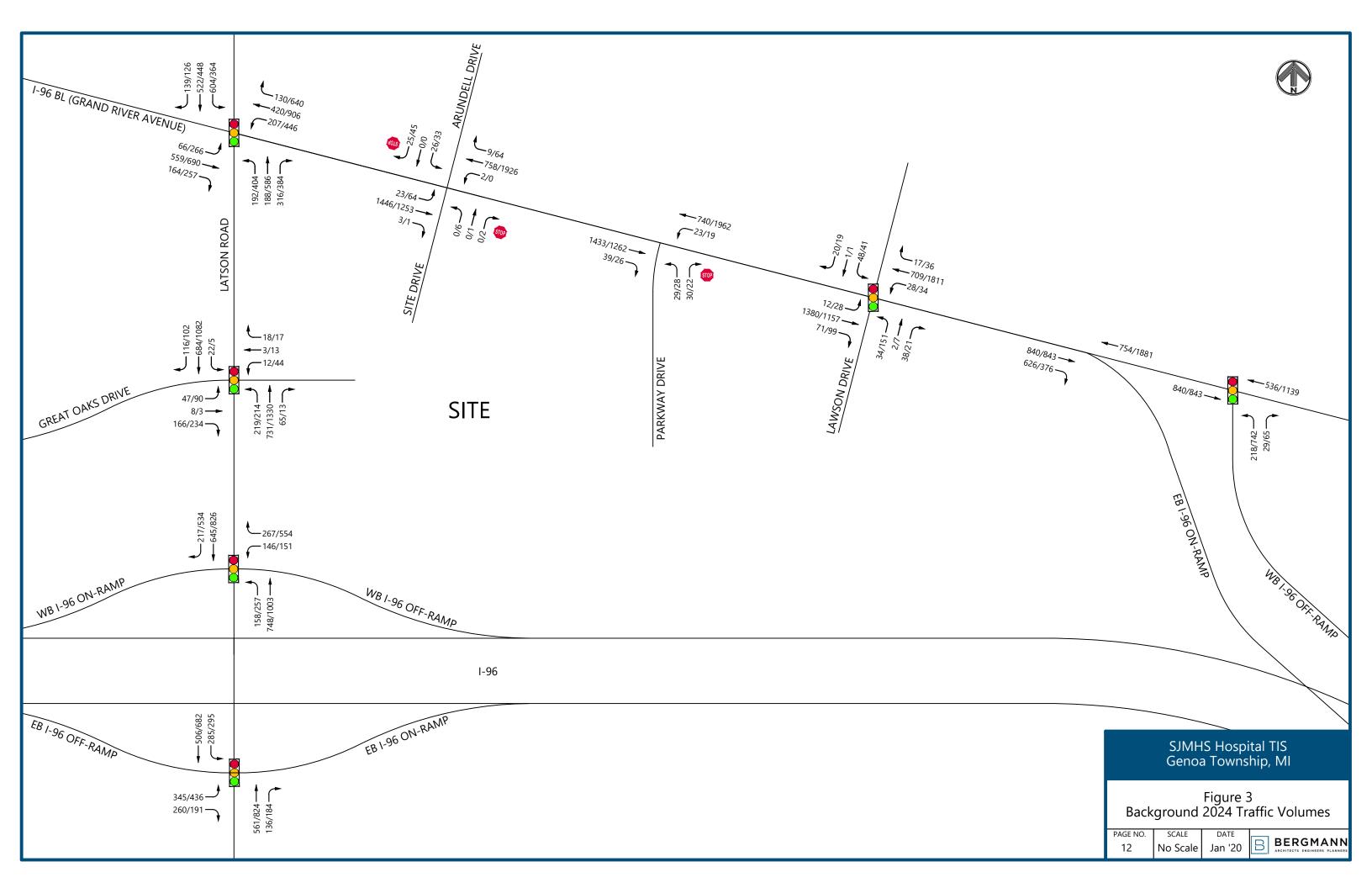


Table 5: Background 2024 Traffic Conditions

		AM Peak I	Hour			DI	1 Peak F	lour	
Intersection	Approacl		†	<u>م</u>	Арр	roach	†	1	~
1. I-96 BL & Latson Road	EB 33.	8 43.8	35.6	23.7	EB	47.7	46.5	55.2	28.6
	С		D	C	ED	D	D	E	С
	WB 48.		30.3	13.9	WB	85.6 F	73.8 E	65.9 F ¹	121.7
Signalized	D 68.		C 37.7	B 101.7		80.1	168.5	48.3	F 35.8
Signalized	NB E		D	F	NB	F	F	D	D
	SB 37.	0 44.3	31.8	25.3	SB	84.5	159.8	40.0	24.9
_	- D		С	C		F	F	D	С
2 + 05 21 0 + 2	Overall	45.3	LOS	D	Ov	erall	75.1	LOS	E 2.4
2. I-96 BL & Lawson Drive	EB 5.2		5.4 A	2.8 A	EB	5.5 A	24.2 C	5.2 A	3.4 A
	0.7		0.6	0.6		12.0	8.5	12.1	12.1
	WB A	Α	Α	Α	WB	В	Α	В	В
Signalized	NB 39.			9.4	NB	48.0	50.3	35	
•	D 42.	_		D 3.5		D 37.4	D 38.2	35	
<u> </u>	SB 42.).5)	SB	57.4 D	36.2 D	33	
	Overall	6.4	LOS	Α	Ov	erall	12.0	LOS	В
3. I-96 BL & WB I-96 Off-Ramp	EB 0.4		0.4		EB	17.4		17.4	
	A		A 2.0			B		B	
Signalized	WB 3.8		3.8 A		WB	14.3 B		14.3 B	
Signalized	39		7.	36.8		32.5	33.2		24.9
	NB D	_		D	NB	C	C		С
	Overall	7.3	LOS	Α	Ov	erall	20.3	LOS	C
4. Latson Road & Grand Oaks Drive /	EB 36.			3.1	EB	31.8 C	26.3 C	33	
Ascension Medical Drive	31	_		7.9		31.5	37.0	23	
	WB C	D	(C	WB	С	D	(3
Signalized	NB 2.9		0.9	0.9	NB	9.1	20.3	7.4	7.3
•	A	_	A	A		A 21.6	C	A 21.7	A
<u> </u>	SB 16.	9 11.6 B	17.1 B	17.1 B	SB	31.6 C	14.1 B	31.7 C	31.7 C
l i	Overall	12.5	LOS	В	Ov	erall	20.7	LOS	С
5. Latson Road & I-96 WB Ramps	WB 35.			35.5	WB	32.6	25.3		34.6
	D 1.6		0.4	D		C 35.2	C 169.3	0.8	С
Signalized	NB A		0.4 A		NB	33.2 D	109.5 F	0.6 A	
	14.	_							
		Ö	15.1	13.9	CD	22.8	r	20.7	25.9
I <mark>ŏ</mark>	SB B		В	В	SB	C		20.7 C	С
<u>•</u>	Overall	12.7		В В		C erall	29.5	20.7	C C
6. Latson Road & I-96 EB Ramps	Overall FB 31.	12.7 7 27.5	В	B B 37.4		C erall 32.8	29.5 32.4	20.7 C	C C 33.6
6. Latson Road & I-96 EB Ramps	Overall B 31. EB C	12.7 7 27.5	В	В В	Ov EB	C erall	29.5	20.7 C	C C
6. Latson Road & I-96 EB Ramps Signalized	Overall EB 31.	12.7 7 27.5 C	7.9 A	B 37.4 D	Ov	C erall 32.8 C 7.0 A	29.5 32.4	20.7 C LOS	C 33.6 C
	Overall EB 31. C NB 7.8 A SB 7.8	7 27.5 C	7.9 A	B 37.4 D 7.2	Ov EB	C erall 32.8 C 7.0 A 11.3	29.5 32.4 C	20.7 C LOS 7.2 A 0.4	C 33.6 C 6.2
	Overall EB 31. NB 7.8 A SB A	7 27.5 C 3 21.1 C	7.9 A 0.3 A	B 37.4 D 7.2 A	EB NB SB	C erall 32.8 C 7.0 A 11.3 B	29.5 32.4 C 36.6 D	20.7 C LOS 7.2 A 0.4 A	C 33.6 C 6.2 A
	Overall EB 31. EB 7.8 NB 7.8 A SB 7.8 Overall	12.7 7 27.5 C 8 21.1 C 14.4	7.9 A 0.3 A	B 37.4 D 7.2 A B	Ov EB NB SB	C erall 32.8 C 7.0 A 11.3	29.5 32.4 C	7.2 A 0.4 A LOS	C 33.6 C 6.2 A
Signalized	Overall EB 31. C NB 7.8 SB 7.8 A Overall	12.7 7 27.5 C 8 21.1 C 14.4 9.8	7.9 A 0.3 A	B 37.4 D 7.2 A	EB NB SB	C	29.5 32.4 C 36.6 D	20.7 C LOS 7.2 A 0.4 A	C 33.6 C 6.2 A
Signalized 7. I-96 BL & Arundell Avenue / Site Drive	B S S S S S S S S S	7 27.5 C 3 21.1 C 14.4 2 9.8 A 0 14.1	7.9 A 0.3 A LOS	B 37.4 D 7.2 A B	Ov EB NB SB	C erall 32.8 C 7.0 A 11.3 B erall 1.2 A 0.0	36.6 D 15.1 25.7 D	7.2 A 0.4 A LOS	C 33.6 C 6.2 A
Signalized 7. I-96 BL & Arundell Avenue / Site Drive Minor STOP	NB 7.8 SB 7.8 A Overall EB 0.2 A WB 0.0 A	7 27.5 C 3 21.1 C 14.4 2 9.8 A 0 14.1 B	7.9 A 0.3 A LOS	B B 37.4 D 7.2 A B	Ov EB NB SB Ov EB	C erall 32.8 C 7.0 A 11.3 B erall 1.2 A 0.0 A	29.5 32.4 C 36.6 D 15.1 25.7 D	7.2 A 0.4 A LOS	33.6 C 6.2 A B
Signalized 7. I-96 BL & Arundell Avenue / Site Drive	B S S S S S S S S S	7 27.5 C 3 21.1 C 14.4 2 9.8 A 0 14.1 B	7.9 A 0.3 A LOS	B B 37.4 D 7.2 A B	Ov EB NB SB Ov EB	C erall 32.8 C 7.0 A 11.3 B erall 1.2 A 0.0	36.6 D 15.1 25.7 D	7.2 A 0.4 A LOS	33.6 C 6.2 A B
Signalized 7. I-96 BL & Arundell Avenue / Site Drive Minor STOP	NB A Overall EB 7.8 SB A A Overall EB 0.2 A WB 0.0 A NB A	7 27.5 C 3 21.1 C 14.4 2 9.8 A 0 14.1 B 0 24	7.9 A 0.3 A LOS Fr 6.0 A 3.2	B B 37.4 D 7.2 A B B eee	Ov EB NB SB Ov EB WB	Cerall 32.8 C 7.0 A 11.3 B erall 1.2 A 0.0 A 1298.8 F	36.6 D 15.1 25.7 D 0.0 A	7.2 A 0.4 A LOS Fr. 1298.8 F	33.6 C 6.2 A B
Signalized 7. I-96 BL & Arundell Avenue / Site Drive Minor STOP	NB 0.0 A NB	7 27.5 C 3 21.1 C 14.4 2 9.8 A 0 14.1 B 0 24	7.9 A 0.3 A LOS Fr 0.0 A	B B 37.4 D 7.2 A B B ee	Ov EB NB SB Ov EB	C erall 32.8 C 7.0 A 11.3 B erall 1.2 A 0.0 A 1298.8 F	36.6 D 15.1 25.7 D 0.0 A	7.2 A 0.4 A LOS Fr. 1298.8 F	C 33.6 C 6.2 A B ee
Signalized 7. I-96 BL & Arundell Avenue / Site Drive Minor STOP	NB A Overall EB 7.8 SB A A Overall EB 0.2 A WB 0.0 A NB A	7 27.5 C 3 21.1 C 14.4 2 9.8 A 0 14.1 B 0 24	7.9 A 0.3 A LOS Fr 6.0 A 3.2	B B 37.4 D 7.2 A B B eee	Ov EB NB SB Ov EB WB	Cerall 32.8 C 7.0 A 11.3 B erall 1.2 A 0.0 A 1298.8 F	36.6 D 15.1 25.7 D 0.0 A	7.2 A 0.4 A LOS Fr. 1298.8 F 4.7	C 33.6 C 6.2 A B B ee ee 28.2
Signalized 7. I-96 BL & Arundell Avenue / Site Drive Minor STOP	NB A A NB A A SB SB A A A A A SB B A A A A A A A	7 27.5 C 3 21.1 C 14.4 2 9.8 A 0 14.1 B 0 24	7.9 A 0.3 A LOS Fr 0.0 A 3.2 Feee	B B 37.4 D 7.2 A B B ee ee	Ov EB NB SB Ov EB WB NB SB	C erall 32.8 C 7.0 A 11.3 B erall 1.2 A 0.0 A 1298.8 F 3517.1 F	29.5 32.4 C 36.6 D 15.1 25.7 D 0.0 A	7.2 A 0.4 A LOS Fr. 1298.8 F 44.7	C 33.6 C 6.2 A B eee ee 28.2 D
Signalized 7. I-96 BL & Arundell Avenue / Site Drive Minor STOP	NB AA SB AA WB AA NB AA	12.7 7 27.5 C 3 3 21.1 C 14.4 2 9.8 A 0 14.1 B 0 24	7.9 A 0.3 A LOS Fr 0.0 A 3.2 Feee	B B 37.4 D 7.2 A B B eee	OV EB NB SB OV EB WB NB	Cerall 32.8 C 7.0 A 11.3 B erall 1.2 A 0.0 A 1298.8 F	36.6 D 15.1 25.7 D 0.0 A	7.2 A 0.4 A LOS Fr. 1298.8 F 44.7	C 33.6 C 6.2 A B B ee ee 28.2
Signalized 7. I-96 BL & Arundell Avenue / Site Drive Minor STOP \$10P 8. I-96 BL & Parkway Drive	NB	7 27.5 C 3 21.1 C 14.4 P 9.8 A 0 14.1 B 0 24 Fr 5 15.3 C 2 58.8	7.9 A 0.3 A LOS Fr 0.0 A 3.2 Feee	B B 37.4 D 7.2 A B B ee ee	Ov EB NB SB Ov EB WB NB SB	C erall 32.8 C 7.0 A 11.3 B erall 1.2 A 0.0 A 1298.8 F 3517.1 F	29.5 32.4 C 36.6 D 15.1 25.7 D 0.0 A	7.2 A 0.4 A LOS Fr. 1298.8 F 44.7	C 33.6 C 6.2 A B eee ee 28.2 D

1. V/C ratio greater than 1.0.

Future Conditions Analysis

Site-Generated Traffic

Trip Generation

The number of Weekday AM and PM peak hour vehicle trips that would be generated by the proposed development was forecast based on data published by ITE in *Trip Generation*, 10th Edition. The ITE land use categories that most closely match the operation of the proposed facility are Land Use #610, Hospital, and Land Use #720, Medical Office Building. For Land Use #610, trip generation data is published by Gross Square Feet (GSF), number of beds, and number of employees. Review of the statistical data published by ITE for each independent variable as summarized in **Table 6** indicates that the number of employees provides the strongest correlation to trip-making potential. Therefore, as the number of employees for the Hospital is known, it was selected as the independent variable for trip generation calculations.

Table 6: Land Use #610 Statistical Summary

Land Use	Time Period	Independent Variable	# of Studies	Standard Deviation	R^2
		Beds	9	1.01	0.88
	AM	GSF	20	0.5	0.86
#610		Employees	17	0.1	0.96
Hospital		Beds	7	0.92	0.91
	PM	GSF	19	0.6	0.88
		Employees	14	0.13	0.96

For Land Use #720, trip generation data is published by GSF and number of employees; however, as the number of employees specific to the MOB is not known, GSF was utilized. The resulting site trip generation forecast is summarized in **Table 7** and was agreed upon by MDOT.

Table 7: Site Trip Generation

Land Use	ITE	Amount	Units	Average	AM	Peak I	lour	PM	Peak H	lour
Lailu Ose	Code	Amount	Ullits	Daily	ln	Out	Total	ln	Out	Total
Hospital	610	502	Employees	2,675	171	63	234	57	153	210
Medical Office Building	720	50,000	SF	1,833	94	27	121	48	124	172
TOTAL				4,508	265	90	355	105	277	382

Traffic Assignments

The vehicle trips that would be generated by the proposed development were assigned to the study road network utilizing a gravity model. A gravity model is a mathematical model used to estimate the number of trips that will be drawn to a development based on population and travel time within a distinct area of influence. For the proposed facility, the area of influence was defined as a 30-minute travel time to the site. The population distribution within the area of influence was determined based on US Census data and separated into zones by City / Township. Each zone was then factored based on travel time information obtained from Google and assigned to the area road system utilizing the following assumptions:

1. All trips to/from north of M-59 were assumed to utilize M-59 to Latson Road to I-96 BL.



- 2. All trips to/from east and south of Brighton were assumed to arrive/depart via the freeway system and would utilize the I-96 & I-96 BL interchange.
- 3. All trips to/from west of Howell were assumed to arrive/depart via the freeway system and would utilize the I-96 & Latson Road interchange.
- 4. Local trips (non-freeway) were assigned to I-96 BL and Latson Road based on proximity of each zone to these roadways.

The resulting directional distributions for site-generated traffic are summarized in **Table 8**. The trip distribution model was also validated based on patient zip code data provided by SJMHS and was agreed upon by MDOT.

To/From Via AM/PM North Latson Road 15% South Latson Road 5% I-96 BL (Grand River Avenue) 10% East 40% I-96 I-96 BL (Grand River Avenue) 10% West I-96 20% **TOTAL** 100%

Table 8: Site Trip Distribution

Utilization of the site driveways to I-96 BL and Parkway Drive was determined based on the direction of origin and destination relative to the site, and proposed site layout including location of the proposed Hospital and MOB and proximity to available parking supplies. Traffic volumes approaching from the west on I-96 BL were assumed to predominantly (90%) utilize the main site drive aligned with Arundell Avenue. Traffic approaching from the east was also assumed to primarily (65%) use this site drive. New trips were assumed to exit via the same drive that was entered. In accordance with ITE methodologies, all trips are expected to return to their direction of origin when departing the site.

The site-generated vehicle trips were assigned to the study network as shown on **Figure 4**. These trips were added to the background traffic volumes shown on **Figure 3** to calculate the future traffic volume shown on **Figure 5**.

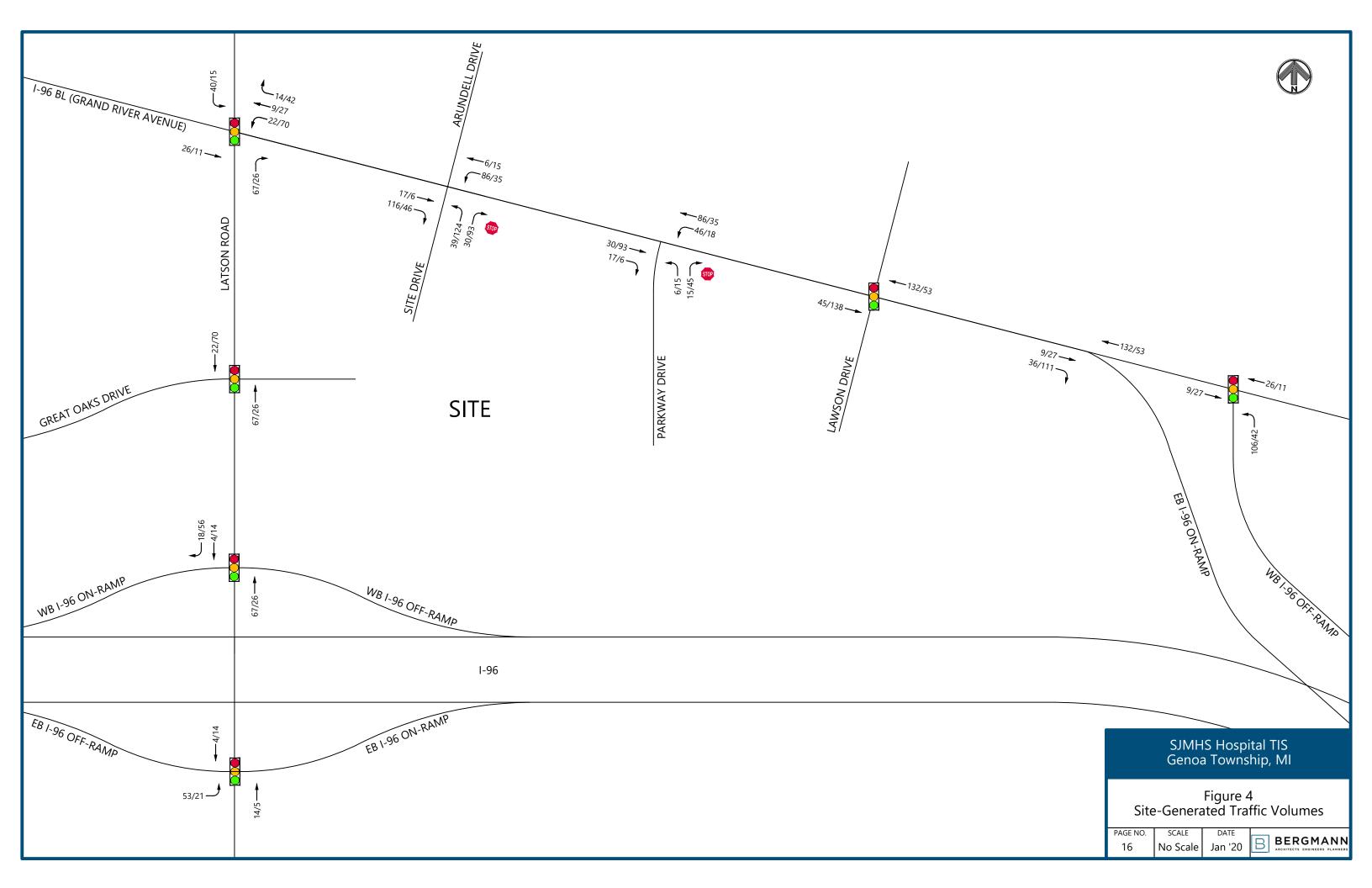
Turn Lane Warrants

Right and left-turn lanes currently exist along I-96 BL at Parkway Drive and the proposed site drive location to facilitate site-generated traffic.

Future 2024 Traffic Conditions

Future peak hour vehicle delays and LOS with the proposed development were calculated based on existing lane configurations and traffic control, future traffic volumes shown on Figure 5, and HCM methodologies. SimTraffic simulations were also utilized to evaluate traffic flow and vehicle queues throughout the study network. The results of the future conditions analysis are presented in Appendix D and summarized in Table 9.





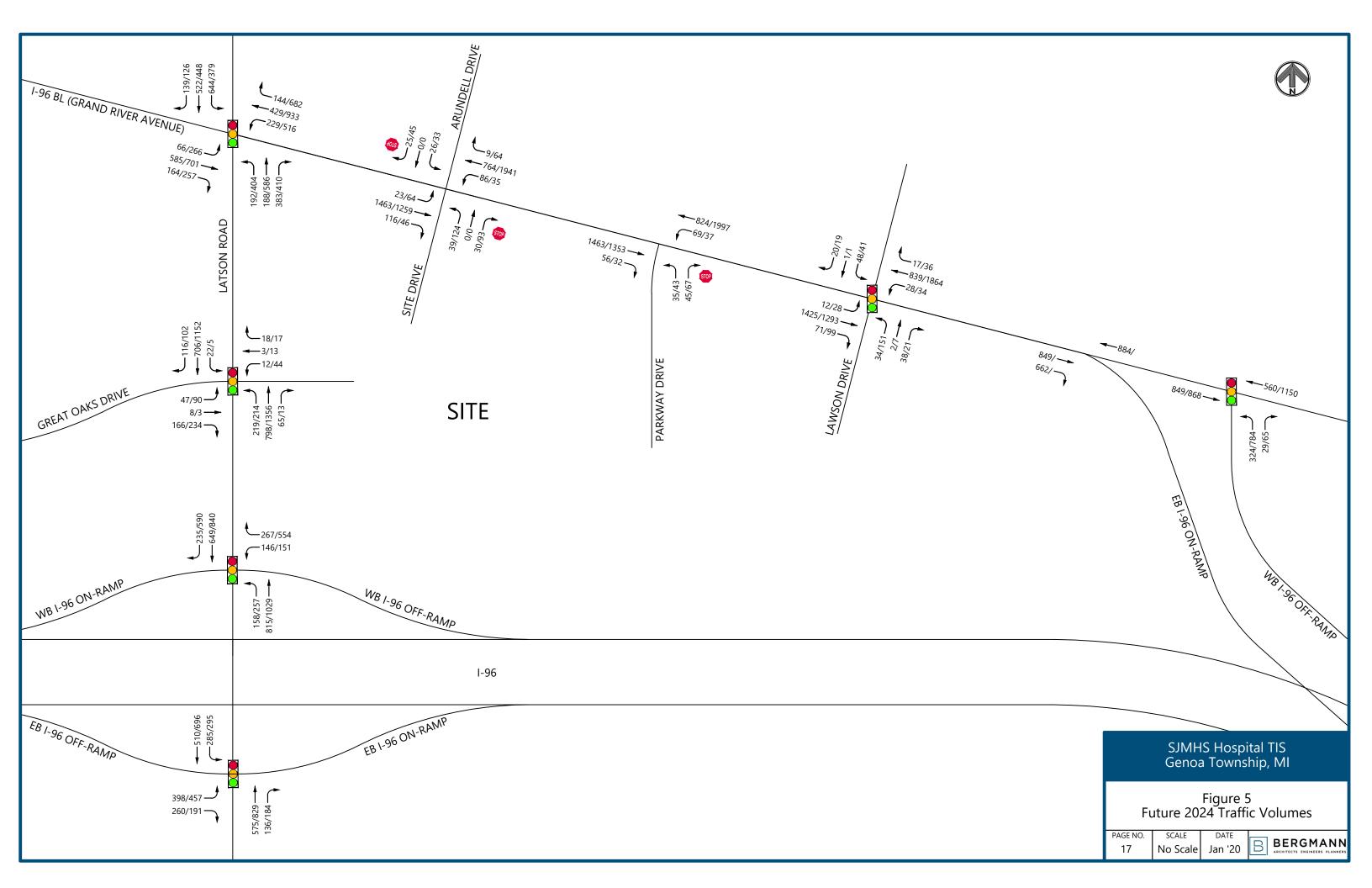


Table 9: Future 2024 Traffic Conditions

		ΔΝ	/I Peak H	lour			PΙΛ	1 Peak Ho	our	
Intersection	Арр	roach	4	1	~	Арр	roach	↑	`` ↑	Ò
1. I-96 BL & Latson Road	EB	35.8	43.8	38.1	24.4	EB	49.4	46.5	58.1	28.6
	LD	D	D	D	С	LD	D	D	E	С
	WB	60.0	142.2 F	31.4 C	14.2 B	WB	110.7	121.2	74.4 F ¹	152.5
Signalized		E 113.0	44.1	37.7	184.6		F 80.3	F 168.5	48.3	F 39.2
5.9.14.1264	NB	F	D	D	F	NB	F	F	D	D
	SB	37.8	46.3	30.9	24.7	SB	93.5	179.6	40.0	24.9
	_	D	D	C	С		F	F	D	C
2. I-96 BL & Lawson Drive	OV	erall 5.4	58.4 2.7	LOS 5.5	2.8	O.	verall 5.9	86.4 26.7	LOS 5.6	F 3.4
2.1 30 BE & Edwison Brive	EB	A	Α	A	A	EB	A	C	Α	A
	WB	8.0	3.6	0.7	0.7	WB	13.0	10.0	13.0	13.0
		Α	Α	Α	Α		В	Α	В	В
Signalized	NB	39.9 D	40.5 D		9.4 D	NB	48.0 D	50.3 D		5.9 D
	- CD	42.4	44.1		3.5	CD	37.4	38.2		5.8
•	SB	D	D		D	SB	D	D)
	Ov	erall	6.3	LOS	Α	0/	rerall	12.4	LOS	В
3. I-96 BL & WB I-96 Off-Ramp	EB	0.4 A		0.4 A		EB	18.4 B		18.4 B	
		4.6		4.6			15.4		15.4	
Signalized	WB	Α		Α		WB	В		В	
	NB	39.8	40.3		34.7	NB	32.3	33.0		24.0
	Ov	D erall	9.4	LOS	C A	O	C rerall	21.1	LOS	C C
4. Latson Road & Grand Oaks Drive /	1	36.3	29.9		3.1		31.8	26.3		3.9
Ascension Medical Drive	EB	D	С	ı	D	EB	С	С	(C
	WB	31.6	37.8		7.9	WB	31.5	37.0		3.4
Signalized		2.9	D 10.2	1.0	1.0		9.5	D 22.3	7.5	7.4
Signalized	NB	Α.	B	Α	Α	NB	9.5 A	22.3 C	Α.	Α.4
	SB	17.2	11.7	17.4	17.3	SB	35.9	14.1	36.0	36.1
		В	B	В	В		D	В -	D	D
5. Latson Road & I-96 WB Ramps	OV	erall 35.3	12.4 34.9	LOS	B 35.5	O.	verall 32.6	22.7 25.3	LOS	C 34.6
5. Latsoff Road & 1-90 WB Ramps	WB	D	C C		D	WB	C	C 25.5		C
	NB	1.6	7.7	0.4		NB	39.9	196.4	0.9	
Signalized	IND	Α	Α	Α		IND	D	F	Α	
	SB	14.9 B		15.2 B	14.3 B	SB	23.6 C		20.8 C	27.5 C
	Ov	erall	12.5	LOS	В	O۱	erall	31.5	LOS	c
6. Latson Road & I-96 EB Ramps	EB	31.5	28.1		36.8	EB	32.3	32.1		32.6
		C	С	0.1	D 7.0		C	С	7.5	C
Signalized	NB	7.9 A		8.1 A	7.2 A	NB	7.3 A		7.5 A	6.4 A
5.9.141.204	SB	8.6	23.5	0.3	,,	SB	12.3	40.5	0.4	,,
		Α	С	Α			В	D	Α	
7 L OC BL 9: A LHA / 6"!	Ov	erall	15.0	LOS	В	O۱	/erall	15.6	LOS	В
7. I-96 BL & Arundell Avenue / Site Drive	EB	0.1 A	9.8 A	Fr	ee	EB	1.2 A	26.1 D	Fr	ee
5vc	WB	1.9	19.3	F.		WB	0.2	13.5	Г.	
Minor STOP	VVD	Α	С		ree	VVD	Α	В		ee
STOP	NB	836.2 F	1465.8 F		7.7 C	NB	11200.5 F	19585.1 F		1.1 C
_		401.4	775.5		2.4		5349.4	12605.0		3.6
	SB	F	F		В	SB	F	F))
8. I-96 BL & Parkway Drive	EB		Fr	ee		EB		Free	9	
\ a=a=	WB	1.4	17.8	Fr	ee	WB	0.2	13.6	Fr	ee
Minor STOP		A 54.2	98.2		20.0		A 46.2	90.5		17.8
STOP	NB	54.2 F	98.2 F		20.0 C	NB	46.2 E	90.5 F		17.8 C
1 V/C ratio greater than 1.0	1									

1. V/C ratio greater than 1.0.

These results indicate that all approaches and movements at the study intersections will continue to operate in a manner similar to background conditions. Most approaches and movements will continue to operate acceptably at a LOS D or better during both peak hours. At the following locations, conditions will continue to worsen with the addition of site generated traffic, if those movements operating at a LOS E or F are not improved under background conditions:

- The signalized intersection of I-96 BL & Latson Road which will operate at an overall LOS E and LOS F during the AM and PM peak hours, respectively. Additionally, several individual movements will operate at a LOS E or F during the peak hours.
- The NB left-turn movement at the signalized intersection of Latson Road & WB I-96 ramps which will operate at a LOS F during the PM peak hour.
- The STOP controlled approaches of Arundell Avenue and the proposed site driveway with I-96 BL which will operate at a LOS F during both peak hours.
- The STOP controlled left turn movement from Parkway Drive to I-96 BL which will operate at a LOS F during both peak hours.

Review of network simulations indicates moderate vehicle queues for the NB right-turn movement at the intersection of I-96 BL & Latson Road and the STOP controlled approaches of Arundell Avenue and the proposed site driveway during the AM peak hour. During the PM peak hour, long vehicle queues and frequent cycle failures continue to occur (similar to background conditions) at the intersection of I-96 BL & Latson Road. It should be reiterated that these operations reflect modeled prohibition of all right turns on red at this intersection. The dynamic case signs allow for additional right turn demand processing and would result in shorter queues for actual field conditions.

Queuing and cycle failures are also observed (similar to background conditions) for the left-turn movements from Latson Road to the EB and WB I-96 on-ramps which exceed available storage length and spill back into the through lanes along Latson Road. Additionally, long vehicle queues are observed on the STOP controlled approaches of Arundell Avenue, Parkway Drive, and the proposed site driveway which last throughout the duration of the peak hour.

Future Improvements

As described above, there are several locations with undesirable or failing background conditions that are expected to worsen in the future. In order to improve future traffic operations to be similar to, or better than, the modeled background conditions, mitigation measures were investigated at the study intersections. At the intersection of I-96 BL & Latson Road, recent improvements were made as part of the I-96 & Latson Road interchange project to provide dual left-turn lanes and right turn overlap phasing on all approaches, as well as additional travel lanes along Latson Road. The intersection of I-96 BL & Latson Road is considered to be built out, and additional physical capacity improvements (i.e. additional lanes) at this intersection are constrained by available right-of-way. Therefore, further geometric improvements at the intersection to mitigate all movements to a LOS D or better are considered to be regional and beyond the scope and context of this study.

As a result, improvements at I-96 BL & Latson Road are limited to signal timing and traffic control modifications. Signal cycle length and timing changes were therefore evaluated. The results of this analysis indicate that optimized timings with a 90 second cycle length in the AM peak hour and 100 second cycle length during the PM peak hour would result in future traffic operations which are similar to or improved compared to background conditions; however, several intersection approaches and



movements would continue to operate at a LOS E or F during the peak hours (noting modeling constraints for dynamic right turn on red).

At the intersections of Latson Road with the EB and WB I-96 ramp approaches, left-turn traffic from Latson Road to the on-ramps are not provided adequate gaps during each signal cycle, resulting in excessive delays and queues. Therefore, a left turn phasing analysis was conducted at the intersection in accordance with the MDOT left turn phasing guidelines and spreadsheet. The results of this analysis indicate that both intersections meet several of the criteria to consider protected left-turn phasing. Therefore, permissive-protected left-turn phasing is recommended at both intersections. As a large amount of future traffic at these intersections will be generated by the Versa development which has a 20-year buildout, MDOT and LCRC should coordinate with the Versa development to determine when the left-turn phasing should be installed.

As shown in **Table 9**, site egress from the proposed site driveway and Parkway Drive will operate at a LOS F under STOP control. Network simulation shows significant vehicle queues at both these approaches with I-96 BL, which may result in safety concerns and/or block internal site circulation. Therefore, the applicable traffic signal warrants outlined in the *Michigan Manual on Uniform Traffic Control Devices (MMUTCD)* were evaluated. The future traffic forecast for this study includes traffic volumes for only two hours (AM and PM peak hours). In order to evaluate the 8-hour and 4-hour traffic signal warrants, hourly variations in hospital and medical office building trip generation published by ITE were referenced. Additionally, hourly data for I-96 BL were obtained from the MDOT Transportation Data Management System (TDMS). The annual background growth rate of 1.0% was applied to the MDOT data as necessary to forecast 2024 traffic volumes.

The results of the warrant analysis indicate that a traffic signal would be warranted for site access to I-96 BL. With signalization of the main site driveway, all approaches and movements at the intersection will operate acceptably at a LOS D or better. A left turn phasing analysis was also conducted at the intersection for the EB and WB approaches. The results of this analysis indicate several of the criteria are borderline to being warranted; however, network simulations indicate acceptable traffic operations without left-turn phasing. Therefore, MDOT and LCRC should monitor the intersection and consider installation of left turn phasing if queues or left turn crash patterns are experienced in the future.

Lastly, with the addition of a new traffic signal along I-96 BL, timings and offsets at all signalized study intersections were optimized during the peak hours to provide acceptable progression, particularly between the closely spaced intersections. With these improvements, all study intersection approaches and movements will operate acceptably or in a manner similar to background conditions during both peak hours as shown in **Table 10**. Network simulation also indicates future traffic operations which are similar to or improved compared to background conditions.

Along Parkway Drive long vehicle queues are continued to be observed for the egress left-turn movement during the PM peak hour; however, there is excess capacity available for egress left-turn movements at the signalized driveway. During peak periods, traffic exiting the site via Parkway Drive can utilize the internal connections with the access roadway around the Hospital and MOB to take advantage of the signal in turning left onto I-96 BL. However, pavement markings should be installed along Parkway Drive to delineate travel lanes and the egress left and right turn lanes at its intersection approach with I-96 BL. With the traffic control outlined above, the proposed site access plan is acceptable.

Table 10: Future 2024 Traffic Conditions with Improvements

			/I Peak I	lour				/I Peak H			
Intersection	Арр	roach	1	<u> </u>	ightharpoons	Ар	oroach	<u> 1</u>	<u> 1</u>	ightharpoonup	
1. I-96 BL & Latson Road	EB	39.2 D	43.8 D	42.4 D	26.2 C	EB	69.2 E	118.1 F	65.6 E	28.4 C	
	WB	42.2	64.7	38.3	17.7	WB	71.4	89.4	55.5	79.4 F ¹	
Signalized		D 91.4	E 48.2	D 41.8	B 137.5		E 62.4	F 77.2	E 64.7	44.6	
<u> </u>	NB	F 20.4	D	D 30.9	F 24.7	NB	E	E	E 46.2	D	
	SB	39.4 D	49.5 D	30.9 C	24.7 C	SB	56.7 E	77.7 E	46.2 D	31.1 C	
	Ov	erall	51.0	LOS	D	ó	/erall	66.2	LOS	E	
2. I-96 BL & Lawson Drive	EB	1.0 A	0.1 A	1.0 A	0.1 A	EB	0.7 A	1.6 A	0.8 A	0.2 A	
	WB	0.8	0.7	0.8	0.7	WB	6.0	0.7	6.1	6.1	
Signalized	-	A 38.1	A 38.8	A 3	7.5		A 44.1	A 45.3	A 37	7.2	
•	NB	D	D		D	NB	D	D	I)	
	SB	40.3 D	41.8 D		5.7 D	SB	38.8 D	39.6 D		7.2 O	
	Ov	erall	3.5	LOS	Α	0	/erall	6.5	LOS	Α	
3. I-96 BL & WB I-96 Off-Ramp	EB	0.4 A		0.4 A		EB	0.7 A		0.7 A		
	WB	4.6		4.6		WB	15.3		15.3		
Signalized	WB	A 39.8	40.3	Α	34.7	VVD	B 37.4	38.3	В	27.1	
	NB	D	D		C 24.7	NB	57.4 D	D		C C	
	Ov	erall	9.4	LOS	Α	ó	/erall	17.4	LOS	В	
4. Latson Road & Grand Oaks Drive / Ascension Medical Drive	EB	49.5 D	33.8 C		3.8 D	EB	41.8 D	33.6 C		1.9 O	
riscension medical prive	WB	35.8	42.9	3	1.6	WB	40.5	47.7	29	9.9	
Signalized		D 2.2	7.0	1.0	C 0.9		D 3.1	D 10.4	2.0	1.9	
•	NB	Α	В	Α	А	NB	Α	В	A	Α	
<u> </u>	SB	2.1 A	0.3 A	2.1 A	2.1 A	SB	7.2 A	1.1 A	7.2 A	7.2 A	
5. Latson Road & I-96 WB Ramps		A	Α	A	Α		Α	A 10.1 32.7	Α	A	
	Ov	A erall 40.6 D 2.3	A 7.8 40.1 D 12.0	A LOS	A A 40.9	O	A /erall 45.5 D 6.6	A 10.1 32.7 C 30.9	0.6	A B 49.0	
5. Latson Road & I-96 WB Ramps Signalized	WB NB	A erall 40.6 D	7.8 40.1 D	LOS	A A 40.9	WB NB	A verall 45.5 D	A 10.1 32.7 C	LOS	A B 49.0	
	WB NB SB	A 40.6 D 2.3 A 19.6 B	A 7.8 40.1 D 12.0 A	0.4 A 19.6 B	A 40.9 D 19.3 B	WB NB SB	A 45.5 D 6.6 A 25.8 C	A 10.1 32.7 C 30.9 C	0.6 A 17.1 B	A B 49.0 D	
Signalized	WB NB SB	A 40.6 D 2.3 A 19.6 B	A 7.8 40.1 D 12.0 A	0.4 A 19.6	A 40.9 D 19.3 B B	WB NB SB	A /erall 45.5 D 6.6 A 25.8 C /erall	A 10.1 32.7 C 30.9 C	0.6 A	A B 49.0 D 38.2 D C	
	WB NB SB	40.6 D 2.3 A 19.6 B erall 38.8	A 7.8 40.1 D 12.0 A	0.4 A 19.6 B	A 40.9 D 19.3 B B 48.5	WB NB SB	A verall 45.5 D 6.6 A 25.8 C verall 45.4	A 10.1 32.7 C 30.9 C 22.5 45.3	0.6 A 17.1 B	A B 49.0 D S 38.2 D C 45.5	
Signalized	WB NB SB Ov EB	A 40.6 D 2.3 A 19.6 B	A 7.8 40.1 D 12.0 A	0.4 A 19.6 B	A 40.9 D 19.3 B B	WB NB SB Ov	A /erall 45.5 D 6.6 A 25.8 C /erall	A 10.1 32.7 C 30.9 C	0.6 A 17.1 B	A B 49.0 D 38.2 D C	
Signalized	WB NB SB	A erall 40.6 D 2.3 A 19.6 B erall 38.8 D 23.1 C	A 7.8 40.1 D 12.0 A 15.4 32.5 C	0.4 A 19.6 B LOS	A 40.9 D 19.3 B B 48.5 D	WB NB SB	A verall 45.5 D 6.6 A 25.8 C verall 45.4 D 22.8 C	A 10.1 32.7 C 30.9 C 22.5 45.3 D	0.6 A 17.1 B LOS	A B 49.0 D S 38.2 D C 45.5 D	
Signalized Output G. Latson Road & I-96 EB Ramps	WB NB SB Ov EB NB SB	A erall 40.6 D 2.3 A 19.6 B erall 38.8 D 23.1 C 7.8 A	A 7.8 40.1 D 12.0 A 15.4 32.5 C	0.4 A 19.6 B LOS	A 40.9 D 19.3 B B 48.5 D 20.9 C	WB NB SB ON EB NB SB	A /erall 45.5 D 6.6 A 25.8 C /erall 45.4 D 22.8 C 7.1 A	A 10.1 32.7 C 30.9 C 22.5 45.3 D 23.0 C	0.6 A 17.1 B LOS 23.5 C	A B 49.0 D 38.2 D C 45.5 D 19.8 B	
Signalized 6. Latson Road & I-96 EB Ramps Signalized	WB NB SB Ov EB NB SB	A erall 40.6 D 2.3 A 19.6 B erall 38.8 D 23.1 C 7.8 A erall	A 7.8 40.1 D 12.0 A 15.4 32.5 C 21.4 C 22.0	0.4 A 19.6 B LOS 23.6 C 0.3 A	A 40.9 D 19.3 B B 48.5 D 20.9 C	WB NB SB ON EB NB SB	A //erall 45.5 D 6.6 A 25.8 C //erall 45.4 D 22.8 C 7.1 A //erall	A 10.1 32.7 C 30.9 C 22.5 45.3 D 23.0 C 22.4	0.6 A 17.1 B LOS 23.5 C 0.3 A LOS	A B 49.0 D S 88.2 D C 45.5 D 19.8 B	
Signalized Output Electric Signalized Output Electric Signalized Signalized Signalized Signalized Signalized Signalized Signalized	WB NB SB Ov EB NB SB	A erall 40.6 D 2.3 A 19.6 B erall 38.8 D 23.1 C 7.8 A	A 7.8 40.1 D 12.0 A 15.4 32.5 C	0.4 A 19.6 B LOS	A 40.9 D 19.3 B B 48.5 D 20.9 C	WB NB SB ON EB NB SB	A /erall 45.5 D 6.6 A 25.8 C /erall 45.4 D 22.8 C 7.1 A	A 10.1 32.7 C 30.9 C 22.5 45.3 D 23.0 C	0.6 A 17.1 B LOS 23.5 C	38.2 D C 45.5 D 19.8 B	
Signalized 6. Latson Road & I-96 EB Ramps Signalized 7. I-96 BL & Arundell Avenue / Site Drive	WB NB SB Ov EB NB SB Ov	A 40.6 D 2.3 A 19.6 B erall 38.8 D 23.1 C 7.8 A erall 0.9 A 0.8	7.8 40.1 D 12.0 A 32.5 C 21.4 C 22.0 0.1 A	0.4 A 19.6 B LOS 23.6 C 0.3 A LOS 1.0 A	A 40.9 D 19.3 B B 48.5 D 20.9 C C 0.2 A 0.6	WB NB SB ON EB NB SB ON	A /erall 45.5 D 6.6 A 25.8 C /erall 45.4 D 22.8 C 7.1 A /erall 10.2 B 10.5	A 10.1 32.7 C 30.9 C 22.5 45.3 D 23.0 C 22.4 14.8 B 5.7	0.6 A 17.1 B LOS 23.5 C 0.3 A LOS 10.1 B	A B 49.0 D S 38.2 D C 45.5 D 19.8 B C 5.7 A 10.8	
Signalized 6. Latson Road & I-96 EB Ramps Signalized 7. I-96 BL & Arundell Avenue / Site	NB SB Ov EB NB SB OW EB	A 40.6 D 2.3 A 19.6 B B C 23.1 C 7.8 A P	7.8 40.1 D 12.0 A 32.5 C	0.4 A 19.6 B LOS 23.6 C 0.3 A LOS 1.0 A 0.6 A	A 40.9 D 19.3 B 48.5 D 20.9 C	WB NB SB Or EB NB SB WB	A //erall 45.5 D 6.6 A 25.8 C //erall 45.4 D 22.8 C 7.1 A //erall 10.2 B	A 10.1 32.7 C 30.9 C 22.5 45.3 D 23.0 C 22.4 14.8 B	0.6 A 17.1 B LOS 23.5 C 0.3 A LOS 10.1 B	A B 49.0 D S 38.2 D C 45.5 D 19.8 B C 5.7 A	
Signalized 6. Latson Road & I-96 EB Ramps Signalized 7. I-96 BL & Arundell Avenue / Site Drive	WB NB SB Ov EB NB SB Ov	A erall 40.6 D 2.3 A 19.6 B erall 38.8 D 23.1 C 7.8 A erall 0.9 A 0.8 A 41.5 D	7.8 40.1 D 12.0 A 15.4 32.5 C 21.4 C 22.0 0.1 A 3.2 A 42.7 D	0.4 A 19.6 B LOS 23.6 C 0.3 A LOS 1.0 A 0.6 A 39	A 40.9 D 19.3 B B 48.5 D C C 0.2 A 0.6 A 9.9 D	WB NB SB O SB SB EB NB SB EB	A verall 45.5 D 6.6 A 25.8 C verall 45.4 D 22.8 C 7.1 A verall 10.2 B 10.5 B 40.8 D	A 10.1 32.7 C 30.9 C 22.5 45.3 D 23.0 C 22.4 14.8 B 5.7 A 44.2 D	0.6 A 17.1 B LOS 23.5 C 0.3 A LOS 10.1 B 10.4 B 36	A B 49.0 D S 38.2 D C 45.5 D 19.8 B S 5.2 D S 5.2 D S 5.2 D S 5.2 D S 6 S 5.2 D S 6 S 6 S 6 S 6 S 6 S 6 S 6 S 6 S 6 S	
Signalized 6. Latson Road & I-96 EB Ramps Signalized 7. I-96 BL & Arundell Avenue / Site Drive	NB SB Ov EB NB SB Ov EB NB SB SB SB SB SB SB	A 40.6 D 2.3 A 19.6 B P 23.1 C 7.8 A P 241.7 D	7.8 40.1 D 12.0 A 32.5 C 21.4 C 22.0 0.1 A 42.7 D	0.4 A 19.6 B LOS 23.6 C 0.3 A LOS 1.0 A 0.6 A	A 40.9 D 19.3 B B 48.5 D C C 0.2 A 0.6 A 9.9	WB NB SB Or EB NB SB WB	A verall 45.5 D 6.6 A 25.8 C verall 45.4 D 22.8 C 7.1 A verall 10.2 B 10.5 B 40.8	A 10.1 32.7 C 30.9 C 22.5 45.3 D 23.0 C 22.4 14.8 B 5.7 A 44.2	0.6 A 17.1 B LOS 23.5 C 0.3 A LOS 10.1 B 10.4 B 36 I 33	A B 49.0 D S 38.2 D C 45.5 D 19.8 B C 5.7 A 10.8 B 5.2	
Signalized 6. Latson Road & I-96 EB Ramps Signalized 7. I-96 BL & Arundell Avenue / Site Drive Signalized	NB SB Ov EB NB SB Ov EB NB SB SB SB SB SB SB	A 40.6 D 2.3 A 19.6 B erall 38.8 D 23.1 C 7.8 A erall 0.9 A 0.8 A 41.5 D 41.7	A 7.8 40.1 D 12.0 A 32.5 C 21.4 C 22.0 0.1 A 42.7 D 42.1	0.4 A 19.6 B LOS 23.6 C 0.3 A LOS 1.0 A 0.6 A	A 40.9 D 19.3 B B 48.5 D 20.9 C C C 0.2 A 0.6 A 9.9 D 1.2	WB NB SB ON EB NB SB ON SB	A //erall 45.5 D 6.6 A 25.8 C //erall 45.4 D 22.8 C 7.1 A //erall 10.2 B 10.5 B 40.8 D 36.4	A 10.1 32.7 C 30.9 C 22.5 45.3 D 23.0 C 22.4 14.8 B 5.7 A 44.2 D 40.7	0.6 A 17.1 B LOS 23.5 C 0.3 A LOS 10.1 B 10.4 B 36 1 33	A B 49.0 D S 38.2 D 45.5 D 19.8 B C 5.7 A 10.8 B 5.2 D 5.2 D 5.2	
Signalized 6. Latson Road & I-96 EB Ramps Signalized 7. I-96 BL & Arundell Avenue / Site Drive	NB SB Ov EB NB SB Ov EB NB SB SB SB SB SB SB	A 40.6 D 2.3 A 19.6 B P 23.1 C 7.8 A P 241.7 D	7.8 40.1 D 12.0 A 32.5 C 21.4 C 22.0 0.1 A 42.7 D 42.1 D	0.4 A 19.6 B LOS 23.6 C 0.3 A LOS 1.0 A 0.6 A	A 40.9 D 19.3 B B 48.5 D 20.9 C C 0.2 A 0.6 A 9.9 D 1.2 D	WB NB SB ON EB NB SB ON SB	A //erall 45.5 D 6.6 A 25.8 C //erall 45.4 D 22.8 C 7.1 A //erall 10.2 B 10.5 B 40.8 D 36.4 D	A 10.1 32.7 C 30.9 C 22.5 45.3 D 23.0 C 22.4 14.8 B 5.7 A 44.2 D 40.7 D	0.6 A 17.1 B LOS 23.5 C 0.3 A LOS 10.1 B 10.4 B 36 LOS LOS	A B 49.0 D S 38.2 D 45.5 D 19.8 B C 5.7 A 10.8 B S 5.2 D 5.2 C 5.2 C 5.2 C 5.2 C 5.3	
Signalized 6. Latson Road & I-96 EB Ramps Signalized 7. I-96 BL & Arundell Avenue / Site Drive Signalized 8. I-96 BL & Parkway Drive	NB SB Ov EB NB SB Ov EB VB SB Ov	A erall 40.6 D 2.3 A 19.6 B Erall 38.8 D 23.1 C 7.8 A erall 0.9 A 0.8 A 1.5 D 41.7 D Erall 1.4	7.8 40.1 D 12.0 A 15.4 32.5 C 21.4 C 22.0 0.1 A 3.2 A 42.7 D 42.1 D 3.1 Fr	0.4 A 19.6 B LOS 23.6 C 0.3 A LOS 1.0 A 0.6 A 34 LOS ee	A 40.9 D 19.3 B B 48.5 D 20.9 C C 0.2 A 0.6 A 9.9 D 1.2 D	WB NB SB On EB NB SB On EB WB SB On	A /erall 45.5 D 6.6 A 25.8 C /erall 45.4 D 22.8 C 7.1 A /erall 10.2 B 10.5 B 40.8 D 36.4 D /erall 0.2	A 10.1 32.7 C 30.9 C 22.5 45.3 D 23.0 C 22.4 14.8 B 5.7 A 44.2 D 40.7 D 13.6 Free 13.6	0.6 A 17.1 B LOS 0.3 A LOS 10.1 B 10.4 B 36 LOS LOS e	A B 49.0 D S 38.2 D 45.5 D 19.8 B S 5.7 A 10.8 B S 5.2 D 5.2 C 5.2 C 5.3	
Signalized 6. Latson Road & I-96 EB Ramps Signalized 7. I-96 BL & Arundell Avenue / Site Drive Signalized	NB SB Ov EB NB SB Ov EB Ov EB	A erall 40.6 D 2.3 A 19.6 B erall 38.8 D 23.1 C 7.8 A erall 0.9 A 41.5 D 41.7 D erall	A 7.8 40.1 D 12.0 A 15.4 32.5 C 21.4 C 22.0 0.1 A 42.7 D 42.1 D 3.1 Fr	0.4 A 19.6 B LOS 23.6 C 0.3 A LOS 1.0 A 0.6 A 34 LOS ee	A 40.9 D 19.3 B B 48.5 D C 0.2 A 0.6 A 9.9 D 1.2 D A	WB NB SB ON EB NB SB ON EB WB NB SB EB	A //erall 45.5 D 6.6 A 25.8 C //erall 45.4 D 22.8 C 7.1 A //erall 10.2 B 10.5 B 40.8 D 36.4 D //erall	A 10.1 32.7 C 30.9 C 22.5 45.3 D 23.0 C 22.4 14.8 B 5.7 A 44.2 D 40.7 D 13.6 Fre	0.6 A 17.1 B LOS 0.3 A LOS 10.1 B 10.4 B 36 LOS LOS e	A B 49.0 D S 38.2 D C 45.5 D 19.8 B S 5.2 D S 3.2 C B	

1. V/C ratio greater than 1.0.

Conclusions and Recommendations

The overall conclusions of this Traffic Impact Study are as follows:

- 1. All study intersection approaches and movements currently operate acceptably at a LOS D or better during both peak hours with the exception of the following:
 - a. The signalized intersection of I-96 BL & Latson Road which currently operates at a LOS E during the PM peak hour with several individual movements currently operating at a LOS E or F during both peak hours.
 - b. The STOP controlled approaches of Arundell Avenue and the Grand River Golf Range & Putt Putt drive with I-96 BL which currently operate at a LOS F during both peak hours.
 - c. The STOP controlled left turn movement from Parkway Drive to I-96 BL which currently operates at a LOS E during both peak hours.
- 2. Ambient traffic growth of 1.0% was applied to establish 2024 background traffic volumes *without the proposed development*. Several background developments were also identified in the study area and included in this study.
- 3. Background (2024) conditions analyses indicate that several movements will experience degraded operations including the intersection of I-96 BL & Latson Road, and the left-turn movements from Latson Road to the EB and WB I-96 on-ramps.

Future (2024) conditions analyses indicate that most study intersection approaches and movements will continue to operate acceptably; however, there are several locations with undesirable or failing conditions that are expected to worsen in the future, if those movements operating at a LOS E or F are not improved under background conditions.

- 4. Egress left turns from the site and Parkway Drive to I-96 BL will not operate acceptably under STOP control with the proposed development. In accordance with MMUTCD requirements a traffic signal is warranted at the site driveway with the proposed development.
- With the improvements outlined below, all study network intersections and site driveways will operate acceptably, or in a manner similar or improved compared to background conditions during the peak hours.

Based on the results of this study, the following should be considered to provide acceptable traffic operations due to existing deficiencies and other development projects, *regardless of the proposed Hospital and MOB:*

1. Install permissive-protected left-turn phasing at the intersections of Latson Road with the EB and WB I-96 ramps.

Based on the results of this study, the following should be considered to provide acceptable traffic operations with all development projects, includig the proposed Hospital and MOB:

- 1. Install a traffic signal at the proposed site driveway to I-96 BL aligned with Arundell Avenue.
- 2. Optimize the network signal timings and coordinate all traffic signals, including the proposed signal along I-96 BL.
- 3. Install pavement markings on Parkway Drive in accordance with MMUTCD guidelines.





St. Joseph Mercy Livingston

New Hospital and Medical Office Building Livingston, MI

February 18, 2020 Revised Concept PUD Submittal

Project Number: 11404.000

Prepared for:

Trinity Health - Michigan d/b/a St. Joseph Mercy Livingston 620 Byron Road Howell, MI 48843

Prepared by:

SMITHGROUP

500 GRISWOLD SUITE 1700 DETROIT, MI 48226 313.983.3600 www.smithgroup.com





SHEET INDEX

SHEET NUMBER SHEET NAME

COVER

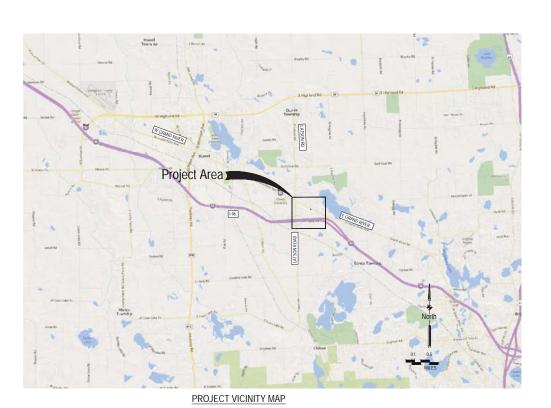
/.1.0-V.1.9 SURVEY SHEETS

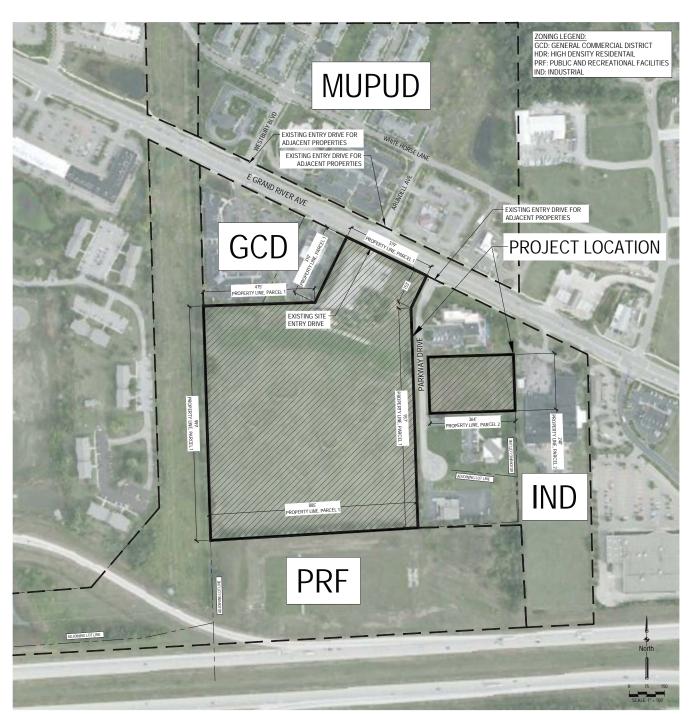
CS100 SITE PLAN

CS101 SITE PLAN REVISION SKETCH

CU100 UTILITY PLAN

A-100 ARCHITECTURE VIEWS
000-004 ARCHITECTURAL FLOOR PLANS





VICINITY SKETCH -GENOA TOWNSHIP, LIVINGSTON COUNTY, MICHIGAN



ALTA/NSPS LAND TITLE SURVEY

PART OF THE NORTHWEST 1/4 OF SECTION 9, TOWN 2 NORTH, RANGE 5 EAST, GENOA TOWNSHIP, LIVINGSTON COUNTY, MICHIGAN

ALTANSPS LAND TITLE SURVEY SHEETS V.1.9 -V.2.0 PARCEL 1 ALTAINSPS LAND TITLE SURVEY SHEETS V.1.1 - V.1.8 %

SURVEY LEGEND

N. 1/4 COR. SECTION 9 TZN, RSE REMON CAP NO. 47065

SMITHGROUP

SAINT JOSEPH MERCY

HEALTH SYSTEM

ST. JOSEPH MERCY LIVINGSTON

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ALTA/NSPS LAND TITLE SURVEY - OVERALL



SITE BENCHMARK 3303 SET BENCHTIE ON NORTH FACE OF UTILITY POLE

SITE BENCHMARK 3881 ATOP ARROW ON HYDRANT

SITE BENCHMARK 8116 ATOP "X" ON BOLT OF LIGHT POLE BASE NORTHING = 395,277.65' EASTING = 13,257,884.19' ELEVATION = 1002.98 FEET (NAVD88)

CONTROL POINT TABLE

Point No.	Northing	Easting	Elevation	Description
103	395039.73	13258195.23	994.31	CP SGJUR CAP REBAR
104	394692.47	13258582.05	994.04	CP SGJUR CAP REBAR
105	394255.82	13258199.83	1003.29	CP SGJUR CAP REBAR
106	394742.25	13258662.52	993.80	CP PK-MAGNAIL
107	394892.96	13258709.32	989.88	CP PK-MAGNAIL
108	395187.41	13257217.98	1009.18	CP SGJUR CAP REBAR
109	394676.19	13257266.76	1006.94	CP SGJUR CAP REBAR
111	395200.99	13257326.18	1009.04	CP SGJUR CAP REBAR
112	395213.77	13257790.76	1005.74	CP PK-MAGNAIL
113	395067.53	13257771.88	1002.71	CP SGJUR CAP REBAR
114	394999.81	13258048.46	1000.91	CP SGJUR CAP REBAR
115	395413.76	13257844.53	1006.68	CP SGJUR CAP REBAR
116	395217.59	13257857.26	1001.36	CP SGJUR CAP REBAR
5101	394388.41	13258297.24	1000.37	CP SGJUR CAP REBAR

– PROJĒCT SITE

NOTES
1. HORIZONTAL DATUM IS ACCORDING TO MICHIGAN STATE PLANE GRID COORDINATES, SOUTH ZONE (2113), GEOID 2012A, INTERNATIONAL FEET AS LINEAR UNIT OF MEASURE.

VERTICAL DATUM IS ACCORDING TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88), WITH BENCHMARKS LISTED HEREON

THIS SURVEY DOES NOT CONSTITUTE A TITLE SEARCH BY THE SURVEYOR, ALL INFORMATION RECARDING LAND BOUNDARY, EASEMENTS, RIGHTS—OF-WAY, AND OTHER INFORMATION THAT MAY AFFECT TITLE WAS GAINED TROM AVAILABLE TAX RECORDS AND/OR RECORDED LEGAL DOUBLETS FOR ADJOINNO PARCELS

COMCAST, AT&T, MHOG, CONSUMER'S ENERGY NATURAL GAS, DTE ELECTRIC, ACD.NET, 6. UNDERGROUND UTILITY LOCATIONS WERE DERIVED FROM ACTUAL MEASUREMENTS ON WISBLE UTILITIES, MISS DIG. MEY SHOULD HAVE THE MET THEY SHOULD HAVE IT BE ASSURED THAT THEY ARE THE ONLY UNDERGROUND UTILITIES IN THE AREA.

7. SUBSURFACE AND ENVIRONMENTAL CONDITIONS WERE NOT EXAMINED AND ARE NOT CONSIDERED A PART OF THIS SURVEY. NO STATEMENT IS MADE CONCERNING THE EXISTENCE OF UNDERGROUND OR OVERHEAD CONTAINERS OR FAULTHES THAT MAY AFFECT THE USE AND OR DEVELOPMENT OF THIS SITE.

MAI APPECLIFE USE AND OR DEVELOPMENT OF THIS SIZE.

ADDITIONAL SPOT ELEVATIONS MAY BE CONTAINED IN THE

PROJECT DATABASE, AND ALTHOUGH NOT APPEARING IN THE

RECORD PLAN DOCUMENT DUE TO SCALE AND YISBILITY, WERE

UTILIZED IN THE DIGITAL TERRAIN MODE, FOR CREATION OF THE

ONE—FOOT CONTOURS. THE ADDITIONAL SPOT ELEVATION DATA

IS AVAILABLE FOR PRIVILE DESIGN AMAI'SS, BEINDS LOCATED ON

A PROZED LATER WITHIN THE PROJECT DATABASE.

9. PARKING LOT STRIPING ON THE SUBJECT PROPERTY IS EXTREMELY FADED. THE PARKING COUNT REFLECTS THE CLEARLY MARKED SPACES, HOWEVER IT ONLY REPRESENTS A SMALL PERCENTAGE OF THE OVERALL AVAILABLE ONSITE PARKING CAPACITY.

 NO CONSTRUCTION ACTIVITIES, OR EVIDENCE THEREOF, WAS OBSERVED DURING THE COURSE OF THE SURVEY. 11. THERE ARE NO KNOWN PROPOSED CHANGES TO THE RIGHTS-OF-WAY OF THE ADJOINING STREETS

12. AT THE TIME OF THE SURVEY, THE SUBJECT PROPERTY HAS NOT YET BEEN VISITED BY AN WETLAND SPECIALIST

FLOODPLAIN NOTE
THE SITE, AS SHOWN ON THIS SURVEY, LIES IN FLOOD AREA "ZONE X". ACCORDING TO FERERAL EMERGENCY MANAGMENT AGENCY (FEMA) FLOOD INSURANCE RATE MAP (FIRM) NO. 26093C, PANEL 0330D, FOR LIWINGSTON COUNTY, MICHORAL FEFTCHE DATE X SEPTEMBER 17, 2008. ACCORDING TO SAID PANEL, ZONE X IS DETRIED AS A SEASO OF MINIMAL FLOODING

STANDARD SPACES = 53
BARRIER FREE SPACES = 2
TOTAL STRIPED SPACES = 55

3. SMITHOROUP PERFORMED A TOPOGRAPHIC SURVEY UTILIZING DATA COLLECTED WITH GLOBAL POSITIONING RECEIVERS REFERENCING THE MICHIGAN CONTINUOUSLY OPERATING REFERENCE NETWORK FOR SITE CONTROL.

ALTA/NSPS LAND TITLE SURVEY

PART OF THE NORTHWEST 1/4 OF SECTION 9, TOWN 2 NORTH, RANGE 5 EAST, GENOA TOWNSHIP, LIVINGSTON COUNTY, MICHIGAN

13 66' WIDE PRIVATE DRIVE PARKWAY DR. L.2101, 486, L.C.R. SURVEY DETAIL SHEETS V.1.2 - V.1.8 CP 1041 CP 109 CENTER SECTION 9 TZN, RSE 3,4° PPE BOAT SPIKE 262 % OF CENTER

LEGAL DESCRIPTION: Parcel 1

N. 1/4 CCR. SECTION 9 T2N, RSE REMON CAP NO. 47055

(Per ATA National Title Group, LLC Title Commitment No. 47–18618665–SCM, Revision No. 2, dated September 17, 2019)

Part of the East 1/2 of the Northwest 1/4 of Section 9, Town 2 North, Range 5 East, Genoo Township, Lifwigston County, Minhigan, described on Sollows: Commencing at the Northwest corner of all Sections 9, thence North 1 degrees 45 minutes 40 seconds East 1982.0 feet during the careful East 1982 on the Section Heart South 60 degrees 55 minutes 25 seconds 60 seconds East 1981.1 feet 1982 on the Section Heart South 60 degrees 55 minutes 25 seconds 60 seconds East 1981.1 feet along the constraints of Osmothe Southers 50 seconds 1982 of the Section 1982 degrees 15 minutes 00 seconds East 175.65 feet; thence North 29 degrees 16 minutes 00 seconds Section 1982 of the Section 1982 of t

LEGAL DESCRIPTION: Parcel 1

Commercing of Ministry (1994) and Ministry (19

SCHEDULE B EXCEPTIONS: Parcel 1

Ger ATA Notional Title Group, LLC Title Commitment No. 47–18618665–SCM, Revision No. 2, dated September 17, 2019)

Belease of Right of Way to the Livingston County Board of Road Commissioners recorded in Liber 153, Page 68, Livingston County Records, (AFFECTS SUBJECT PROPERTY, 50' WIDE DEDICATION FOR GRAND RIVER AVE. SHOWN HEREON)

(2) Easement for public utilities purposes vested in Township of Genoa by instrument recorded in Liber 1847, Page 667, Livingston Country Records. (AFFECTS SUBJECT PROPERTY, 15' WIDE EASEMENT SHOWN HEREON)

ZONING
THE SITE, AS SHOWN ON THIS SURVEY, IS ZONED PRF (PUBLIC & RECREATIONAL FACILITIES DISTRICT) ACCORDING TO THE GENOA TWP. ZONING MAP, LAST REVISED

GCD (GENERAL COMMERCIAL DISTRICT). GCD HAS A FRONT YARD SETBACK WITH PARKING OF 70 FEET.

SIDE YARD SETBACK - PRF DISTRICT WITH A DRIVING RANGE IS NOTED TO HAVE A 75 FOOT SIDE YARD SETBACK

REAR YARD SETBACK - PRF DISTRICT WITH A DRIVING RANGE IS NOTED TO HAVE A 50 FOOT SETBACK FOR TRAILS AND ATHLETIC FIELDS MAX HT. - 35 FEET OR 2 STORIES

CERTIFICATION TO: PARKPROP, LLC; D.A.V., LLC; STEWART TITLE; ATA NATIONAL TITLE GROUP THE IS TO CERTIFY THAT THIS WAR OF PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2016 MINIMAL STANDARD DETAIL REQUIREMENTS FOR ALTA/MSS LAND THE SURVEYS, SOUTHY ESTABLISHED AND R. 9, 9, 13, 14, 16, 17, 18, 20 AND 21 OF TABLE A THEREOF. THE FIELDWORK WAS COMPLETED ON SEPTEMBER 9, 2019.

CHARLES A. LANGOLF, PS 56210 PROFESSIONAL SURVEYOR STATE OF MICHIGAN charlie langolf@smithgroup.com SMITHGROUP, INC.





ST. JOSEPH MERCY LIVINGSTON

SMITHGROUP

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ALTA/NSPS LAND TITLE SURVEY - PARCEL 1

OVERALL

DRAWING NUMBER

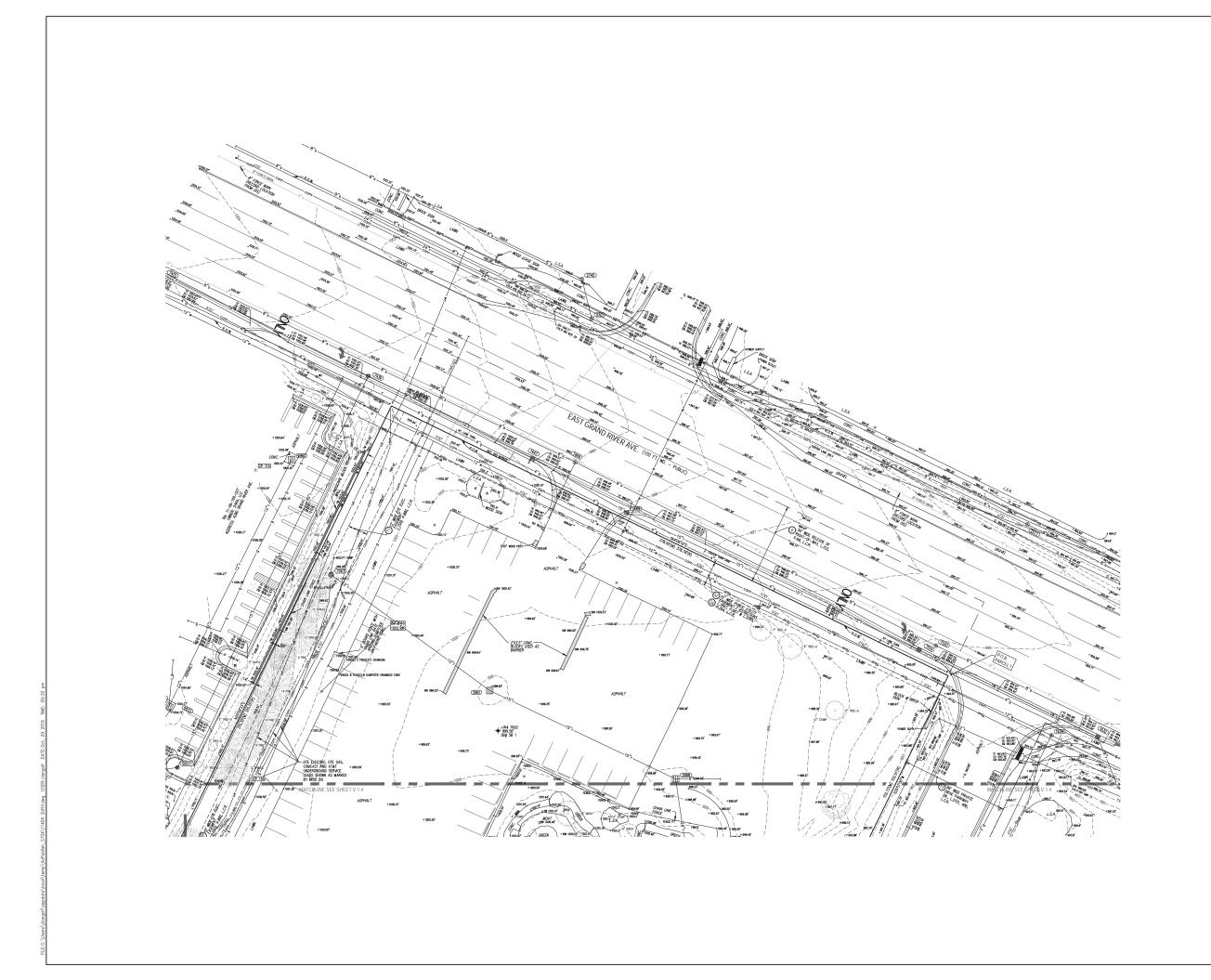
0' 40' 80' SCALE: 1" = 80'

11404.000 V.1.1

2017
- LORC NW CORNER OF SECTION 9, T2N, RSE, GENOA TOWNSHIP, LIVINGSTON COUNTY, MICHIGAN (E-3), RECORDED IN INSTRUMENT NO. 2013:—0057, DATED MARCH 8, 2013
- LORC N 1/4 CORNER OF SECTION 9, T2N, RSE, GENOA TOWNSHIP, LIVINGSTON COUNTY, MICHIGAN (F-3), RECORDED IN LSC NO. 1720M, DATED MARCH 11, 2002

REFERENCE SURVEY DOCUMENTS: BOUNDARY SURVEY BY ADVANTAGE CE, JOB NO. 00170, DATED NOVEMBER 6TH,
2000.

BOILDARY SURVEY BY ADVANTAGE CE, JOS NO. 00170, DATED NOVABEER ETH, 2000
 BOILDARY SURVEY BY CHARLES E. CARLOCK FOR LOBENTZEN, BOOK 162, PAGE 20, DATED SETPIERER 79, 1989
 BOILDARY SURVEY BY CHARLES E. CARLOCK FOR ROSE REALTY, BOOK 164, BOILDARY SURVEY BY CHARLES E. CARLOCK FOR ROSE REALTY, BOOK 164, DATED NOVABEER 18, 1993
 BOILDARY SURVEY BY CHARLES E. CARLOCK FOR CORRENAM, BOOK 16820A, DATED NOVABEER 18, 1993
 BOILDARY SURVEY BY CHARLES E. CARLOCK FOR L. PRANCE, BOOK 164, PAGE 800, DATED NOVABEER 18, 1993
 BOILDARY SURVEY BY CHARLES E. CARLOCK FOR L. PRANCE, BOOK 154, PAGE 800, DATED NOVABEER SURVEY BY BOOK BURKERNON, JOS NO. 9192-L. DATED DECEMBER 1, 1998
 BOILDARY SURVEY BY BOOK BURKERNON, JOS NO. 9192-D, DATED JARY 25, 1998
 BOILDARY SURVEY BY BOOK BURKERNON, JOS NO. 9192-D, DATED JARY 25, 1998
 BOILDARY SURVEY BY BOOK BURKERNON, JOS NO. 9192-D, DATED JARY 25, 1998
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 BOILDARY SURVEY BY BOOK BURKERNON, JOS NO. 9192-D, DATED JARY 25, 1998
 BOUNDARY SURVEY BY BURKER BY BOOK BURKERNON, JOS NO. 9193-D, JOS NO. 330-D, BORNAMEN SURVEY BY CORGANIES POWER CO. NO. 9403-SER, DATED JARY 72
 BOOK DATE SURVEY BY CORGANIES POWER CO. NO. 9403-SER, DATED JARVEY 72
 BOOK DATE SURVEY BY CORGANIES POWER CO. NO. 9403-SER, DATED JARVEY 72
 BOOK DATE SURVEY BY CORGANIES POWER CO. NO. 9403-SER, DATED JARVEY 72
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 BOOK DATE SURVEY BY CORGANIES POWER CO. NO. 9403-SER, DATED JARVESTON POWER CO. 9403-SER, DATED JARVESTON POWER





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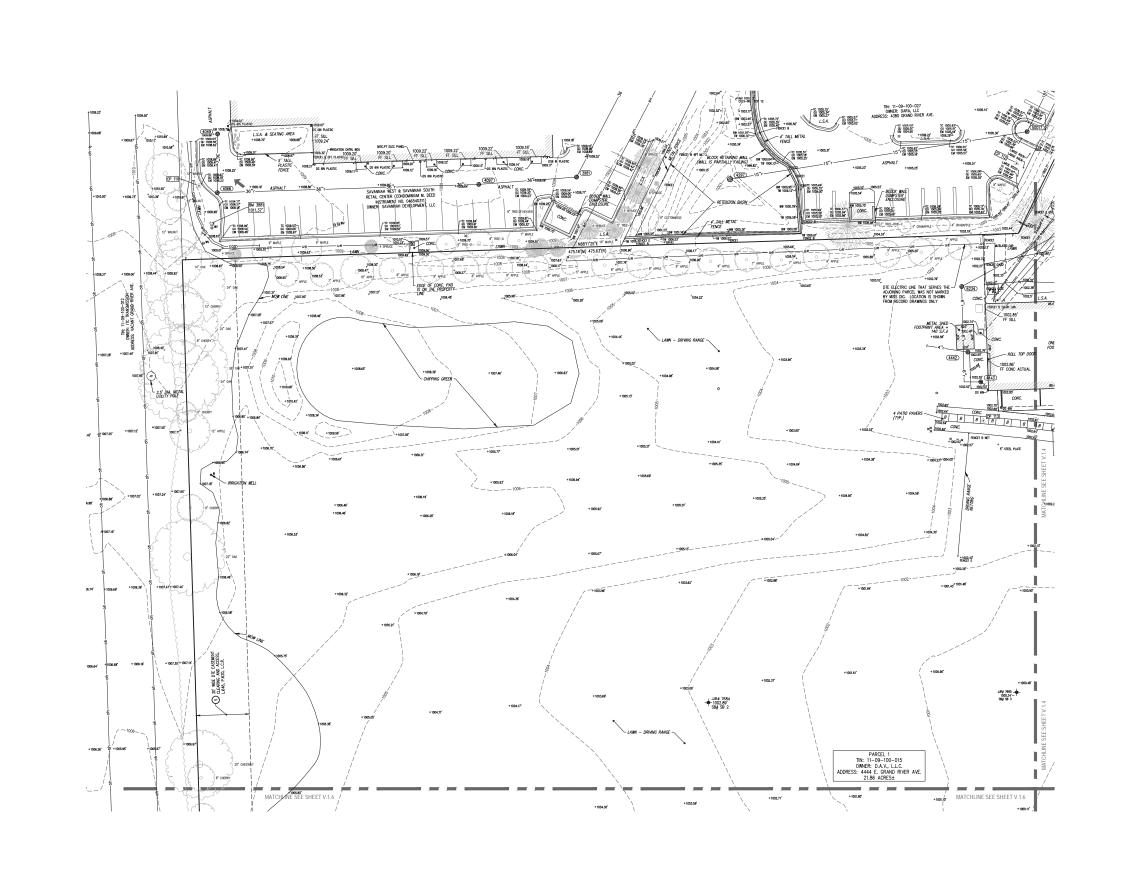




ALTA/NSPS LAND TITLE SURVEY - PARCEL 1 OVERALL



SCALE 11404.000 PROJECT NUMBER V.1.2





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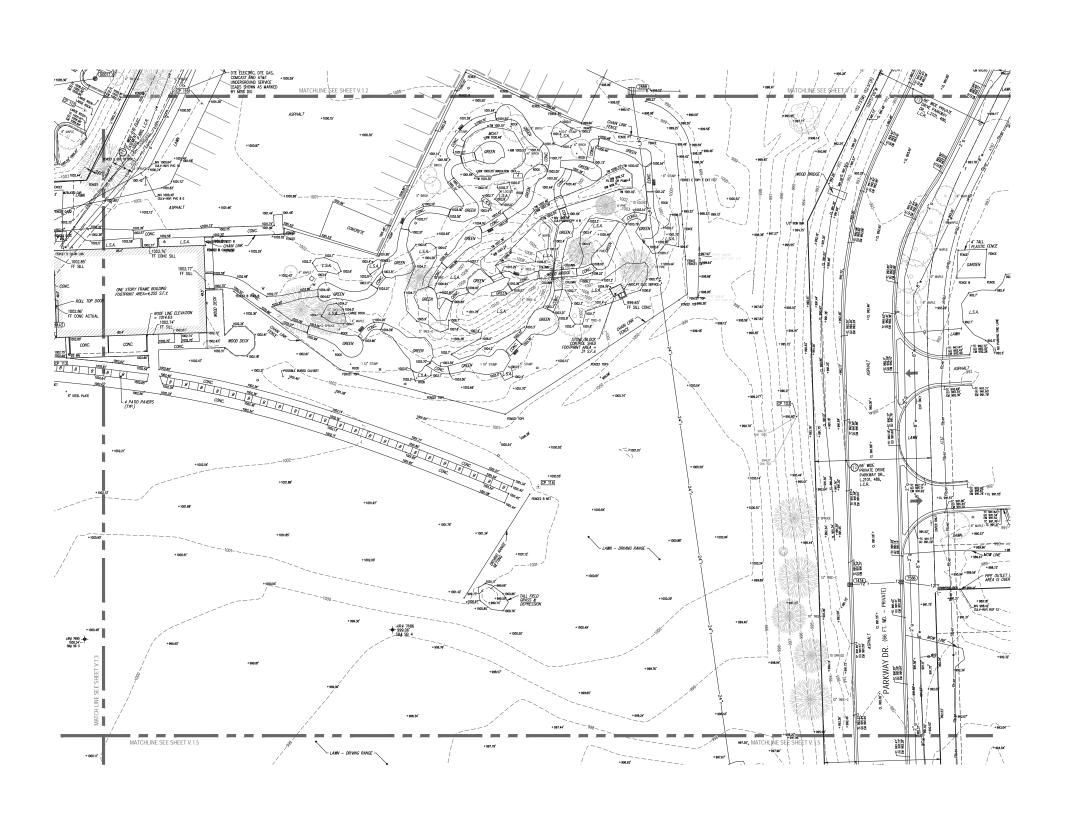














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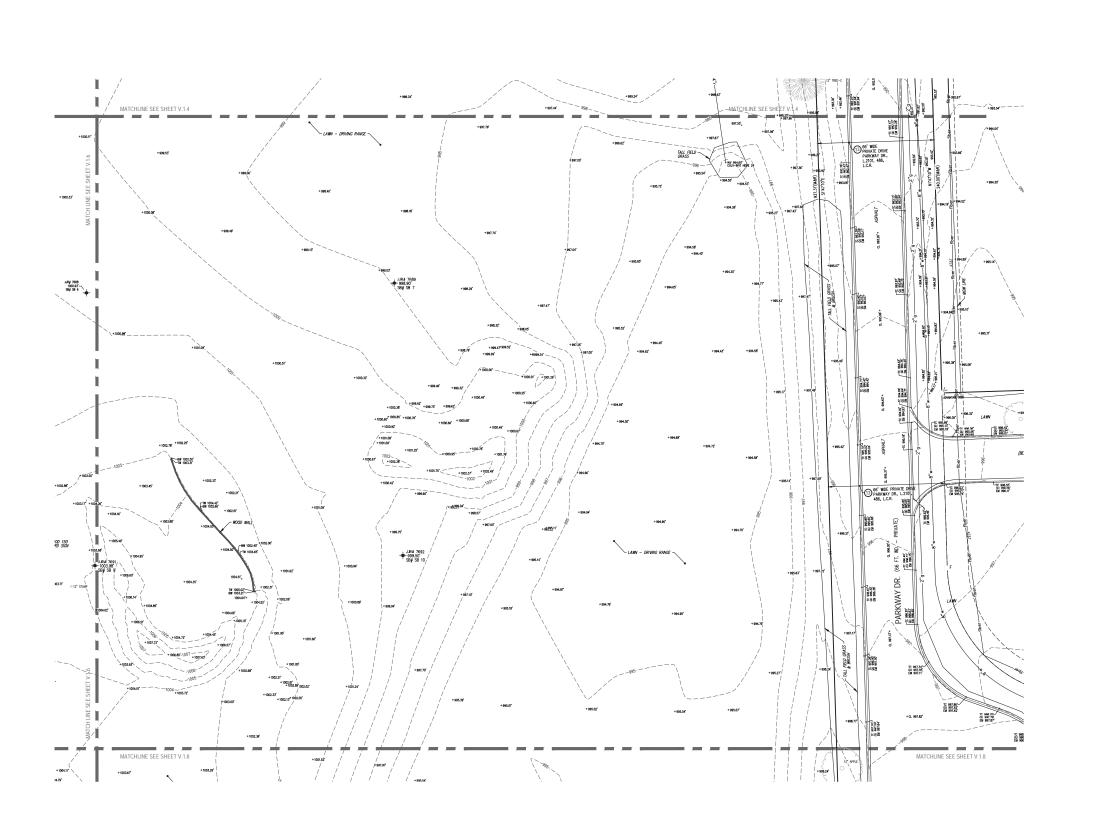




NOT FOR CONSTRUCTION









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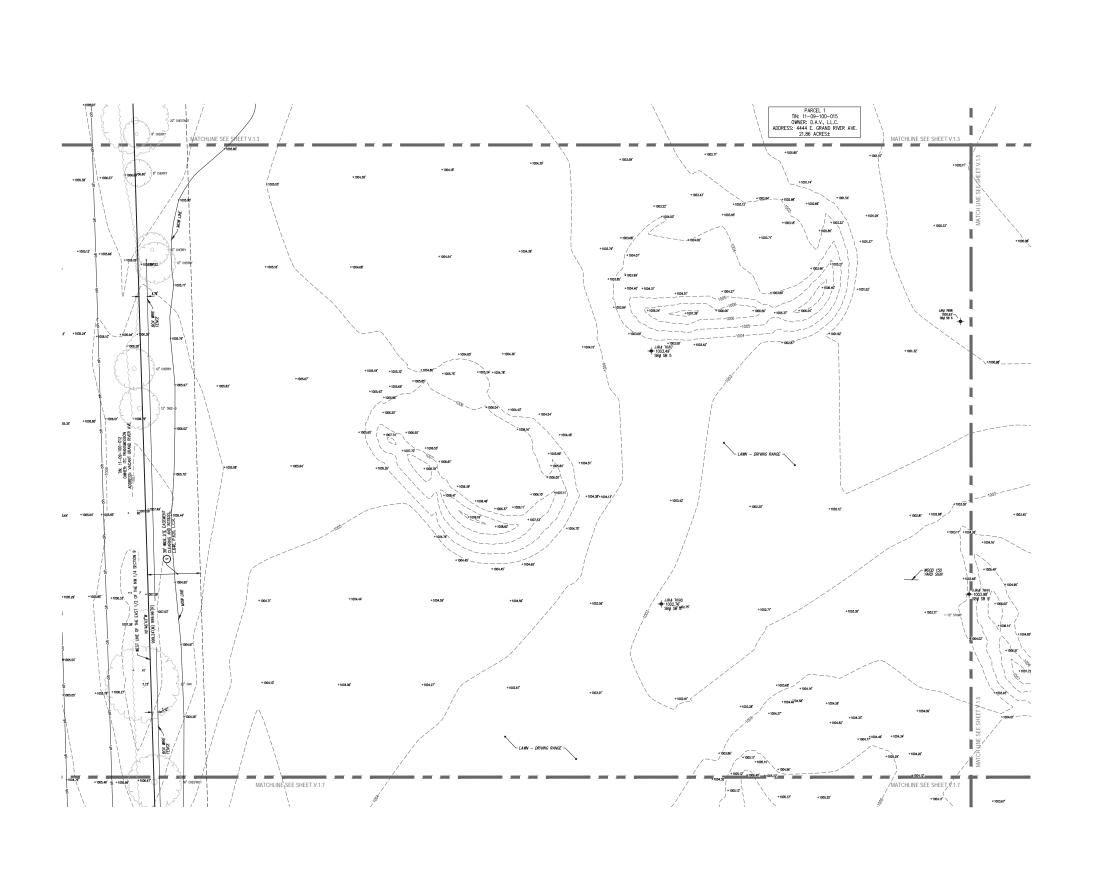














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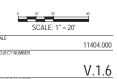


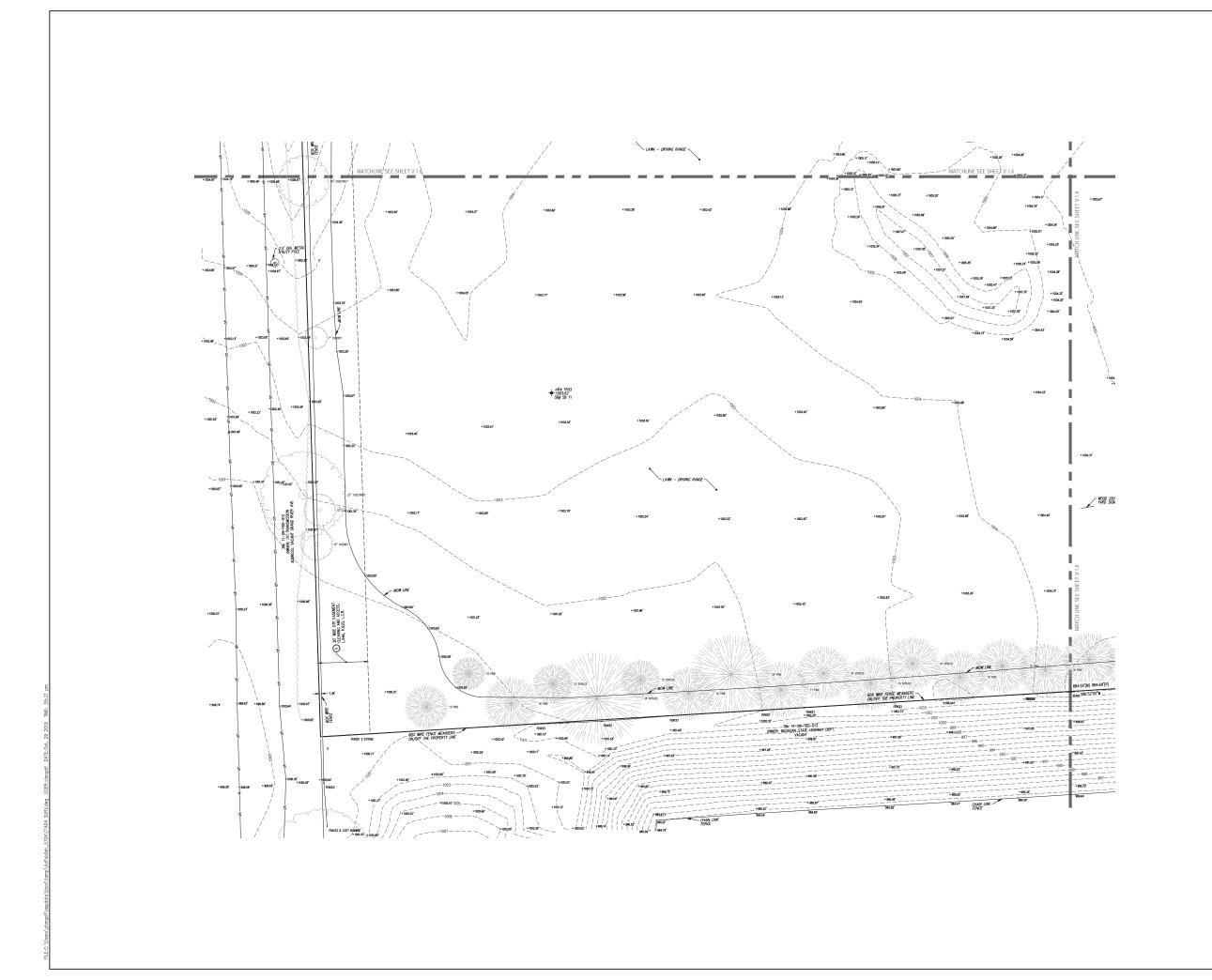


SELECTION SIGNATOR











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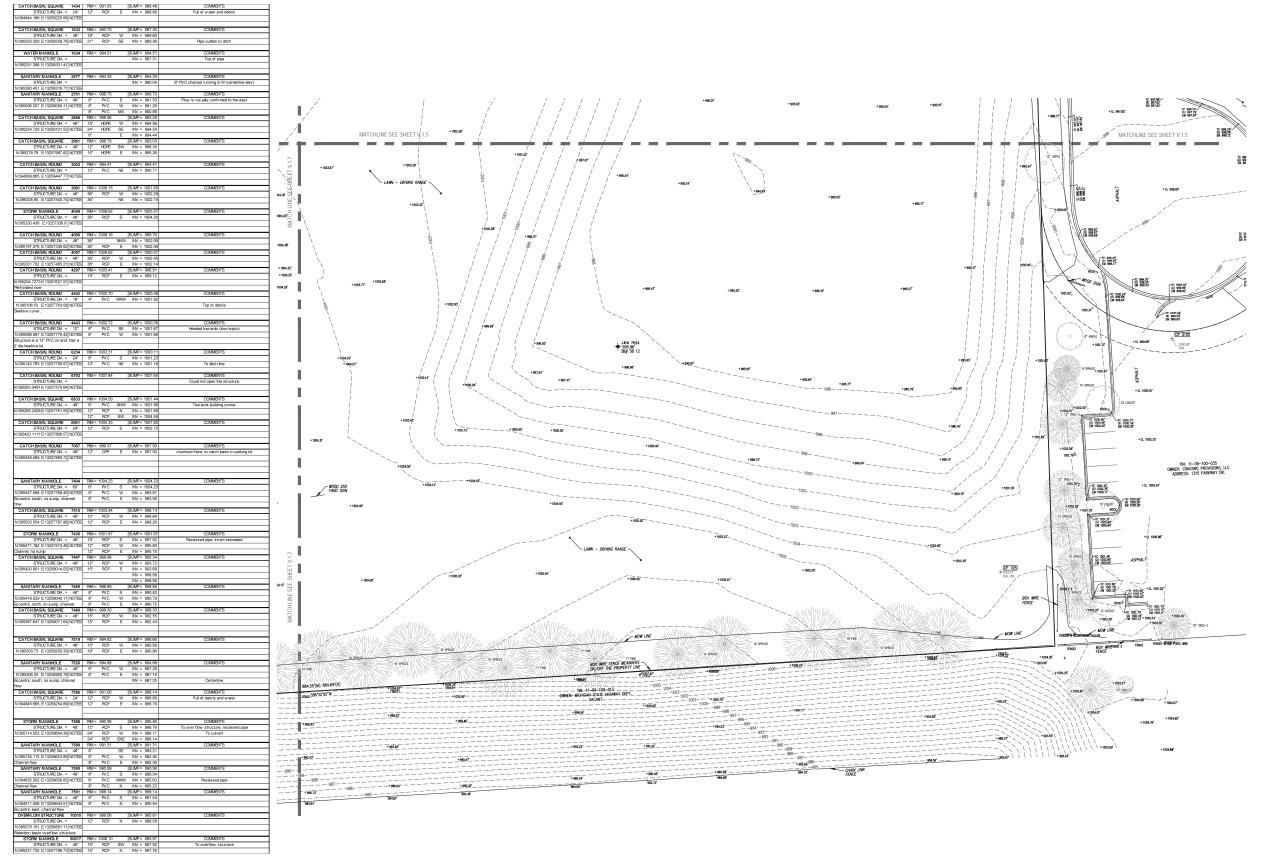




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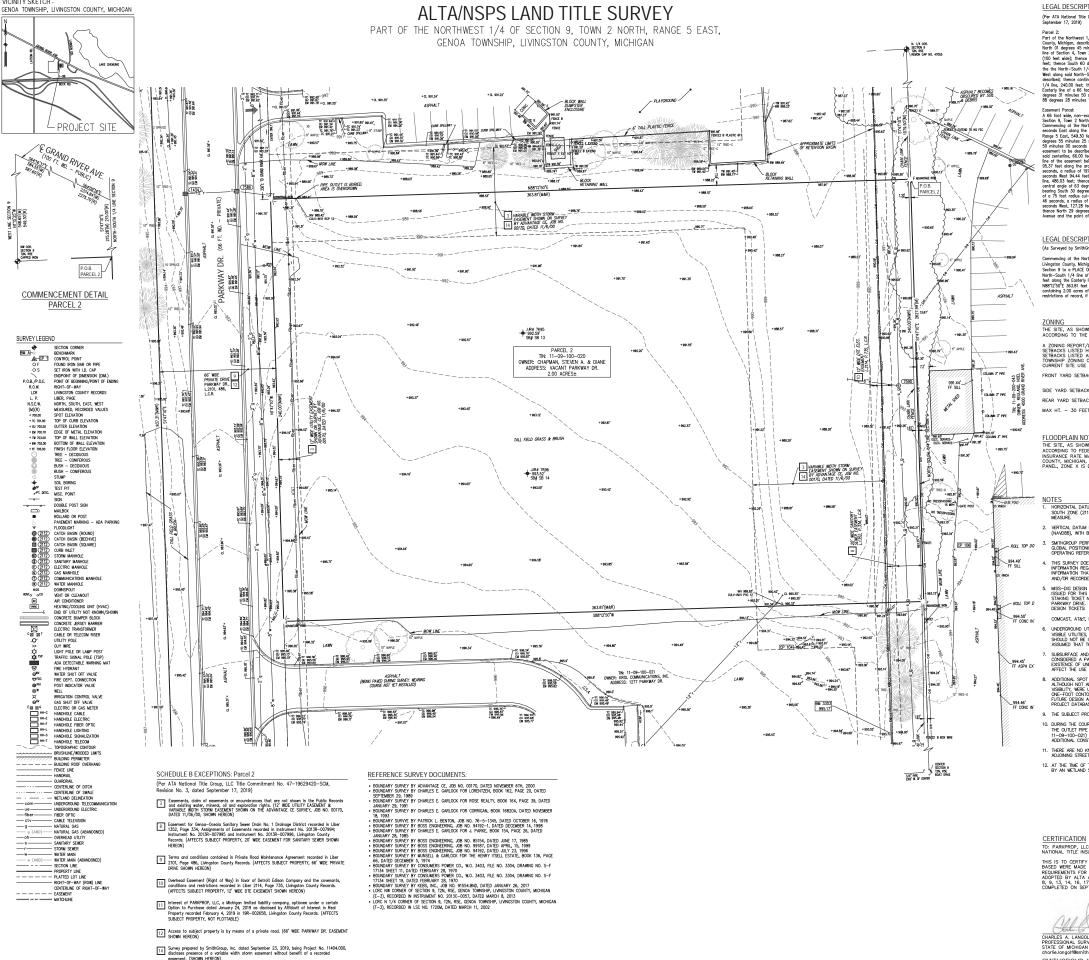


OT FOR CONSTRUC



ALTA/NSPS LAND TITLE SURVEY





LEGAL DESCRIPTION: Parcel 2

(Per ATA National Title Group, LLC Title Commitment No. 47-19629420-SCM, Revision No. 3, dated September 17, 2019)

September 17, 2019)
Parcel 2:
Part of the Northwest I/A of Section 9, Town 2 North, Range 5 East, Genco Township, Livingston County, Michigan, described ac Commencing of the Northwest corner of said Section 9; theree North 01 degrees 45 minutes 40 seconds East doing the centerline of Lation Road and the West inte of Section 4, none 2 North, Range 5 East, 5440, De for to the centerline of Carde River Avenue of Section 4, none 2 North, Range 5 East, 5440, De for to the centerline of Carde River Avenue 5 (1998) and 1998 of the Section 4 North 1998 of Section

86 degrees 28 minutes 10 seconds East, 363.81 feet to the point of beginning.

Exement Proced.

All Bird visite, non-exclusive, private road essement and cut-de-sec, part of the Northwest 1/4 of Section 5, 10 mil. 70 keV, 10 mil. 10 mil.

LEGAL DESCRIPTION: Parcel 2

Commercing of the North 1/4 corner of Section 9, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan, thence 501/47/0°E 1079-50 feet design the North-South 1/4 line of said Section 9 to a PLACE OF BECOMING, One section 9 to a PLACE OF BECOMING OF SECTION 1/4 line of said Section 9. Thems: SSST/250°W 303.81 feet; thence NOTATION 244.0.0 NoRTY250°W 303.81 feet to the Photol Respiniency, being open of the Northeast 1/4 of Section 9, containing 2.00 cores of land, more or less, being subject to any essements, conditions or restrictions of record, if nor.

ZONING
THE SITE, AS SHOWN ON THIS SURVEY, IS ZONED IND (INDUSTRIAL DISTRICT) ACCORDING TO THE GENOA TWP. ZONING MAP, LAST REVISED 11/13/14.

A ZONING REPORT/LETTER HAS NOT BEEN PROVIDED TO SMITHGROUP. THE SETBACKS LISTED HEREON REQUIRE ADDITIONAL VERIFICATION BY OTHERS. THE STRACKS LISTED AND SHOWN HEREON WERE TAKEN DIRECTLY FROM THE GENOA TOWNSHIP ZONING ORDINANCE ARTICLE 8, AND ARE INTENDED TO REFLECT THE CURRENT STEL USE WHICH IS VACANT.

FRONT YARD SETBACK - 85 FEET WITH A PARKING LOT AND 50 FEET WITH NO PARKING LOT. THE SITE IS PRESENTLY VACANT

SIDE YARD SETBACK - 25 FEET REAR YARD SETBACK - 40 FEET

FLOODPLAIN NOTE
THE SITE, AS SHOWN ON THIS SURVEY, LIES IN FLOOD AREA "ZONE X",
ACCORDING TO FEDERAL EMERGENCY MANAGMENT ACENCY (FEMA) FLOOD
COUNTY, MICHIGAN, EFFECTIVE DATE SEPTEMBER 17, 2008. ACCORDING TO SAID
PAREL, ZONE X IS DEFINED AS AREAS OF MINIMAL FLOODING.

NOTES

1. HORIZONTAL DATUM IS ACCORDING TO MICHGAN STATE PLANE GRID COORDINATES, SOUTH ZONG (2113), GEOID 2012A, INTERNATIONAL FEET AS UNEAR UNIT OF MEASURE.

VERTICAL DATUM IS ACCORDING TO THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88), WITH BENCHMARKS LISTED HEREON.

SMITHGROUP PERFORMED A TOPOGRAPHIC SURVEY UTILIZING DATA COLLECTED WITH GLOBAL POSTIONING RECEIVERS REFERENCING THE MICHIGAN CONTINUCUSLY OPERATING REFERENCE RETWORK FOR SITE CONTINUC.

4. THIS SURVEY DOES NOT CONSTITUTE A TITLE SEARCH BY THE SURVEYOR. ALL INFORMATION RECARDING LAND BOUNDARY, EASEMENTS, RIGHTS-OF-WAY, AND OTHER INFORMATION THAT MAY AFFECT TITLE WAS GANDED FROM AVAILABLE TAX RECORDS AND/OR RECORDED LEGAL DOCUMENTS FOR ADJOINING PARCELS

MIS-DIG DESIGN TICKET NUMBERS B091651216-008 & B091651936-008 WERE ISSUED FOR THIS PROJECT. BOTH TICKETS MATURED ON JAME 28, 2019. MISS-DISTANKING TICKET NO. B924728257 WAS SUBMITTED ON SEPTEMBER 4, 2019 FOR PARKWAY DRIVE. THE FOLLOWING UTILITY VENDORS RESPONDED TO THE MISS-DIG DESIGN TICKETS.

COMCAST, AT&T, MHOG, CONSUMER'S ENERGY NATURAL GAS, DTE ELECTRIC, ACD.NET,

6. UNDERGROUND UTILITY LOCATIONS WERE DERIVED FROM ACTUAL MEASUREMENTS ON VISIBLE UTILITIES, MISS DIG FLAGS/MARKINGS, AND/OR AVAILABLE RECORDS. THEY SYOLD NOT BE INTERPRETED TO BE EXACT LOCATIONS, NOR SYOLD THEY IT BE ASSUMED THAT THEY ARE THE ONLY UNDERGROUND UTILITIES IN THE AREA.

SUBSURFACE AND ENVIRONMENTAL CONDITIONS WERE NOT EXAMINED AND ARE NOT CONSIDERED A PART OF THIS SURVEY. NO STATEMENT IS MADE CONCERNING THE EXISTENCE OF UNDERGROUND OR OURFREED CONTINUERS OR FACILITIES THAT MAY AFFECT THE USE AND OR DEVELOPMENT OF THIS STE.

ADDITIONAL SPOT ELEVATIONS MAY BE CONTAINED IN THE PROJECT DATABASE, AND ALTHOUGH NOT APPEARMS IN THE RECORD PLAN DOCUMENT DUE TO SCALE AND VISUALITY REPORT OF THE PROJECT PROPRIES. THE PROJECT OF THE PROJECT DATABASE.

9. THE SUBJECT PROPERTY IS CURRENTLY VACANT AND LACKS ESTABLISHED PARKING

10. DURING THE COURSE OF THE SURVEY IT WAS OBSERVED THAT THE LANDS AROUND THE COUTLET PIPE ENTERING THE STIE FROM THE SOUTHERLY ADJUSTANCE PARCEL (TIN: 11-09-100-021) WERE EXCAVATED TO EXPOSE THE COUTLET STRUCTURE. NO ADDITIONAL CONSTRUCTION ACTIVITIES, OR EVIDENCE THEREOF, WAS OBSERVED.

11. THERE ARE NO KNOWN PROPOSED CHANGES TO THE RIGHTS-OF-WAY OF THE ADJUNING STREETS

AT THE TIME OF THE SURVEY, THE SUBJECT PROPERTY HAS NOT YET BEEN VISITED BY AN WETLAND SPECIALIST

TO: PARKPROP, LLC; STEVEN A. CHAPMAN AND DIANE CHAPMAN; OLD REPUBLIC NATIONAL TITLE INSURANCE COMPANY; ATA NATIONAL TITLE GROUP

THIS IS TO CERTIFY THAT THIS MAP OR PLAT AND THE SURVEY ON WHICH IT IS BASED WERE MADE IN ACCORDANCE WITH THE 2016 MINIMUM STANDARD DETAIL. AND ADDRESS OF THE PROPERTY OF THE

SMITHGROUP INC

10/09/2019 DATE CHARLES A LANGOLF PROFESSIONAL SURVEYOR NO. 565210



ST. JOSEPH MERCY LIVINGSTON

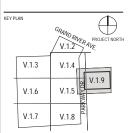
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ALTA/NSPS LAND TITLE SURVEY - PARCEL 2



11404.000 V.1.9







— — — PROPERTY
— — EASEMENT



St. Joseph Mercy Livingston

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REVISED PUD SUBMITTAL		18 FEB 2020

SEALS AND SIGNATUR



KEYF



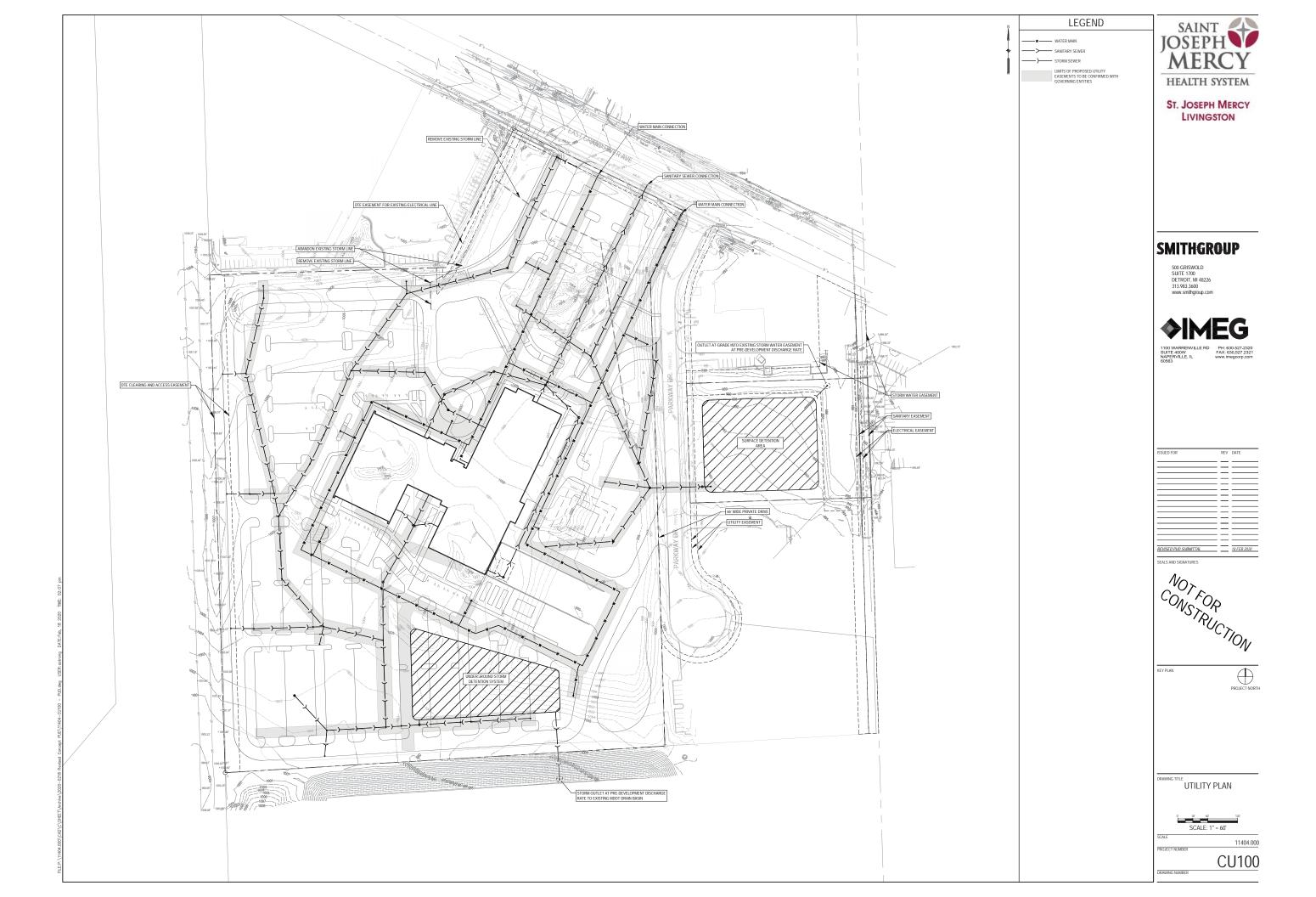
SITE PLAN REVISION SKETCH



PROJECT NUMBER

11404.000

CS101









MAIN ENTRANCE: PARKING LOT









6 REAR ENTRANCE: COURTYARD



WEST FAÇADE
(HOSPITAL VOLUME)
26%
26%
25%
46%

WEST FAÇADE (MOB VOLUME) 65% 32% 38%

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REVISED PUD SUBMITTAL	==	18 FEB 2020





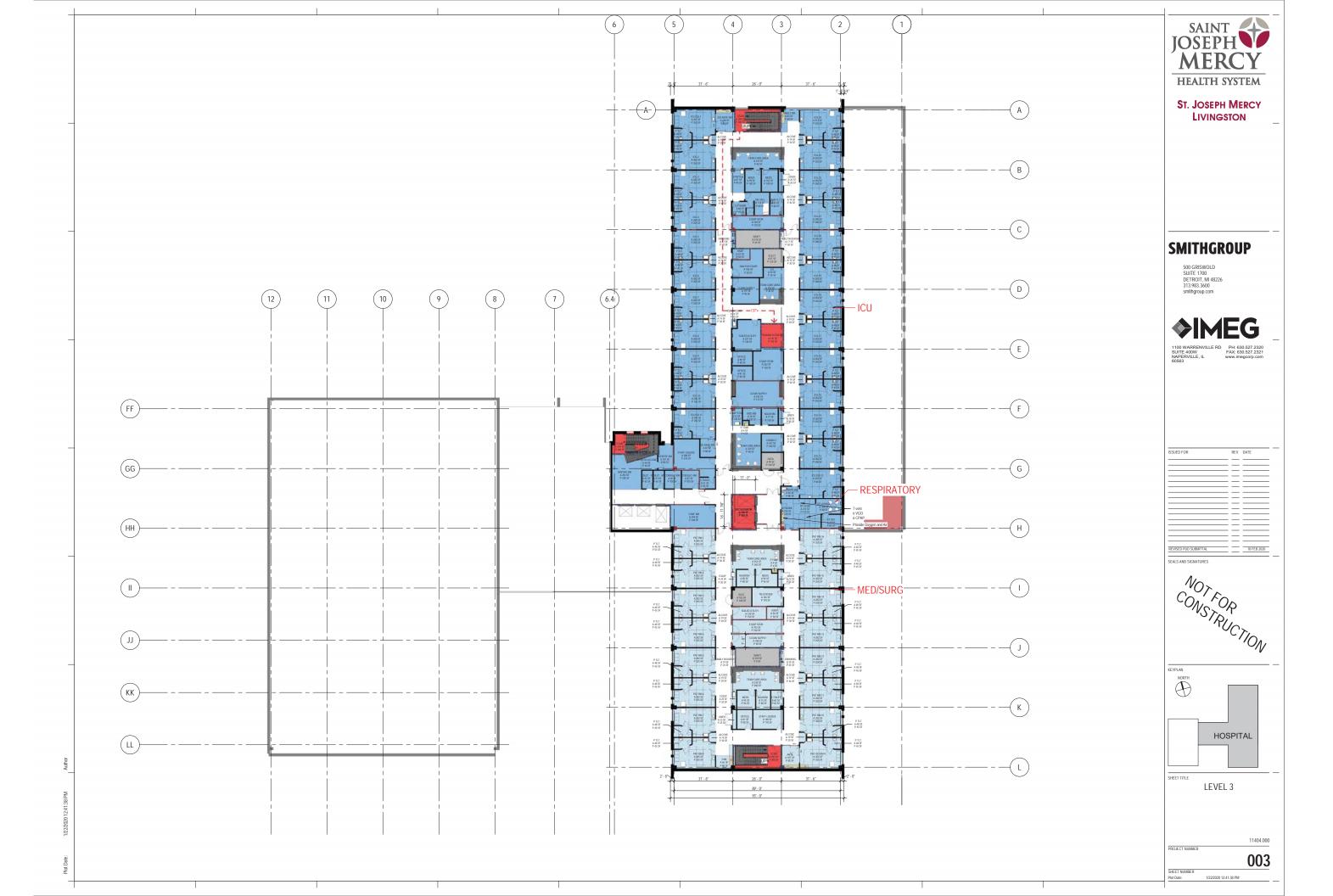
ARCHITECTURAL VIEWS

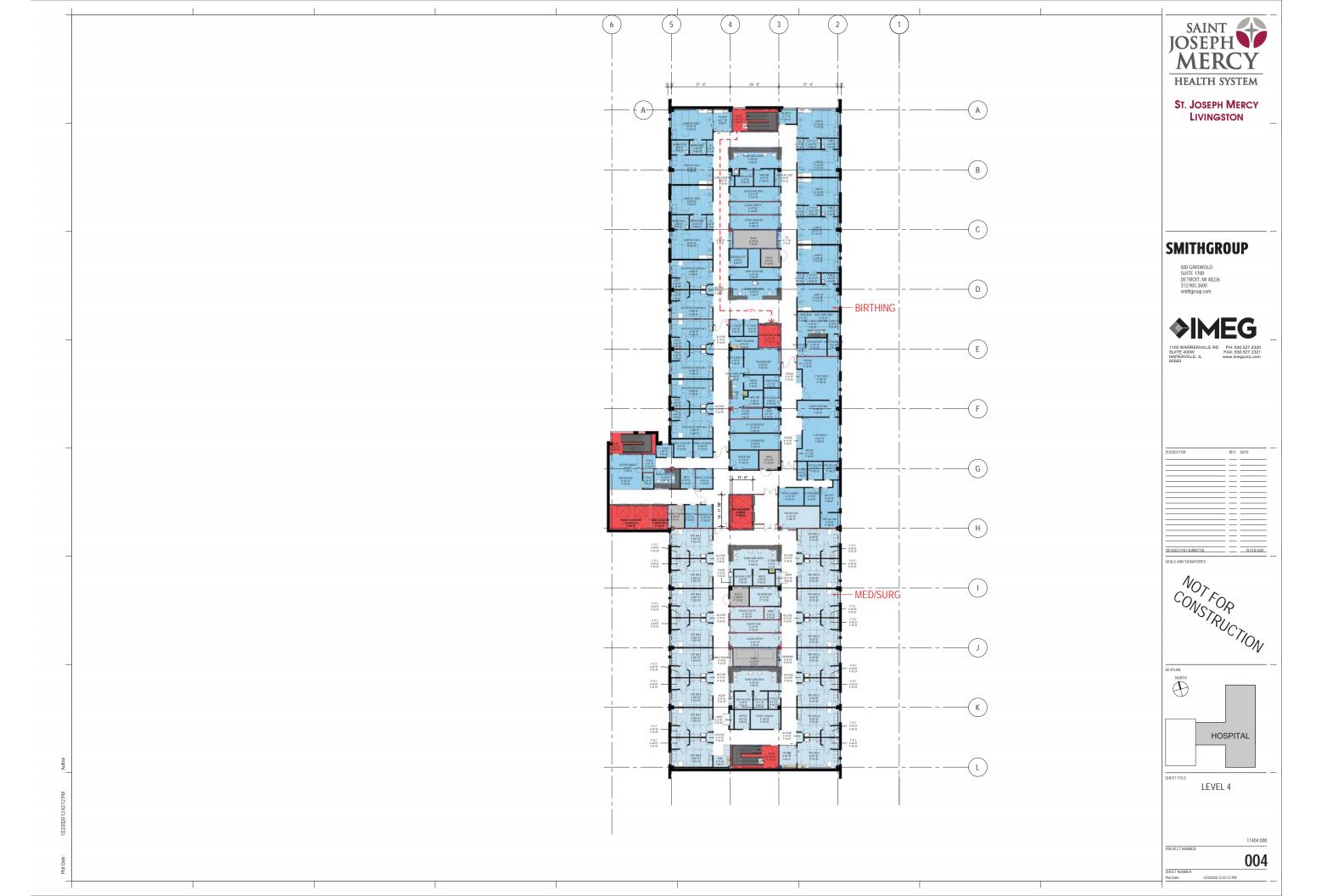
SCALE	11404.00
PROJECT NUMBER	
	A-100
DRAWING NUMBER	











HOSPITAL + MOB FAÇADE MATERIALS

New Hospital and Medical Office Building

Trinity Health St. Joseph - Livingston

1/29/2020

MATERIAL	SOUTH FAÇADE	NORTH FAÇADE	WEST FAÇADE (MOB VOLUME)	WEST FAÇADE (HOSPITAL VOLUME)	EAST FAÇADE (HOSPITAL VOLUME)
MASONRY	57%	32%	65%	26%	36%
GLAZING	23%	28%	32%	26%	19%
METAL PANEL	17%	37%	3%	2%	2%
TERRACOTTA	3%	3%		46%	43%

NOTE: the exterior materials shall be high quality, natural in appearance, attractive, durable, expressive, and inviting; cladding may include but shall not be limited to decorative masonry, stone, high performance glazing, and premium metal panels.



GENOA CHARTER TOWNSHIP Application for Re-Zoning

APPLICANT NAME: BMH Realty, L.L.C.	_ ADDRESS:ADDRESS
OWNER NAME: BMH Realty, L.L.C.	_ ADDRESS:ADDRESS ADDRESS ADDRESS ADDRESS ADDRESS
PARCEL #(s):PARCEL #(s):	PRIMARY PHONE: (810)229-6323
EMAIL 1: BMaynes@corriganoil.com EMA	

We, the undersigned, do hereby respectfully make application to and petition the Township Board to amend the Township Zoning Ordinance and change the zoning map of the township of Genoa as hereinafter requested, and in support of this application, the following facts are shown:

A. REQUIRED SUBMITTAL INFORMATION

- 1. A legal description and street address of the subject property, together with a map identifying the subject property in relation to surrounding properties;
- 2. The name, signature and address of the owner of the subject property, a statement of the applicant's interest in the subject property if not the owner in fee simple title, and proof of consent from the property owner;
- 3. It is desired and requested that the foregoing property be rezoned from:

 OSD to GCD
 GCD (entire site) to RDPUD
- 4. A site plan illustrating existing conditions on the site and adjacent properties; such as woodlands, wetlands, soil conditions, steep slope, drainage patterns, views, existing buildings, sight distance limitations, relationship to other developed sites. and access points in the vicinity;
- 5. A conceptual plan demonstrating that the site could be developed with representative uses permitted in the requested zoning district meeting requirements for setbacks, wetland buffers access spacing, any requested service drives and other site design factors;
- A written environmental impact assessment, a map of existing site features as described in Article 18 describing site features and anticipated impacts created by the host of uses permitted in the requested zoning district;
- 7. A written description of how the requested rezoning meets Sec. 22.04 "Criteria for Amendment of the Official Zoning Map."
- 8. The property in question shall be staked prior to the Planning Commission Public Hearing.

B. DESCRIBE HOW YOUR REQUESTED RE-ZONING MEETS THE ZONING ORDINANCE CRITERIA FOR AMENDING THE OFFICIAL ZONING MAP:

1. How is the rezoning consistent with the goals, policies and future land use map of the Genoa Township Master Plan, including any subareas or corridor studies. If not consistent, describe how conditions have changed since the Master Plan was adopted?

The proposed uses are consistent with the Master Plan. The site will provide a cohesive, cluster of general commercial businesses that will serve Genoa and the surrounding communities. The businesses will also serve the needs of the residents and employees within a 2-mile radius.

Th	e site is physically, geologically, and hydrologically suitable for the uses that are proposed with the rezoning.
3.	Do you have any evidence that a reasonable return on investment cannot be received by developing the property with one (1) of the uses permitted under the current zoning?
De	espite development work and consistent exposure to the market, it has not been possible to achieve a
rea	sonable return on investment for the property for one permitted use, as-zoned. The physical and access constraints
of	development around the existing gas station, and the split zoning also make development for one use unpalatable.
4.	How would all the potential uses allowed in the proposed zoning district be compatible with surrounding uses and zoning in terms of views, noise, air quality, the environment, density, traffic impacts, drainage and potential influence on property values?
Tł	ne proposed RDPUD uses are compatible with the surrounding commercial uses in the
G	rand River Corridor. The proposed uses will not have a negative effect on the views, noise,
ai	r quality, density, traffic, drainage and property values of the existing uses in the area.
5.	Are infrastructure capacity (streets, sanitary sewer, water, and drainage) and services (police and fire protection, etc.) sufficient to accommodate the uses permitted in the requested district?
Th	ne existing infrastructure capacity and services are sufficient for the uses. The project includes
th	e construction of a retention pond to collect the storm water from the development.
6.	Is there a demonstrated demand in Genoa Township or the surrounding area for the types of uses permitted in the requested zoning district? If yes, explain how this site is better suited for the zoning than others which may be planned or zoned to accommodate the demand.
Apa	art from the existing gas station, the currently-proposed uses are allowed as of right on the eastern half of the property, demonstrating a need perceived by
the	Township, that Owner seeks only to expand to the full site. In addition to this, the 2018 Annual Self Storage Almanac demonstrates a square-feet
pei	person storage need in Michigan of 7.12. In the immediate submarket around the Property, there is an excess demand of 52,000 - 158,300 sf.
7.	If you have a particular use in mind, is another zoning district more appropriate? Why should the Township re-zone the land rather than amend the list of uses allowed in another zoning district to accommodate your intended use?
Th	ne proposed plan could be effectuated through the use of GC zoning with a text amendment
aı	nd variances, however, the site as currently configured has split zoning between GCD
 aı	nd OS. As a result, a simple text amendment is not sufficient to effectuate the plan.

8. Describe any deed restrictions which could potentially affect the use of the property. There are no deed restrictions that affect the use of the property.
C. AFFIDAVIT
The undersigned says that they are the amember of owner (owner, lessee, or other specified interest) involved in this petition and that the foregoing answers and statements herein contained and the information herewith submitted are in all respects true and correct to the best of his/her knowledge and belief.
BY: Michael B. Corrigan
ADDRESS: 775 N. Second St, Brighton, MI 48116
ADDRESS: 775 N. Second St, Brighton, MI 48116 SIGNATURE
· ·
The following contact should also receive review letters and correspondence: Prod Maynos and Dan Boorstoin https://progressing.com/
Name: Brad Maynes and Dan Boorstein Email: bmaynes@corriganoil.com; djb@sviprop.com
Business Affiliation: BMH Realty, L.L.C. and SVI Properties, LLC
FEE EXCEEDANCE AGREEMENT
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PROJECT NAME:
PROJECT LOCATON & DESCRIPTION: Property bounded by Grand River, Lawson and White Horse Drive.
Redevelopment of several adjacent, stand-alone commercial uses into a new coordinated, mixed-use site, currently proposed to include a gas station/c-store, self storage, retail, food service, and office uses.
SIGNATURE: Michael & Colly DATE: 2-19-2020
PRINT NAME: Michael B. Corrigan PHONE: 810-229-6323
COMPANY NAME & ADDRESS: BMH Realty, L.L.C., 775 N. Second St, Brighton, MI 48116



GENOA CHARTER TOWNSHIP APPLICATION Planned Unit Development (PUD)

ΑP	PLIC	CANT NAME: BMH Realty L.L.C.
		CANT EMAIL: bmaynes@corrigan.com; djb@sviprop.com
ΑP	PLIC	CANT ADDRESS & PHONE: 775 N. Second Street, Brighton, MI 48116 (810) 229-6323
OV	VNE	R'S NAME: BMH Realty, L.L.C.
		R ADDRESS & PHONE: 775 N. Second Street, Brighton, MI 48116 (810) 229-6323
		ODE(S): 11-09-200-005, -010, -014, -015, -016 & -017
QU	JALI	FYING CONDITIONS (To be filled out by applicant)
1.	A P	PUD zoning classification may be initiated only by a petition.
2.	It is	s desired and requested that the foregoing property be rezoned to the following type of PUD designation:
		Residential Planned Unit Development (RPUD) Planned Industrial District (PID) Mixed Use Planned Unit Development (MUPUD) Redevelopment Planned Unit Development (RDPUD) Non-residential Planned Unit Development (NRPUD) Town Center Planned Unit Development (TCPUD)
3.		e planned unit development site shall be under the control of one owner or group of owners and shall be vable of being planned and developed as one integral unit.
EX	PLA	IN The subject site is wholly owned by BHM Realty, LLC and is being planned as a phased plan
mi:	xed ι	use plan with multiple uses and each use will have complimentary customers and traffic demands.
4.		e site shall have a minimum area of twenty (20) acres of contiguous land, provided such minimum may reduced by the Township Board as follows:
	A.	The minimum area requirement may be reduced to five (5) acres for sites served by both public water and public sewer.
	B.	The minimum lot area may be waived for sites zoned for commercial use (NSD, GCD or RCD) where the site is occupied by a nonconforming commercial, office or industrial building, all buildings on such site are proposed to be removed and a new use permitted within the underlying zoning district is to be established. The Township Board shall only permit the PUD on the smaller site where it finds

that the flexibility in dimensional standards is necessary to allow for innovative design in redeveloping the site and an existing blighted situation will be eliminated. A parallel plan shall be provided showing how the site could be redeveloped without the use of the PUD to allow the

minimum necessary to allow redevelopment of the site, while still meeting the spirit and intent of the ordinance.

- C. The PUD site plan shall provide one or more of the following benefits not possible under the standards of another zoning district, as determined by the Planning Commission:
 - preservation of significant natural or historic features
 - a complementary mixture of uses or a variety of housing types
 - common open space for passive or active recreational use
 - mitigation to offset impacts
 - redevelopment of a nonconforming site where creative design can address unique site constraints.
- D. The site shall be served by public sewer and water. The Township may approve a residential PUD that is not served by public sewer or water, provided all lots shall be at least one (1) acre in area and the requirements of the County Health Department shall be met.

Siz	e of property is acres.
	SCRIBE BELOW HOW THE REQUESTED PUD DESIGNATION COMPLIES WITH OREMENTIONED MINIMUM LOT SIZE REQUIREMENTS.
The	e entire 5.12 acres is serviced by sewer and water. The proposed mix of uses are all complimentary uses in terms of
cus	stomer demand and traffic generation. Each use has different traffic and parking demands and the provided parking
and	d access points with the surrounding streets each balance harmoniously.
	ANDARDS FOR REZONING TO PLANNED UNIT DEVELOPMENT (RESPOND HERE OR THIN THE IMPACT STATEMENT) How would the PUD be consistent with the goals, policies and future land use map of the Genoa
	Township Master Plan, including any subarea or corridor studies. If conditions have changed since the Master Plan was adopted, the consistency with recent development trends in the area;
Se	ee Impact Statement
_	
2.	The compatibility of all the potential uses in the PUD with surrounding uses and zoning in terms of land suitability, impacts on the environment, density, nature of use, traffic impacts, aesthetics, infrastructure and potential influence on property values;
Se	ee Impact Statement
3.	The capacity of infrastructure and services sufficient to accommodate the uses permitted in the requested district without compromising the "health, safety and welfare" of the Township;
Se	ee Impact Statement
-	100 DE 10
_	

AFFIDAVIT The undersigned says that they are the owner (owner, lessee, or other specified interest) involved in this petition and that the foregoing answers and statements herein contained and the information herewith submitted are in all respects true and correct to the best of his/her knowledge and belief. BY: Michael B. Corrigan ADDRESS: 775 N. Second Street, Brighton, Michigan 48116 Contact Information - Review Letters and Correspondence shall be forwarded to the following: 1.Brad Maynes & 2. Dan Boorstein of BMH Realty L.L.C. and SVI Properties, L.L.C. Name Business Affiliation FEE EXCEEDANCE AGREEMENT As stated on the site plan review fee schedule, all site plans are allocated two (2) consultant reviews and one (1) Planning Commission meeting. If additional reviews or meetings are necessary, the applicant will be required to pay the actual incurred costs for the additional reviews. If applicable, additional review fee payment will be required to pay the actual incurred costs for the additional reviews. If applicable, additional review fee payment will be required concurrent with submittal to the Township Board. By signing below, applicant indicates agreement and full understanding of this policy. PROJECT NAME: BMH - PREMIER GENOA PUD PROJECT LOCATON & DESCRIPTION: Property bounded by Grand River, Lawson and White Horse Drives. Redevelopment of existing mixed use site with new set of mixed uses including gas/C-store, set storage, retail, food and office uses. SIGNATURE: Marchael B. Corrigan (810) 229-6323	4. The apparent demand for the types of uses permitted in the PUD;
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COMPANY NAME & ADDRESS: BMH, LLC 775 N. Second Street, Brighton, MI 48116	As stated on the site plan review fee schedule, all site plans are allocated two (2) consultant reviews and one (1) Planning Commission meeting. If additional reviews or meetings are necessary, the applicant will be required to pay the actual incurred costs for the additional reviews. If applicable, additional review fee payment will be required concurrent with submittal to the Township Board. By signing below, applicant indicates agreement and full understanding of this policy. PROJECT NAME: BMH - PREMIER GENOA PUD Property bounded by Grand River, Lawson and White Horse Drives. PROJECT LOCATON & DESCRIPTION: Property bounded by Grand River, Lawson and White Horse Drives. Redevelopment of existing mixed use site with new set of mixed uses including gas/C-store, self storage, retail, food and office uses. PRINT NAME: Michael B. Corrigan PHONE: (810) 229-6323



GENOA CHARTER TOWNSHIP Application for Site Plan Review

TO THE GENOA TOWNSHIP PLANNING COMMISSION AND TOWNSHIP BOARD:

APPLICANT NAME & ADDRESS: BMH Realty L.L.C., 775 N. Second St, Brighton, MI 48116
If applicant is not the owner, a letter of Authorization from Property Owner is needed.
OWNER'S NAME & ADDRESS: BMH Realty, L.L.C., 775 N. Second St, Brighton, MI 48116
OWNER'S NAME & ADDRESS: DWITT Realty, E.E.G., 775 N. Second St, Brighton, Wil 48116 SITE ADDRESS: 4525 & 4433 E. Grand River & 1098 Lawson Dr PARCEL #(s): 11-09-200-005, -010, -014, -015, -016, & -01
APPLICANT PHONE: (810) 229-6323 OWNER PHONE: (810) 229-6323
OWNER EMAIL: BMaynes@corriganoil.com & mark@corriganoil.com
LOCATION AND BRIEF DESCRIPTION OF SITE: Site bounded by Grand River, Lawson & White Horse Drive
The site is currently occupied by a gas station, two retail buildings, one small industrial building with outdoor storage and a vacant parcel
BRIEF STATEMENT OF PROPOSED USE: A mixed use development which will include the following uses; a gas station
with propane and kerosene services (existing), a drive-through, climate controlled self storage business with ancillary retail sales
and rental of small flex-office spaces and other buildings featuring office, retail and restaurant uses.
THE FOLLOWING BUILDINGS ARE PROPOSED: A 37,275 sq ft multi-purpose, adaptive use building, currently proposed to be
up-fit with equipment to operate a drive-through, climate controlled building, with the potential for small flex-office spaces for rent, a 7,000 sq ft building
appropriate for use for office, retail, restaurant, or a mix of those uses, a 2,336 sq ft out parcel pad suitable for use for retail or drive through fast food use,
and the existing gas station with a convenience store and related (existing) ancillary propane and kerosene sales.
I HEREBY CERTIFY THAT ALL INFORMATION AND DATA ATTACHED TO AND MADE PART OF THIS APPLICATION IS TRUE AND ACCURATE TO THE BEST OF MY KNOWLEDGE AND BELIEF.
BY: Mulau & arry
ADDRESS:

Contact Information - Review Letters and Correspondence shall be forwarded to the following:		
1.) Brad Maynes and Dan Boorstein of BMH Realty, L.L.C.& SVI Properties, LLC at BMaynes@configuresl.com; djb@sv/prop.com		
Name	Business Affiliation	E-mail Address

FEE EXCEEDANCE AGREEMENT

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SIGNATURE: PHONE: 810-229 PRINT NAME Michael B. Corrigan

ADDRESS: 775 N. Second St, Brighton, MI 48116



Planning Commission Genoa Township 2911 Dorr Road Brighton, Michigan 48116

Attention:	Kelly Van Marter, AICP	
	Planning Director and Assistant Township Manager	
Subject:	BHM-Premier Genoa – PUD Rezoning and Conceptual Plan Review #2	
Location:	4525 and 4433 E. Grand River Avenue – northwest corner of Grand River and	
	Lawson Drive	
Zoning:	GCD General Commercial District and OSD Office Service District	

Dear Commissioners:

At the Township's request, we have reviewed the revised submittal from BHM Realty, LLC requesting establishment of a PUD for 5.12 acres of land at the northwest corner of Grand River and Lawson.

A. Summary

- 1. PUD Qualifying Conditions: the minimum PUD site area (20 acres) may be reduced to 5 for sites served by public utilities. Any engineering, utility or public service concerns must be addressed.
- 2. Rezoning Criteria:
 - a. The requests are consistent with the Township Master Plan.
 - b. Per the Zoning Ordinance, uses within the RDPUD overlay are limited to those allowed in the underlying zoning designation, which is proposed as GCD.
 - c. The applicant requests two use deviations to allow gas stations and climate-controlled indoor commercial storage as by right uses. The reference to gas stations in Exhibit D should be limited to a single such use.
 - d. OSD to GCD rezoning is appropriate and use of the RDPUD overlay seems reasonable provided the provisions of Section 10.03.04 are met.

3. Conceptual PUD Plan:

- a. All existing buildings must either be removed or renovated as part of a RDPUD. Two of the three buildings will be removed, while the gas station/convenience store building will be improved. We suggest additional enhancement of the fuel pump canopy/support columns.
- b. The applicant must clarify the intended use of the future building in the southeast corner of the property. More specifically, the concept plan notes a drive-through restaurant; however, neither the GCD nor Exhibit D allow such uses.
- c. The request includes a dimensional deviation for a reduced front building setback along Lawson Drive.
- d. As one of the primary entranceways to the PUD, we suggest additional plantings be provided in the southeast corner of the property.
- e. The future use/building in the southeast corner of the property could result in a drive-through lane in both the Grand River and Lawson front yards. Its visibility will need to be mitigated either via additional landscaping/screening or a revised building layout/circulation pattern.
- f. Deviations are sought from the building material standards of Section 12.01 for both the climate-controlled indoor commercial storage building and the commercial/office building.
- g. A deviation is requested from the window requirements of Section 7.02.02(z) for the climate-controlled indoor commercial storage building.

5. PUD Agreement:

- a. Exhibit D needs to incorporate a reference to the applicable use requirements of Section 7.02.02.
- b. We suggest consideration be given to elimination of some of the uses listed.
- c. We request that channel-cut lettering be required for new wall signage.

248.586.0505 www.safebuilt.com



Aerial view of site and surroundings (looking north)

B. Proposal/Process

The request is to establish a Redevelopment Planned Unit Development (RDPUD) for approximately 5.12 acres of land at the northwest corner of Grand River and Lawson. Because of the split-zoned nature of the subject area (OSD and GCD), and the fact that the RDPUD is an overlay district, the revised request includes rezoning of the OSD land to GCD (as was previously suggested).

The conceptual PUD plan proposes retention of the existing gas station/convenience store, as well as 3 new buildings – a climate-controlled self-storage building (37,275 square feet of ground floor area), a multi-use building for office, retail, restaurant or a combination thereof (7,000 square feet of ground floor area), and an outparcel for retail, commercial or a drive-through restaurant (2,310 square feet of ground floor area).

The site currently contains 3 buildings, 2 of which will be demolished to accommodate the proposed development. As noted above, the existing gas station/convenience store building will remain.

Procedurally, following the required public hearing, the Planning Commission is put forth recommendations to the Township Board on the OSD to GCD rezoning, RDPUD overlay zoning, conceptual PUD plan, Environmental Impact Statement and draft PUD Agreement.

C. Qualifying Conditions

We have reviewed the proposal for compliance with the PUD Qualifying Conditions (Section 10.02), as follows:

- 1. Single Ownership. The application forms identify BHM Realty, LLC as the property owner.
- **2. Initiated by Petition.** The request has been initiated by the property owner, including applications for Planned Unit Development, rezoning and site plan review.

- **3. Minimum Site Area.** The Ordinance allows the Township to reduce the 20-acre minimum requirement for a PUD to 5 acres for sites served by public water and sanitary sewer. The submittal identifies the site area as 5.12 acres and notes access to necessary public utilities.
- **4. Benefits.** The submittal references a complementary mixture of uses and includes demolition of two existing, relatively unsightly buildings as part of the project. The proposal will also result in a cohesive development with shared drives and parking, and no additional curb cuts along Grand River Avenue.
- **5. Sewer and Water.** The site is currently served by public water and sanitary sewer; however, the Commission should consider any technical comments provided by the Township Engineer and/or Utilities Director under this criterion.

D. Rezoning Criteria

We have reviewed the proposal for compliance with the Criteria for Amendment of the Official Zoning Map (Section 22.04), as follows:

1. Consistency with the goals, policies and future land use map of the Genoa Township Master Plan, including any subarea or corridor studies. If conditions have changed since the Master Plan was adopted, the consistency with recent development trends in the area.

The Township Master Plan and Future Land Use map identify the subject site as General Commercial, which is compatible with GCD zoning. This category in intended for "businesses which serve the requirements of the community at large including Genoa Township, Howell, Brighton, and pass-by traffic along Grand River Avenue."

The site currently contains both GCD and OSD zoning. The revised request includes rezoning of the OSD land to GCD, since the RDPUD is an overlay district.

We are of the opinion that both requests (OSD to GCD and addition of the RDPUD overlay) are consistent with the Township Master Plan.

2. Compatibility of the site's physical, geological, hydrological and other environmental features with the host of uses permitted in the proposed zoning district.

Much of the subject site was previously developed and there are no apparent environmental conditions limiting re/development as a PUD.

3. The ability of the site to be reasonably developed with one (1) of the uses permitted under the current zoning.

The rezoning application form states that the split zoning of OSD and GCD, along with the presence of the existing gas station/convenience store have created difficulties for unified re/development of the subject parcels.

4. The compatibility of all the potential uses allowed in the proposed zoning district with surrounding uses and zoning in terms of land suitability, impacts on the environment, density, nature of use, traffic impacts, aesthetics, infrastructure and potential influence on property values.

The RDPUD is an overlay district, which allows the host of permitted and special land uses of the underlying zoning district(s). As proposed, the underlying district would be GCD. As such, the host of uses allowed in the RDPUD are essentially the same as current zoning (though the OSD designation would be removed).

An exception of note is the continued operation of the gas station, which is not allowed in the GCD.

While this allowance under GCD/RDPUD zoning requires the Township to modify the host of permitted uses, it is a lawful nonconforming use with a long history at this location, and its continued operation is anticipated to be compatible with the surrounding area.

5. The capacity of Township infrastructure and services sufficient to accommodate the uses permitted in the requested district without compromising the "health, safety and welfare" of the Township.

As noted under our review of the PUD Qualifying Conditions, the site is served by public utilities. Additionally, as noted above, the host of allowable uses are limited to those allowed under GCD zoning.

The primary infrastructure concern appears to be stormwater management. As such, the proposal includes a new retention pond north of the subject site across White Horse Drive.

The submittal also includes a traffic study, the conclusions of which are that the proposal is anticipated to have minimal impact on traffic conditions in the surrounding area. The conclusions also recommend optimization and lengthening of signal timing at the Grand River/Lawson intersection.

The applicant must address any additional comments provided by the Township Engineer, Utilities Director and/or Brighton Area Fire Authority related to this criterion.

6. The apparent demand for the types of uses permitted in the requested zoning district in the Township in relation to the amount of land in the Township currently zoned to accommodate the demand.

Similar to comments above, the host of uses allowed within the proposed RDPUD are the same as allowed under GCD zoning.

The most prominent new use/building proposed is a climate-controlled storage building and the application form notes an apparent shortage of storage uses withing the "immediate submarket" surrounding the subject site.

7. Where a rezoning is reasonable given the above criteria, a determination the requested zoning district is more appropriate than another district or amending the list of permitted or Special Land Uses within a district.

GCD zoning for the entire subject site matches the planned usage in this area.

Use of the PUD option will result in a more unified overall development, particularly with respect to shared drives and parking, and consistent site design features (landscaping, lighting, etc.).

As a previously developed site, use of RDPUD appears to be reasonable provided the provisions of Section 10.03.04 are met.

8. The request has not previously been submitted within the past one (1) year, unless conditions have changed or new information has been provided.

This request has not been submitted within the past year.

E. Conceptual PUD Plan

We have reviewed the proposal for compliance with the Redevelopment PUD standards (Section 10.03.04), as follows:

1. Existing Conditions. Establishment of a RDPUD requires that all existing buildings be removed or renovated. As previously noted, 2 of the 3 existing building are to be removed, while the gas station/convenience store is to remain.

The proposed elevation drawings for the gas station/convenience store building (Sheet A-4) note the removal of EIFS on the north and south elevations. These areas will be replaced by paneling with a wood appearance.

Additional improvements include the installation of vertical panels with a masonry appearance and painting of the existing block that covers much of the building. The drawings also note removal of the small accessory building on the north side of the principal building.

As noted in our initial review letter, we believe the fuel pump canopies would benefit from use of a masonry material on the support columns.

2. Land Use. As previously noted, the RDPUD is an overlay district that allows uses of the underlying zoning district(s). Given the request to create GCD zoning for the entire subject site, the allowable uses are those listed for the GCD in Table 7.02 of the Township Zoning Ordinance.

Exhibit D of the draft PUD Agreement identifies the host of allowable uses (both by right and those requiring special approval). The list matches Table 7.02 with two proposed exceptions:

- Climate-controlled indoor commercial storage would be a by right use, as opposed to a special land use; and
- Auto/gasoline service stations would be a by right use, as opposed to not being allowed.

Provided the Township is agreeable to these deviations, we suggest that Exhibit D also include a reference to the use requirements of Section 7.02.02, as applicable.

Furthermore, we request that the applicant refine the language regarding auto/gasoline service stations, such that only a single station is allowed (with the intent being continued operation of the existing use).

The applicant and/or Township may also wish to eliminate some of the uses from this PUD. For instance, vehicle sales, auto repair and amusement parks do not necessarily seem appropriate.

Lastly, the conceptual PUD plan and other elements of the submittal refer to a drive-through restaurant, though such uses are not allowed in the GCD, nor are they listed in Exhibit D; however, a coffee shop with a drive-through would be allowed with special land use approval. We request the applicant clarify their intention for this use/building.

3. Dimensional Standards. A revised dimensional table is included on the Conceptual PUD Plan (Sheet PD) noting the need for a dimensional deviation from the front building setback along Lawson Drive (70' required; 59' proposed).

The initial submittal included lot coverage calculations (for both buildings and impervious surface). We request this information be included in the dimensional table.

The remainder of the conceptual PUD plan meets dimensional standards.

4. Site Design Elements. The submittal includes a preliminary landscape plan that generally depicts compliant landscaping overall. There are some individual components (Grand River and Lawson greenbelts) that are slightly deficient, though they are offset by excess plantings in other locations (White Horse greenbelt).

Our initial review suggested that additional plantings be provided in the southeast corner of the property. The revised submittal includes information from the applicant noting that since the exact nature of the future use/building in this area of the property is unknown, they do not wish to install landscaping at this time.

Since the timeframe for development of this area is unknown, we continue to recommend that additional plantings be provided. At a minimum, the required greenbelt plantings should be provided given that this intersection is one of the primary entranceways to the PUD.

Furthermore, the proposed layout of the future use/building in this area would result in a drive-through lane within both the Grand River and Lawson front yards. This is a relatively unusual design and visibility of a drive-through should be mitigated either via landscaping or a revised building layout/circulation pattern.

The submittal does not include a preliminary lighting plan, though the revised submittal materials state that the PUD will utilize consistent site lighting throughout the development (as previously requested).

Additional details include masonry-faced waste receptacle enclosures and two types of fencing – 4' tall ornamental and 6' tall chain link with a fabric backing.

Lastly, we request the applicant incorporate a requirement for channel-cut lettering on new wall signs as part of the PUD Agreement. This is a relatively common request, and has been incorporated into most PUDs in the Township.

All of these details will be reviewed for compliance with Ordinance standards and/or the PUD Agreement at the time of final PUD site plan submittal (if the RDPUD is granted).

5. Building Design and Materials. Building elevation drawings are included for the gas station/convenience store, climate-controlled self-storage and retail buildings; however, details are not provided for the proposed building near the intersection of Grand River and Lawson.

Neither the proposed climate-controlled indoor commercial storage building nor the commercial/office building fully comply with the building material standards of Section 12.01.

The revised submittal includes a detailed request/rationale for the design deviations sought.

Lastly, as the applicant is aware, the climate-controlled indoor commercial storage use/building is subject to the requirements of Section 7.02.02(z). The request includes a deviation from the window requirements of this section; however, the remainder of the requirements must be met (unless further deviation is sought and granted).

Should you have any questions concerning this matter, please do not hesitate to contact our office.

Respectfully,

SAFEBUILT STUDIO

Brian V. Borden, AICP Planning Manager



March 4, 2020

Ms. Kelly Van Marter Genoa Township 2911 Dorr Road Brighton, MI 48116

Re: Premier Genoa PUD Site Plan Review No. 2

Dear Ms. Van Marter:

Tetra Tech conducted a second review of the proposed Premier Genoa PUD site plan last dated February 18, 2020. The plans and impact assessment were submitted by Desine Inc. on behalf of Premier Genoa LLC. The site consists of six partially developed parcels totaling 5.12 acres at 4525 E Grand River Avenue at the northwest quadrant of the Lawson Drive and Grand River Avenue intersection. The petitioner is proposing to retain the existing Sunoco gas station and develop 42,740 net rentable square feet of self-storage space, 7,000 square feet of retail space, 3,700 square feet of flex-office space, and 2,336 square feet of new food-based retail. Site improvements will include the extension of the existing 8-inch water main on the northwest corner of the site and extending municipal water and sanitary sewer services to the proposed buildings. We offer the following comments:

GENERAL NOTES

1. The petitioner should obtain approval from the Brighton Area Fire Authority prior to site plan approval.

DRAINAGE AND GRADING

1. Soil boring data was provided for the proposed retention basin area. The existing soils and water level appear to be adequate for the proposed retention pond.

UTILITIES

- 1. After final site plan approval, the petitioner will need to submit construction plans to MHOG Sewer and Water Authority for review and approval. The connection to the existing 8-inch water main on Lawson Drive will be an 8-inch to 8-inch connection. This type of connection cannot be achieved with a live tap and more detail on this connection will be necessary on the construction plans.
- 2. Based on the Genoa Township Residential Equivalency (REU) Table, the proposed PUD will account for approximately 11 REUs, not including the REUs for the existing gas station on site. This may change based on the final use of the proposed buildings.

Ms. Kelly Van Marter Re: Premier PUD Site Plan Review No. 2 March 4, 2020

Page 2

TRAFFIC

1. A traffic impact assessment was conducted by Rowe Professional Services Company and provided with the site plan submittal. The recommendations provided in the study correspond to what is shown on the plans and we do not have any concerns with the projected traffic impacts.

The revised site plan addresses our previous comments and we have no further engineering related concerns to the proposed PUD.

Please call or email if you have any questions.

Sincerely,

Gary J. Markstrom, P.E.

Vice President

Shelby Scherdt

helby Schordt

Project Engineer

BRIGHTON AREA FIRE AUTHORITY

615 W. Grand River Ave. Brighton, MI 48116 o: 810-229-6640 f: 810-229-1619

February 28, 2020

Kelly VanMarter Genoa Township 2911 Dorr Road Brighton, MI 48116

RE: BMH - Premier PUD

4525 E. Grand River, Howell, MI

Genoa Twp., MI

Dear Kelly:

The Brighton Area Fire Authority has reviewed the above-mentioned site plan. The plans were received for review on February 20, 2020, and the drawings are dated February 19, 2020. The project is a 5.12-acre site comprised of six parcels to be used for the proposed redevelopment and construction of a two-story IIB construction 74,500 square foot climate-controlled storage (S-1) structure with accessory office space. Additionally, there are two smaller out structures consisting of a proposed single story 7,000 sq. ft. mixed-use at the west end of the parcel and a 2,336 sq.ft mixed-use at the east end of the parcel. The existing gas station will remain and will undergo minor renovation. This plan review is based on the requirements of the International Fire Code (IFC) 2018 edition.

1. Provide the size of the above-ground LPG tank so proper separation distance from the structure can be determined. (Discussions with the engineer indicated that the proposed LPG tank size was 5,000-gallons. Table 6104.3 indicates that tanks that exceed 2,000-gallons are required to be located a minimum of 50-feet from buildings. As there is no alternate location to place the tank, they have agreed to maintain the size no more than 2,000-gallons.)

Additional comments will be given during the building plan review process (specific to the building plans and occupancy). The applicant is reminded that the fire authority must review the fire protection systems submittals (sprinkler & alarm) prior to permit issuance by the Building Department and that the authority will also review the building plans for life safety requirements in conjunction with the Building Department. If you have any questions about the comments on this plan review please contact me at 810-229-6640.

Cordially,

Rick Boisvert, CFPS Fire Marshal Comments previously made by the Township Attorney but not incorporated are included as call-outs in the following pages of the PUD Agreement.

STATE OF MICHIGAN COUNTY OF LIVINGSTON GENOA CHARTER TOWNSHIP

PLANNED UNIT DEVELOPMENT AGREEMENT

THIS PLANNED UNIT DEVELOPMENT AGREEMENT ("Agreement") is made and entered into on this
day of, 2020, by BMH Realty, L.L.C., a Michigan limited liability company, 775 N Second Street, Brighton, Michigan 48116 ("Owner"), and GENOA CHARTER TOWNSHIP, a Michigan
municipal corporation, 2911 Dorr Road, Brighton, Michigan 48116 (referred to as "Township").
WHEREAS, Owner owns real property located in the Charter Township of Genoa, County of Livingston, State of Michigan, and more particularly described on Exhibit A attached hereto ("Property") There is an existing gas station and two retail buildings located within the Property. The balance of the Property is vacant. Owner intends to develop the Property as a planned unit development.
WHEREAS, Owner has submitted to the Township a request for rezoning of the Property to Redevelopment Planned Unit Development ("RDPUD"), including all Conceptual Submittal items set forth in Section 10.05 of the Township Zoning Ordinance, including, but not limited to, proof of ownership of the Property; a completed application and application fee; an impact assessment meeting the requirements of Article 18 of the Township Zoning Ordinance, a copy of which is attached hereto as Exhibit B ("Impact Assessment"); this Agreement; drawings of at least 24" x 36", containing a cover sheet, a plan sheet of existing conditions, a parallel plan, and a conceptual PUD plan sheet, a copy of which drawings are attached to this Agreement as Exhibit C ("PUD Plan").
WHEREAS, the Charter Township of Genoa Planning Commission ("GPC") has reviewed the PUE Plan, conducted a public hearing on, and recommended approval of the Conceptual PUE Site Plan to the Charter Township of Genoa Board of Trustees ("Township Board") and Livingston County Planning Commission ("LCPC") on, 2020.
WHEREAS, on, the LCPC conducted a public hearing on the requested Conceptual PUD Site Plan, and recommended approval to the Township Board on
WHEREAS, Owner made revisions to incorporate conditions noted by the GPC and LCPC, if any and submitted required copies to the Township, providing sufficient time for review prior to the Township Board meeting.

WHEREAS, the Township	Board conducted a public hearing on the PUD rezoning, PUD Plan and
PUD Agreement on	, 2020, and provided conceptual approval of these documents
pursuant to Section 10.04.01 of the	he Township Zoning Ordinance on
in Section 10.06 of the Township 2 all materials required by Article	omitted to the Township all Final PUD Site Plan Submittal items set forth Zoning Ordinance, including, but not limited to, the PUD Plan containing 18 of the Township Zoning Ordinance; an Impact Statement; a Traffic npact assessment, a final copy of this Agreement (collectively the "Final
WHEREAS, the GPC has re the Township Board on	eviewed the Final Plan, and recommended approval of the Final Plan to, 2020.
•	revisions to incorporate conditions noted by the GPC, if any, and Zoning Administrator, providing sufficient time for review prior to the
	nip Board conducted a public hearing on the Final Plan on nd approved the documents contained in the Final Plan on

WHEREAS, the GPC and Township Board actions set forth above have been taken in strict compliance with the Township Zoning Ordinance and with Act 110 of the Public Acts of 2006, as amended, and have rezoned the Property as an RDPUD, finding that such classification properly achieved the purposes of Article 10 of the Township's Zoning Ordinance, as amended, including the encouragement of innovation in land use, compatibility with adjacent uses, the promotion of efficient provision of public services and utilities, the reduction of adverse traffic impacts, and the provision of adequate employment. Further, the GPC and Township Board find the RDPUD, the PUD Plan, and this Agreement are consistent with the adopted Master Plan.

WHEREAS, the Township Board has found and concluded that the uses and future development plans and conditions shown on the approved Final Plan and as set forth herein are reasonable and promote the public health, safety and welfare of the Township, and that they are consistent with the plans and objectives of the Township and consistent with surrounding uses of land for reasons including, but not limited to, the following:

- a. the Property has been previously developed for the purpose of a commercial, office, or industrial use, and redevelopment of the site will be an enhancement to the site and surrounding area;
- b. all buildings on the site are proposed to be removed or renovated;
- c. a use permitted within the underlying zoning district is proposed;
- d. the Township has determined that flexibility in dimensional standards is necessary to allow for innovative design in redeveloping a site with constraints and where a clear public benefit is being derived;
- e. to encourage flexibility and creativity consistent with the intent of the PUD, the Township is permitting specific departures from the requirements of the Zoning Ordinance as a part of the approval process;

- f. For all deviations, the Township has found that the deviation shall result in a higher quality of development than would be possible using conventional zoning standards;
- g. a parallel plan was provided showing how the site could be redeveloped without the use of the PUD;
- h. a table was provided on the site plan that specifically details all deviations from the zoning regulations, including ordinance provisions from which deviations are sought, the reasons the deviations are necessary and mechanisms to be utilized to mitigate any impacts; and
- i. The permitted deviations are consistent with the intent of the Township's PUD ordinance.

NOW, THEREFORE, OWNER AND TOWNSHIP, in consideration of the mutual promises contained in the Agreement, HEREBY AGREE AS FOLLOWS:

ARTICLE I. GENERAL TERMS OF AGREEMENT

- 1.1 The Township and Owner acknowledge and represent that the recitations set forth above are true, accurate and binding on the respective parties.
- 1.2 The Township acknowledges and represents that the zoning of the Property as RDPUD, regulated by the PUD Plan and this Agreement may be relied upon for future land use and development of the Property by Owner, its successors, assigns and transferees. This Agreement is for the benefit of the Property, and shall run with the Property, and shall bind and inure to the benefit of the successors, assigns and transferees of the parties to this Agreement.
- 1.3 The PUD Plan, attached as Exhibit C, has been approved by the Township in accordance with its authority granted by the Genoa Charter Township Zoning Ordinance, The Michigan Zoning Enabling Act, being MCL 125.3101, et seq, and the Michigan Planning Enabling Act, being MCL 125.3801, et seq, subject to the terms of this Agreement.
- 1.4 Deviations from the Township Zoning Ordinance shall be permitted as set forth in this Agreement or the PUD Plan, or as otherwise be agreed upon by the Township and the Owner. Changes to the PUD Plan and/or PUD Agreement shall be processed as set forth in the Zoning Ordinance and this Agreement.
- 1.5 All improvements constructed in accordance with this Agreement and the PUD Plan shall be deemed to be conforming under the Township Zoning Ordinance and in compliance with all other ordinances of the Township.
- 1.6 The approval of the PUD Plan shall be subject to the conditions set forth herein, inclusive of Exhibits.
- 1.7 All common areas and features, such as walkways, signs, lighting and landscaping, will be maintained by Owner, and financed on a pass-through basis to tenants through Owner's standard lease.

1.8 The construction, improvement and maintenance of all streets and necessary utilities (including public water, wastewater collection and treatment) to mitigate the impacts of the PUD project through construction shall be performed by the Owner, or sufficient performance bond given by the Owner to assure such construction.

ARTICLE II. LAND USE AUTHORIZATIONS

- 2.1 In addition to the uses set forth in the PUD Plan, uses listed in the GCD zoning classification of the Township Zoning Ordinance shall be allowed subject to the applicable permitted or special land use regulations, as well as those uses set forth on Exhibit D attached hereto.
- 2.2 The PUD Plan identifies the location and configuration of the currently-proposed structures that may be developed on the Property subject to the following:
 - A. Owner shall be permitted to adjust the size or shape of the various parcels and/or the size or configuration of the proposed buildings within the parcels, as set forth in, and in compliance with, Section 10.11 of the Township's Code of Ordinances.
 - B. The buildings identified on the PUD Plan are flexible use buildings that may be converted to retail, office or other uses in the future, provided that such uses are permitted under this Agreement.

The timing of development of the PUD shall be in compliance with the Charter Township of Genoa Code of Ordinances, but otherwise as determined by the Owner.

Owner shall have the right to develop the Property as a commercial condominium project by following the procedures required by Michigan law and any applicable Township ordinances. Any approval required from the Township for the purposes of creating a commercial condominium shall not be unreasonably withheld, conditioned or delayed. Owner shall additionally retain the right to apply to the Township for one or more lot splits that comply with Michigan law, Township Ordinances and the PUD Plan.

ARTICLE III. CURB CUTS AND OFF-SITE TRANSPORTATION IMPROVEMENTS

3.1 The number and general location of entrances to the site from adjacent public thoroughfares shall be as identified on the PUD Plan. The entrances were approved by the Livingston County Road Commission on ______.

ARTICLE IV. INTERNAL ROAD NETWORK

4.1 The internal system of private roads or drives shall be as identified on the PUD Plan.

ARTICLE V. DRAINAGE

5.1 Storm drainage from the Property shall be managed by means of an off-site retention basin to be developed by Owner on the adjacent property shown on the PUD Plan, with permission of the owner of that property. Owner has obtained the necessary easements to improve, utilize, maintain and repair the off-site retention basin (the "Basin"), and to

No reason for this language. We already have to comply with the MZEA and due process requirements.

access the Basin across White Horse Drive, which easements are attached hereto as Exhibit E.

ARTICLE VI. SITE IMPROVEMENTS

- There shall be a coordination of site improvements within the overall Property, with the objective of creating site improvements that are integrated and mutually supportive among the respective portions of the development, including the utilities, landscaping and lighting, as more specifically set forth in the PUD Plan.
- 6.2 Comprehensive and interconnected pedestrian walkways shall be constructed as shown on the PUD Plan contemporaneously with the buildings they are designed to serve.
- 6.3 Site elements, including site design and landscaping to diminish the prominence of parking lots as viewed from public roads and parking lot configuration, shall be constructed as shown on the PUD Plan.
- The Township shall grant to Owner and its contractors and subcontractors all Township permits and authorizations necessary to bring and/or construct all utilities necessary to service the Property and to otherwise develop and improve the Property in accordance with the PUD Plan, provided the Owner has complied with any and all legally-applicable requirements in applying for such permits and authorizations, including paying any required fees. Any applications for permits or authorizations from the Township shall be processed by the Township in the customary manner. The Township shall cooperate with Owner in connection with Owner's applications for any necessary county, state, federal or utility company approvals, permits or authorizations to the extent that such applications and/or discussions are consistent with the PUD Plan and this Agreement.
- 6.5 All trees and woodlands will be preserved as shown on the PUD Plan, or replaced on a caliper-for-caliper basis, as more fully set forth in the PUD Plan.

ARTICLE VII. DESIGN OF BUILDING AND SIGNS

- 7.1 The architecture, building materials, colors and shapes of all buildings shall be consistent with the building elevations attached to this Agreement as Exhibit F and otherwise conform to the Zoning Ordinance.
- 7.2 All signs shall be permitted as shown on the PUD Plan or as otherwise authorized in the Zoning Ordinance. Any permitted sign shall have a base constructed of materials that coordinate with and are consistent with the architecture of the building, unless mounted directly on the building.
- 7.4 The landscaping within the PUD shall demonstrate consistency in terms of design and materials. Parking lot lighting shall be consistent throughout the PUD. Street and walkway lighting shall be consistent throughout the PUD and comply with the Zoning Ordinance.

ARTICLE VIII. UTILITIES

- 8.1 All buildings that require plumbing must connect to the municipal water system and municipal sanitary sewer system. The internal water and sewer main plan shall be completed and approved with the site plan. The Township does not guarantee public utility availability without adequate planning and approval of the Township Engineer.
- Fees, charges and costs for utilities shall be as set forth in the Genoa Township Equivalent User Table as applicable and as may be amended from time to time.

ARTICLE IX. MISCELLANEOUS

9.1

I am not sure this is needed because of paragraphs 1.3, 1.4 and 1.5 and the representations made in paragraphs 1.1 and 1.2

- This Agreement may not be modified, replaced, amended or terminated without the prior written consent of the parties to this Agreement. The Owner and the Township shall be entitled to modify, replace or amend this Agreement without the consent of any other person or entity, regardless of whether such person or entity now or hereafter has any interest in any part of the Property, including subsequent purchasers, tenants, mortgagees, or others, unless required by Federal, State or local law or ordinance. Amendments and deviations, whether minor or major, shall be made in compliance with the procedures set forth in the Charter Township of Genoa Code of Ordinances at the time the amendment or deviation is sought. Nothing whatsoever provided in this Agreement shall be construed so as to prevent Owner from seeking major and/or minor changes to the PUD Plan in accordance with the applicable provisions of the Zoning Ordinance.
- 9.2 Reference in this Agreement to activities by the Owner in relation to development is intended to include Owner's transferees and assigns unless context dictates to the ontrary.
- 9.3 In the event of any direct conflict between the specific terms and provisions of this Agreement (including the attached PUD Plan) and the provisions of the Zoning Ordinance, or other Township ordinances, rules or regulations, the provisions of this Agreement shall control.
- 9.4 In the event a portion of the Property is submitted for site plan approval, and such approval is denied, the party submitting such site plan shall be entitled to appeal such decision to the Zoning Board of Appeals as provided by law.
- 9.5 The undersigned parties acknowledge that the conditions imposed upon the development of the Property are reasonable conditions necessary to ensure that public services and facilities affected by the proposed land use or activity will be capable of accommodating increased service and facility loads caused by the land use or activity, to protect the natural environment and conserve natural resources and energy, to ensure compatibility with adjacent uses of land, and to promote the use of land in a socially and economically desirable manner. Further, it is acknowledged that the conditions meet all of the requirements of Section 503 of Public Act 110 of 2006, MCL 125.3503.

THE PARTIES have executed this Planned Unit Development Agreement on the dates set below their names, to be effective on the date set on the first page of this agreement.

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GENOA CHARTER TOWNSHIP

a Michigan municipal corporation

Dve			
By: Its:			
STATE OF MICHIGAN))SS		
COUNTY OF)		
This Amendment was ackn of, 2020, by	owledged before me in _	County, Michigan, this	day
of Genoa Charter Township,	a Michigan municipal corp	, the poration, on behalf of the corporation.	
		, Notary Publ	ic
		County, Michigan	
		My Commission Expires:	
DAALL DEALTY L.L.C		Acting in the County of	
BMH REALTY, L.L.C. a Michigan limited liability c	omnany		
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of BMH Realty, L.L.C., a Mich	nigan limited liability comp	any, on behalf of the company.	
		Notary Dubl	io
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		My Commission Expires:	
		Acting in the County of	

DRAFTED BY AND WHEN RECORDED RETURN TO: Bradford L. Maynes, Esq. Corrigan Oil Company 775 North Second Street Brighton, Michigan 48116

Exhibit A

Property Description

Real estate situated in the Township of Genoa, County of Livingston, State of Michigan, described as:

Part of the Northeast quarter of Section 9, Town 2 North, Range 5 East, Michigan, described as follows: Beginning at a point South 2°24' West 636.58 feet from the North quarter corner of said Section 9; thence South 2°24'West 172.53 feet; thence South 60°0' East 141.66 feet along the Northerly right of way line of Grand River; thence North 30°0' East 200.00 feet; thence North 72°0' West 226.54 feet to the point of beginning.

Commonly known as: 4483 E. Grand River, Howell, MI PARCEL ID NO. 4711-09-200-005

A part of the Northeast quarter of Section 9, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan, described as follows: Commencing at the North quarter corner of said Section 9; thence South 02°24'00" West along the West line of Charles Garlock Survey No. 142-33, dated February 2, 1977, 809.11 feet to the Northerly right-of-way of Grand River Avenue; thence South 60°00'00" East along the Northerly right-of-way of Grand River Avenue, 141.66 feet to the point of beginning of the parcel to be described; thence North 30°00'00" East 200.00 feet; thence South 60°00'00" East 274.00 feet; thence South 25°32'20" West 217.67 feet to the Northerly right-of-way of Grand River Avenue; thence North 60°00'00" West along the Northerly right-of-way 289.86 feet to the point of beginning.

Commonly known as: 4525 E. Grand River, Howell, MI PARCEL ID NO. 4711-09-200-010 and 4711-09-200-015

A part of the Northeast quarter of Section 9, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan, described as follows: Commencing at the North quarter corner of said Section 9; thence South 02°24'00" West along the West line of Charles Garlock Survey No. 142-33, dated February 2, 1977, 809.11 feet to the Northerly right-of-way of Grand River Avenue; thence South 60°00'00" East along said Northerly right-of-way of Grand River Avenue, 431.52 feet to the point of beginning of the parcel to be described; thence North 25°32'20" East 217.67 feet; thence South 64°27'40" East 106.63 feet; thence South 05°09'00" West 23.38 feet; thence South 16°19'00" West 210 feet to the Northerly right-of-way of Grand River Avenue; thence North 60°00'00" West along said Northerly right-of-way 148.90 feet to the point of beginning.

Commonly known as: 4533 E. Grand River, Howell, MI PARCEL ID NO. 4711-09-200-014

A part of the Northeast quarter of Section 9, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan, described as follows: Commencing at the North quarter corner of said Section 9; thence South 02°24′00″ West along the West line of Charles Garlock Survey No. 142-33, dated February 2, 1977,

459.11 feet to the point of beginning of the parcel to be described; thence South 67°30′00″ East 612.00 feet; thence South 05°09′00″ West 205.00 feet; thence North 64°27′ 40″ West 106.63 feet; thence North 60°00′00″ West 274.00 feet; thence North 72°00′00″ West 226.54 feet; thence North 02°24′00″ East t77.47 feet to the point of beginning.

Commonly known as: 1098 Lawson Road, Howell, MI PARCEL ID NO. 4711-09-200-016

A part of the Northeast quarter of Section 9, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan, described as follows: Commencing at the North quarter corner of said Section 9; thence South 02°24'00" West 459.11 feet; thence South 67°33'00" East 501.50 feet to the point of beginning; thence South 03°14'18" West 177.38 feet; thence South 64°26'59" East 121.69 feet; thence- South 16°19'00" West 210.00 feet; thence North 19°06'45" East 232.36 feet; thence North 13°44'25" East 192.25 feet; thence North 8 19°06'45" West 44.87 fee±; thence North 67°33'00" West 110.50 feet to .the point of beginning.

Commonly known as: 4525 E. Grand River, Howell, MI PARCEL ID NO. 4711-09-200-017

Exhibit B

Impact Assessment

Exhibit C:

PUD Plan

Exhibit D

Uses

Uses Permitted by Right

- Retail establishments and shopping centers which provide goods such as bakery goods, including bakery items produced on the premises, groceries, produce, meats, provided no slaughtering shall take place on the premises, seafood; dairy products, beverages appliances, electronics, furniture, and home furnishings, apparel, jewelry, art, pharmaceuticals, home improvement supplies, hardware, and garden supplies, sporting goods, bicycles, toys, hobby crafts, videos (rental, and sales), music, musical instruments, books, computer hardware and software, antiques, flower shops, greeting card shops, auto parts and similar establishments not specifically addressed elsewhere (up to 30,000 square feet of gross floor area);
- banquet halls, assembly halls, dance halls, private clubs, fraternal order halls, lodge halls or other similar places of assembly;
- business services such as mailing, copying, data processing and retail office supplies;
- child care centers, preschool and commercial day care;
- funeral home or mortuary;
- bed and breakfast inns, hotels and motels with no more than 25 rooms not including accessory convention/meeting facilities or restaurants, which may include the residence for the owner/manger's family;
- hotels and motels with more than 25 rooms including accessory convention/meeting facilities and restaurants;
- laundromats;
- personal and business service establishments, performing services on the premises, including: dry cleaning drop-off stations (without on site processing), photographic studios, copy centers, mailing centers, data processing centers, dressmakers and tailors, shoe repair shops, tanning salons, beauty parlors, barber shops, and similar establishments;
- restaurants, taverns, bars, delicatessen, food carryout, coffee shops, and similar establishments serving food or beverages, including, but not limited to:
 - standard restaurants and coffee shops;
 - o restaurants and bars serving alcoholic beverages;
 - bars providing dancing and live music;
 - restaurants with outdoor seating;
 - carry-out restaurants;
 - o brewpub;
- studios of photographers and artists;
- tattoo parlors;
- tool and equipment rental, excluding vehicles;
- climate-controlled indoor commercial storage;
- auto/gasoline service station;
- banks, credit unions, savings and loan establishments and similar;
 - o with up to 3 drive-through teller windows
- offices of non-profit professional, civic, social, political and religious organizations;
- medical urgent care facilities, medical centers and clinics;

- medical offices of doctors, dentists, optometrists, chiropractors, psychiatrists, psychologists and similar or allied professions, excluding clinics, and urgent care centers;
- offices of lawyers, engineers, architects, insurance, and real estate agents, financial consultants, and brokers, advertising offices, accounting, and bookkeeping services, clerical, and stenographic services, sales offices, other types of executive, or administrative offices and similar or allied professions;
- motion picture theaters;
- public parks and open space;
- health clubs, fitness centers, gyms and aerobic clubs; commercial schools and studios for teaching photography, art, music, theater, dance, martial arts, ballet, etc.;
- vocational and technical training facilities;
- churches, temples and similar places of worship and related facilities;
- essential public services and structures, not including buildings and storage yards;
- essential public buildings;
- public/government buildings such as: township/state/county offices, public museums, libraries and community centers;
- temporary outdoor sales and Temporary outdoor events;
- accessory uses, buildings and structures customarily incidental to any of the above;

Uses Permitted with Special Land Use Approval

- Retail establishments and shopping centers which provide goods such as bakery goods, including bakery items produced on the premises, groceries, produce, meats, provided no slaughtering shall take place on the premises, seafood; dairy products, beverages appliances, electronics, furniture, and home furnishings, apparel, jewelry, art, pharmaceuticals, home improvement supplies, hardware, and garden supplies, sporting goods, bicycles, toys, hobby crafts, videos (rental, and sales), music, musical instruments, books, computer hardware and software, antiques, flower shops, greeting card shops, auto parts and similar establishments not specifically addressed elsewhere (between 30,000 and 60,000 square feet of gross floor area), including, but not limited to pharmacies with a drive-up window;
- automobile, motorcycle, boat and recreational vehicle sales, new and used;
- outdoor commercial display, sales or storage;
- conference centers;
- dry cleaning drop-off stations with drive-through service;
- restaurants, taverns, bars, delicatessen, food carryout, coffee shops, and similar establishments serving food or beverages, limited to the following:
 - o restaurants with open front windows
 - o drive-in restaurants
 - o coffee shop with drive-through
 - o micro-brewery, small distillery and small winery
- kennel, commercial;
- pet day care center;
- minor auto repair establishment;
- automobile wash, automatic or self serve;
- leasing and rental of automobiles, trucks and trailers;

- adult day care facilities;
- banks, credit unions, savings and loan establishments and similar, limited to the following:
 - o with more than 3 drive-through teller windows;
 - o stand alone automatic drive-up teller machines.
- veterinary hospitals;
- veterinary clinics without boarding or overnight care;
- carnivals, fairs, commercial cider mills and amusement parks;
- recreation (outdoor) commercial or private, recreation centers, including children's amusement parks, batting cages, and go-cart tracks;
- miniature golf courses and driving ranges;
- recreation (indoor) such as bowling alleys, skating rinks, arcades, indoor golf or softball, indoor shooting/archery ranges;
- animal shelters;
- bus passenger stations;
- shelters and rehabilitation centers for philanthropic or non-profit institutions;
- upper floor dwelling units;
- accessory drive-through service not listed above;
- accessory fuel storage and use or storage of hazardous materials.

Exhibit E

Easements

Exhibit F

Elevations

BMH REALTY, L.L.C.

775 N. Second Street Brighton, MI 48116 (810) 229-6323

February 19, 2020

VIA HAND DELIVERY

Genoa Charter Township Planning Commission Charter Township of Genoa 2911 Dorr Road Brighton, Michigan 48116

Re: Basis for Deviations – Grand River and Lawson PUD

Dear Commissioners,

BMH Realty, L.L.C. ("Owner") seeks to redevelop an underutilized area of the Township bounded on three sides by Grand River Avenue, Lawson Drive and White Horse Drive through a Redevelopment PUD ("PUD"). The site is unique in that it acts as the buffer between an industrial area to the north and the primary commercial corridor to the south — Grand River Avenue. On the south end of the site, Owner has an existing gas station ("Gas Station"), which is proposed to be flanked by an office/retail building to the west ("Office/Retail"), and a drive-through restaurant to the east ("Drive-Through"), at the corner of Grand River Avenue and Lawson Drive. These three front uses will screen a primarily metal-finished climate-controlled storage use at the rear of the site ("Self-Storage"), bordering on White Horse Drive and the existing industrial uses. As a result, the site needs to be developed in a way that is sensitive to the uses on all sides, as well as the existing uses on the site itself.

The goal of the entire site is to provide uses that complement each other and service the overall community. The large site accommodates uses that generate traffic but allows the traffic to enter and exit the site from three public roads. The proposed PUD layout of the site and uses provide the necessary parking and loading zones while reducing the number of buildings and parking that could be built according to the parallel plan. The proposed PUD layout of the site provides a cluster of buildings versus a strip center. The location of the retention pond and landscaping will provide a buffer between the proposed commercial development and the adjacent property owners. In trying to reconcile the nearby proliferation of differing uses and aesthetics with the desire for a coherent site, it is necessary that Owner

seek a number of dimensional and use deviations from the underlying GCD¹ zoning as a part of the approval of the PUD. Please find below the basis for those deviations.

I. Township Standard

Deviations within proposed planned unit developments in the Charter Township of Genoa (the "Township") are governed by Section 10.07.01(h) of the Township's Zoning Ordinance, which sets forth as a required finding for approval of a Conceptual PUD Site Plan that "Any deviations from the applicable zoning regulations are reasonable and meet the intent of this Article". In addition, Section 10.03.04 requires a finding that "the deviation shall result in a higher quality of development than would be possible using conventional zoning standards". Finally, 10.03.04(c) requires that the table of deviations contain, in part, "the reasons the deviations are necessary and mechanisms to be utilized to mitigate any impacts". The proposed deviations in the PUD development meet all of these standards.

With regard to the 10.07.01(h) standard, the intent of the PUD Article is set forth in Section 10.01.01, which states the Purpose as follows:

to permit the coordinated development on larger sites, protect significant natural features present which the property owner and Township wish to preserve, to provide the opportunity to mix compatible uses or residential types, or allow clustering of residential units to preserve common open space and natural features.

The purpose, then, sets forth four factors for inquiry:

- (a) whether the deviations are a part of a coordinated development on a larger site;
- (b) whether the deviations help to protect significant natural features that the owner and Township wish to preserve;
- (c) whether the deviations provide the opportunity to mix compatible uses or residential types; or
- (d) whether the deviations allow clustering of residential units to preserve common open spaces and natural features.

It is the position of the Owner that factors (b) and (d) are inapplicable in the present instance as there are no significant natural features within the PUD, and residential units are not proposed

¹ As the rezoning of the underlying property to GCD is necessary for the proposed PUD, this letter is assuming a rezoning to GCD of the current portion of the property zoned OSD.

within the PUD. The question, then, is whether the proposed deviations meet factors (a) and (c). All of these standards are met by the various deviations requested, as set forth below.

II. The Deviations on the Self-Storage Building Meet the Ordinance Standard.

A. Finish Deviations

The finish deviations on the Self-Storage are fundamentally reasonable due to the nature of the use. This building is metal-finished for the pragmatic reason that lesser-quality storage facilities are block finished on the inside – the finish on the outside is the same as that on the inside. High quality climate-controlled storage requires metal-shelled storage units to prevent water and moisture intrusion, a cost that most other non-storage uses are not required to incur. Allowing a necessary internal material to also act as the functional exterior finish that it is allows form to follow function in a reasonable way for a building with unusual requirements. This is further reasonable as the metal finish, with commercial accents, allows it to sit comfortably between the industrial uses to the north and the commercial uses to the south, without looking like a sharp break from either perspective. Finally, the need for overhead doors at either end of the building to allow drive-in access is an industrial-appearing requirement that does not comport with a 75% brick, commercial design. The building finishes, as designed, are the most reasonable way to accommodate all of these needs.

With regard to providing coordinated development, the use of metal exterior finishes on the Self-Storage bordering on White Horse Drive provides a transition from the industrial uses to the north to the commercial uses to the south. Additionally, the coordination of the Self-Storage with the front buildings allows those buildings to provide more traditional commercial façades for the view from Grand River Avenue.

The highest quality self-storage facilities provide both security from water, moisture and mold and drive-through access to permit ease of access without weather intrusion. Allowing exterior metal finishes allows for a cost-effective means to construct the highest-quality storage on the interior by allowing the interior finish to serve as the exterior finish as well. Additionally, the appearance of the exterior metal finish allows for the overhead doors to appear more appropriate aesthetically, while retaining the highest functionality.

Finally, the deviations in terms of materials are mitigated by three different factors. First, the finished development will contain three different buildings between the Self-Storage Building and Grand River Avenue – the Gas Station, the Drive-Through and the Office/Retail. The presence of those three buildings are what will establish the retail/commercial appearance of the project from Grand River Avenue, rather than the Self-Storage that they screen. Second, the mix of industrial and commercial finishes on the Self-Storage are mitigated by the true

industrial finishes to the north of the property. This is not the case of a non-traditional commercial building sitting among nothing but commercial buildings – the blend of commercial and industrial finishes allows for a transition between the two, as discussed above. Finally, the presence of additional landscaping trees on White Horse Drive will help to mitigate any visual impact to passing motorists.

B. Window Deviations

With regard to the window deviations, a figure of 25% window covering for the façade of the Self-Storage is inherently unreasonable for a storage use to be economically viable, particularly given the other economic challenges on the site, including, but not limited to the retention pond. Owner has inserted as much window space as is possible, but, ultimately, the walls of the building are the walls of the storage units. It is neither reasonable to a) install windows that compromise the security of clients' storage units nor b) significantly increase the cost of the building by creating an additional wall with false windows in it directly outside of the walls of clients' storage units. The only reasonable alternative is to use windows where possible, as Owner has done around the Self-Storage, and mitigate the appearance through other means.

The Self-Storage has been coordinated with the balance of the site through screening by trees to the north, and by the other three buildings to the south. The trees break up the appearance of the Self-Storage when viewed from the north, and the significantly higher volume of windows present on the Office/Retail, Gas Station and Drive-Through uses alleviate the same issues from the Grand River Avenue side.

As discussed above, the same arguments as to having an identical interior and exterior wall for higher-quality development purposes are applicable here. Constructing a metal storage facility, the highest quality build available, is not feasible with 25% window coverage.

With regard to mitigating the deviation, the arguments are the same as set forth above with regard to coordination of the site. The entire site design has been coordinated so as to mitigate the potential harms of the requested deviation.

III. The Deviations on the Office/Retail Building Meet the Ordinance Standard.

The Office/Retail Building requires only one deviation – the materials usage for the north elevation of the building, which faces the Self-Storage Building. Allowing a deviation in materials presentation on the north elevation is reasonable as the north elevation will be almost entirely screened from outside the property by the Self-Storage Building. With regard to compliance with the intent of the PUD Ordinance, the placement of the retail building was coordinated with the other buildings and uses during site design. The retail building will have

high visibility along Grand River while providing a screen for the self-storage building. The uses that will occupy the retail building will not conflict with the other uses proposed within the site. As to the third factor, the savings that can be obtained on materials on this elevation afford the remainder of the public-facing elements of the development the ability to be of a higher quality. This deviation is mitigated entirely by the lack of visibility from outside of the property.

IV. The Deviations on the Fast Food Drive-Through Building Meet the Ordinance Standard.

The deviations on the Drive-Through are twofold – the deviation on the east setback of the site, and the use itself.

A. Reasonability

The PUD plan proposes a 59-foot setback along Lawson Drive. This setback deviation is reasonable due to the angle of Lawson Drive. Making effective use of the site and squaring-up the Drive-Through as much as possible to the primary view of Grand River Avenue results in a building placement on this corner where the northernmost corner of the building will have a 59-foot setback. Moving south, the building comes closer to conformance. This setback additionally is in greater conformance with the ordinance than the existing building.

With regard to the Drive-Through, while there has not historically been a stand-alone drive-through restaurant located on the PUD property, the existing gas station was constructed with a drive-through restaurant within the convenience store. That existing drive-through facility does not meet the current Township ordinances applicable to drive-through service uses. It is reasonable to increase the conformance of the site overall by separating out the drive-through use and removing the existing drive-through window. The PUD plan proposes the construction of the Drive-Through restaurant that, apart from the use itself, will be in greater conformance with the Township's ordinances than the existing corner use.

B. Meets Intent of PUD Ordinance

The deviation to the east setback is necessary for a coordinated development around the existing Gas Station. The parcel between the Gas Station and the corner is large enough to look odd if left vacant, but small enough that it is difficult to utilize for meaningful development without intrusion into the setback, as demonstrated by the currently-existing construction.

In addition, gas stations and drive-through restaurants are generally complimentary uses. The Shell station at the I-96 and Grand River interchange shares space with Baskin-Robbins and Dunkin' Donuts, while the BP station at Latson Road has a drive-through Panera located directly across Grand River Avenue from it. These are only two of several examples along the Grand

River corridor. Further, with the location of the Gas Station and Lawson Drive, there are few commercial uses that will fit in the space allocated.

C. Higher Quality of Development

The highest quality development is one that presents a package where the site is occupied at a uniform density, and where the internal uses are complimentary to one another. As set forth above, the setback deviation allows the use of what would otherwise result in the most valuable portion of the development being empty, and further allows a use that is complimentary to all three of the other uses in the development.

D. Mechanisms to Mediate

The primary mechanism to mediate the setback deviation is the orientation of the Drive-Through on the site. As discussed above, this is not a setback deviation where the building is consistently situated 59 feet from the parcel boundary, this is a circumstance where the deviation is consistently lessened as one proceeds south towards the point of greatest visibility – Grand River Avenue.

As for the use itself, the only meaningful potential impact could be additional traffic. By funneling traffic to three individual accesses on three separate roads, whatever minimal traffic may be generated by the Fast Food Drive-Through Building over and above the two permitted uses and the gas station is minimized by distributing that traffic over three roads rather than all flowing out onto one road, as a stand-alone fast-food drive through use likely would.

V. The Deviations on the Gas Station Meet the Ordinance Standard.

The Gas Station is a unique building from the perspective of the PUD as it is a lawfully preexisting use. As a part of the redevelopment, Owner is seeking two approved deviations – the gas station use itself, and the use of improved quality materials that still do not comply with the ordinance requirements.

The reasonability of the gas station use has been long established from its pattern of usage within the community. Owner first constructed the gas station on the property in approximately 1994. Since that time, Owner has received no complaints about the operation of a gas station on the property, and the gas station has been a profitable business providing a valuable service to the citizens of the Charter Township of Genoa. The station is the only gas station in the 5.5 mile stretch of Grand River Avenue between Latson Road to the west, and the I-96 interchange in Brighton. With regard to the façade improvements, the nature of the current exterior finishes are such that any improvement would be very difficult to retrofit, and

make renovations less reasonable. As a result, replacing the existing EIFS with Nichiha vintagewood and the addition of Nichiha sandstone columns to break up the elevations make reasonable progress towards bringing the building into closer conformance with the ordinance while providing higher quality elevations.

With regard to the compliance with the intent of the PUD ordinance, the goal of the entire site is to provide uses that complement each other and service the overall community. The large site accommodates uses that generate traffic but allows the traffic to enter and exit the site from three public roads. The proposed PUD layout of the site and uses provide the necessary parking and loading zones while reducing the number of buildings and parking that could be built according to the parallel plan. The proposed PUD layout of the site provides a cluster of buildings versus a strip center. The location of the retention pond and landscaping will provide a buffer between the proposed commercial development and the adjacent property owners. Finally, the gas station use is extremely common in concert with fast food drive-throughs, as discussed above, and works well with a potential commuter. As a result, the use is in compliance with the intent of the PUD ordinance.

As discussed above, the difficulty of retrofitting the existing building makes creating a higher-quality development challenging while still complying with the Township's finish requirements. Allowing the proposed retrofits is the best way to create a higher quality of development from the perspective of the gas station building. Additionally, by allowing the gas station as a permitted use in the PUD, it allows for a higher quality of development in the future, as the status of a permitted use makes financing for improvements substantially easier to obtain.

The changes to the Gas Station are, themselves, mitigating the current impact of the Gas Station Building itself, by bringing it further into conformance. With regard to the use itself, the internal routing of traffic, eliminating additional access to Lawson Drive and adding an access to White Horse Drive, helps to mitigate any traffic impact of the gas station.

VI. The Landscape Deviations Meet the Ordinance Standard.

The proposed landscaping plan shows a gap in landscaping at the corner of Grand River and Lawson Drive. This area is the future site of the Fast Food Drive-Through. As the nature of the Fast Food Drive-Through is not currently known, it is difficult to know how to orient the landscaping to best work with that particular franchise. As a result, it is reasonable to allow a deviation presently, and require compliance when the drive-through restaurant submits for site plan approval at a later date. It is the Owner's intent to meet the ordinance requirements with a future submittal, however, we do not want to plant landscaping that could be damaged or need alterations when the site is eventually developed. Allowing the landscaping to wait for the necessities of the actual development best effectuates the goals of compliance with the

intent of the PUD ordinance (coordinated development) and a higher quality of development. At this time as part of the site plan, the applicant is proposing 8 additional trees along White Horse Drive to mitigate the 8 trees that are not shown at the corner of Grand River and Lawson Drive, as well as mitigation of the Self-Storage Building, discussed above.

BMH Realty, L.L.C. has made a concerted effort to redevelop the area around its existing gas station into a high-quality, functioning development that will improve the quality of life in the Township. Due to surrounding uses, the peculiarities of the site, and the nature of the users desiring the site, it is necessary to request some deviations as a part of the Redevelopment PUD. For the reasons set forth above, BMH Realty, L.L.C. believes those deviations meet the Township's standards, and requests approval of the PUD, including the deviations.

Very truly yours,

Michael B. Corrigan

Muhand longo

BMH – PREMIER GENOA PUD Genoa Township, Michigan Site Plan Application

IMPACT ASSESSMENT

Owner:

BMH Realty, L.L.C. 775 N. Second Street Brighton, Michigan 48116

Prepared by:

DESINE INC. 2183 Pless Drive Brighton, Michigan 48114

A. INTRODUCTION

This impact assessment has been prepared pursuant to Article 18 – <u>SITE PLAN REVIEW</u> of the Zoning Ordinance for the Township of Genoa, Livingston County, Michigan. This assessment addresses the impact of the proposed building additions and parking expansion on the surrounding community and the economic condition and social environment of the Township.

This Impact Assessment has been prepared under the direction of Wayne Perry, P.E., DESINE INC., 2183 Pless Drive, Brighton, Michigan 48114. Mr. Perry is a licensed Civil Engineer, providing professional engineering services in Livingston County since 1988 with experience in private and municipal development including projects within Genoa Township and Livingston County.

B. SITE LOCATION / DESCRIPTION

The site is comprised of six parcels, containing a total of 5.12 acres of property, bordered on the North by White Horse Drive, Lawson Drive on the East, Grand River Avenue on the South and the Lake Trust Credit Union to the West, as shown on Figure 1. The Southeasterly four parcels are zoned General Commercial (GCD) district, the Southwesterly Parcel is zoned Office Service (OSD) district, and the North Parcel is split zoned with the East 1/2 zoned GCD and the West 1/2 zoned OSD.

The property currently contains three buildings and related improvements. An existing fuel station with a 2,670 square foot convenience store building, parking lot and storm water basin is located on the two center parcels. A 7,900 square foot commercial building, occupied by the Total Pool Supplies and Service, is located on the Southeast parcel. The Northeast parcel contains a 2,440 square foot industrial building, with a fenced outdoor storage yard, currently home to Security Access Controls and Fence Company. A residential home and related improvements, located on the Westerly parcel, were previously removed. The Existing Conditions Plan provides a detailed overview of the existing site features.

The BMH-Premier PUD development plan depicts proposed site improvements to be constructed on the site. Improvements consist of A 37,275 sq ft multi-purpose, adaptive use building, currently proposed to be up-fit with equipment to operate a drive-through, climate controlled building, with the potential for small flex-office spaces for rent, a 7,000 sq ft building appropriate for use for office, retail, restaurant, or a mix of those uses, a 2,336 sq ft out parcel pad suitable for use for retail or drive through fast food or other commercial use, and the existing gas station with a convenience store and related (existing) ancillary propane and kerosene sales. Additionally, the PUD plan features new parking areas, access drives, a storm water management system, lighting, landscaping and related site improvements.

Access to the property from Grand River Avenue and from Lawson Drive is provided through existing drives to the fuel station and convenience store. New access drives are proposed on White Horse Drive, in the Northeast portion of the project, and connection to the existing cross-access drive to Lake Trust Credit Union, located at the Southwest corner

of the site. The development will provide 102 parking spaces, including 6 barrier free accessible spaces, are to be developed on the site. Appurtenant features including commercial access drives, drive aisles, unloading/loading area, sidewalks and landscaping are also presented in the site plan. An aerial photograph depicting the proposed site improvements is provided in Figure 2.

Adjacent uses include the Lake Trust Credit Union to the West and the DTE Service Center to the Northeast. Property to the North and East of the site is currently vacant. An aerial photograph depicting the proposed site improvements is provided in Figure 2.

C. IMPACT ON NATURAL FEATURES

Natural features on the subject parcel consist of re-established field grasses and shrub/scrub brush on the Westerly portion of the property. Existing topography of the site is generally flat to gently sloping terrain. The elevation of the property varies from an elevation of 991 at the Westerly property line, to approximately 986 at the Southeast corner of the parcel. Surface water drainage on the property generally flows to the Southeast.

Existing soils on the property are primarily Miami loam. These soils are generally moderately drained, moderately permeable, loams. Soil classifications are prepared by the United States Department of Agriculture, Soil Conservation Service, and "Soil Survey of Livingston County". The Soils Map, shown in Figure 3, shows the locations of specific soil types as classified.

The proposed construction and improvements will require filling and grading in the Southeast portion of the property, filling the existing storm water basin and modifications to site elevations on the Northwest corner of Lawson Drive and Grand River. Excavation will be required to construct the proposed storm water retention system proposed on the North side of White Horse Drive.

The limits of disturbance have been depicted on the grading plan. Grading for this project will maintain the general character of the existing site. Development of this project will require earthwork to construct to proposed retention basin and modify site grades with useable materials from the site, requiring the exporting of excess soil and importing of additional structural fill material. The proposed elevations and grading of the site mesh with the existing grades at the property lines.

Landscaping is proposed for the developed portion of the site to reduce the visual impact of the proposed project. All proposed landscaping areas meet or exceed the planting requirements of Genoa Township's current Zoning Ordinance and have been designed to improve the aesthetics of the property. Within the developed portion of the site, areas not otherwise covered, shall have lawn or other vegetative surface cover established.

Surface drainage characteristics on the property will be affected by the construction of the proposed building expansions and parking lots. Construction of the proposed improvements will reduce the permeable area of the property resulting in an increase in the surface water runoff generated. A storm water management system has been designed to

collect and control the surface water runoff, reducing the discharge rate from the property and increasing the infiltration of surface water runoff.

The proposed changes and modifications to the surface drainage conditions will not significantly impact local aquifer characteristics or groundwater recharge capacity. All surface water runoff from the site will be directed into the proposed retention basin to be constructed North of White Horse Drive. Reduction in the surface permeability will affect onsite infiltration, surface water flow path and duration. Surface water runoff from the development will be reduced and no significant impact to adjacent properties are anticipated from the proposed construction and re-development of the site.

Upland wildlife habitats on the property are minimal and consist of primarily field grass and shrub/scrub brush areas in the Northwest portion of the property. Wildlife supported in this area is generally smaller field animals and birds. Previous development and the existing use of the property, the adjoining developments and the proximity of Grand River, limits the existing upland habitat.

The project site does not currently support any significant wildlife habitat and the proposed construction will not have a significant impact on overall habitat quality. No significant adverse impact to natural features is anticipated due to the proposed re-development of this property.

D. IMPACT ON STORM WATER MANAGEMENT

Excavation and grading will be undertaken to construct the proposed stormwater retention basin. Earthwork will be required to direct storm water flow into the storm water collection system. This system will discharge surface water runoff generated by development of the property to the proposed sedimentation basin and retention basin. Site grading will mesh with existing grades on adjoining properties. No adverse impact to adjoining properties is anticipated due to the construction and grading of the property.

Surface water runoff generated from all improved areas of the site will be collected by catch basins, conveyed through a storm sewer system, and discharged to the sedimentation and retention basin located on the North side of White Horse Drive. Storm water will be retained within the basin during a storm event and will infiltrate into the ground.

Soil erosion and sedimentation are controlled by the Soil Erosion Control Act No. 347 of the Public Acts of 1972, as amended and is administered by the Livingston County Drain Commissioner. Silt fencing will be installed around a majority of the site during construction. The Contractor shall comply with all regulations including control during and after construction.

Impact on adjoining properties due to the construction of this site will be minimized by implementing soil erosion control methods. No adverse impact to adjacent properties due to surface water runoff will be created as a result of the proposed improvements.

E. IMPACT ON SURROUNDING LAND USES

Surrounding land uses to the North of the site, on the North side of White Horse Drive, zoned Mixed-Use Planned Unit Development (MU-PUD), consists of an industrial user and vacant property. Land to the East is zoned Non-Residential Planned Unit Development (NR-PUD) and is vacant. Land to the South, zoned Non-Residential Planned Unit Development (NR-PUD) and Industrial (IND), contains a mix of commercial and industrial users including an auto wash and Reuland Electric Company. Property to the West is the Lake Trust Credit Union, zoned Mixed-Use Planned Unit Development (MU-PUD).

The Genoa Township Future Land Use Plan designates this property for General Commercial uses. The surrounding property is designated for a mix of General Commercial, Neighborhood Commercial and Medium Density Residential uses.

The proposed uses depicted on the development plan are consistent with existing development in the area and are generally consistent with the long-term planning within the Township.

The landscaping and architecture proposed will allow this site to be developed to compliment the surrounding properties. The impact of the improvements to the surrounding area has been minimized.

Ambient noise levels on and around the property are largely generated by Grand River vehicle traffic. Daily activities within the proposed building are not anticipated to create an increase in the sound level in the area.

All site lighting shall meet the requirements of the Genoa Township Zoning Ordinance. Proposed building mounted fixtures and pole mounted site lighting will be shielded and down directed on the site. The existing fuel station and convenience store lighting shall be on from dusk until dawn. General site lighting for other uses, excluding safety and emergency lighting, shall be energized between the times from dusk to 12:01 a.m. and from 5:00 a.m. to dawn.

The building façade along the building front (South side) and the East side elevations are most visible from Grand River. The East and West ends of the storage building provide primary entrance features. The storage building is designed to grant vehicular access into and out of the respective East and West ends of the building. The entire length of the building is designed with a 30' wide drive and parking aisle through the building, reducing demand for exterior onsite parking needs and eliminating the need for any outdoor parking and loading areas to access the building for pick up and delivery of stored items.

HVAC units serving the multi-purpose facility are placed inside of the building eliminating unsightly large heating and cooling mechanical equipment typically located either on rooftops or outside within landscaped buffers, which if placed on the available area outside of the building these mechanicals would be seen along White Horse Drive or Lawson Drive. The proposed interior placement reduces visibility by drivers accessing nearby residential neighborhoods and indoor placement of the HVAC lessens the total height and

mass of the structure.

The proposed use of the property does not create any significant emissions of smoke, airborne solids, odors, gases, vibrations, noise or glare discernable and substantially annoying or injurious to person and/or property beyond the lot lines. No significant change in air pollution is anticipated.

The Contractor shall be responsible for initiating and maintaining adequate dust control measures during and after construction until the project site is fully stabilized and a vegetative cover established. Dust control measures used during construction may consist of site watering, mulching of completed areas, installation of windbreak fencing, and application of chemical dust control materials. The site will comply with the performance standards contained in Section 13.05 of the Township Zoning Ordinance.

F. IMPACT ON PUBLIC FACILITIES AND SERVICES

The Livingston County Sheriff and Michigan State Police will provide Police protection. Public safety services required to accommodate the proposed use are anticipated to be minor.

The Brighton Area Fire Department as a part of an existing governmental agreement will provide fire protection service. A fire hydrant will be constructed on the property within the White Horse Drive Right-of-way to provide adequate fire protection capabilities. The building is provided with an internal fire suppression system. A Fire Department Connection is located near the Northwest corner of the building along with a Knox box and required address labeling to meet the Fire Departments requirements. No significant increase in fire protection services are anticipated as a result of the proposed use.

The property is accessed from Grand River, White Horse Drive and Lawson Drive, providing adequate access for emergency vehicles. A wide drive aisle on the West side of the building will serve as a fire access lane between Grand River and White Horse Drive.

The proposed uses will not create any direct adverse impact on the public schools.

G. IMPACT ON PUBLIC UTILITIES

The property is presently within municipal sewer and water districts and existing buildings are connected to the municipal utilities.

Water service to the storage building is provided from a new water main extension to be constructed along White Horse Drive. A water service lead is connected to this main to supply the building and a fire suppression line is also provided from this main. An easement for repair, maintenance and access are provided for this connecting water main. Capacity is available within the existing water system to provide adequate service to this site.

Sanitary sewer is located along the North side of Grand River. Capacity is available within the existing sanitary sewer system to provide adequate service for the site.

The site is currently serviced by electric, gas, phone and cable systems located along Grand River Avenue.

All solid wastes will be properly disposed of through a licensed disposal firm on a regular basis. Dumpster enclosures with screen walls are located on the East, South and West sides of the storage building. The enclosures will be constructed decorative CMU material, matching material used on the site and complimenting the adjacent architecture of the building. Additional landscaping is provided in these areas to further enhance the aesthetics of these enclosures.

Delivery services are generally limited to parcel trucks, such as UPS, and similar single axle vehicles. These delivery service providers use available parking and drop off areas during their deliveries. Large vehicles accessing the site will be capable of maneuvering on the proposed access drives around the building and will utilize the proposed storage building for loading and unloading purposes.

H. STORAGE AND HANDLING OF ANY HAZARDOUS MATERIALS

The proposed uses within the building will not use, store, generate and/or discharge potentially polluting materials. Small quantities of material such as cleaning products and chemicals may be stored on-site.

I. TRAFFIC IMPACT STUDY

A traffic impact study for the development has been prepared by Rowe Engineering and is submitted with the Site Plan packet. ¹

Pedestrian access to the property is provided by an existing sidewalk along Grand River. A new sidewalk will be constructed along the East side of the property on Lawson Drive. Sidewalks throughout the property provide safe and efficient means for pedestrians to access the various uses within the property.

No adverse impact on pedestrian traffic in the area is anticipated as a result of developing the proposed project.

J. HISTORIC AND CULTURAL RESOURCES

The existing buildings on the property do not have any major historic significance on a local, regional or state level.

K. SPECIAL PROVISIONS

No special provisions or requirements are currently proposed for this facility.

END NOTE

1 Net difference between development plan analyzed within the Rowe Engineering Traffic Study compared to the PUD plan part of this proposal includes a net 3,100 sq ft reduction in the retail/flex-office out building on the West end of the site and an increase of 2.685 net rentable sf in storage space.

FIGURE 1

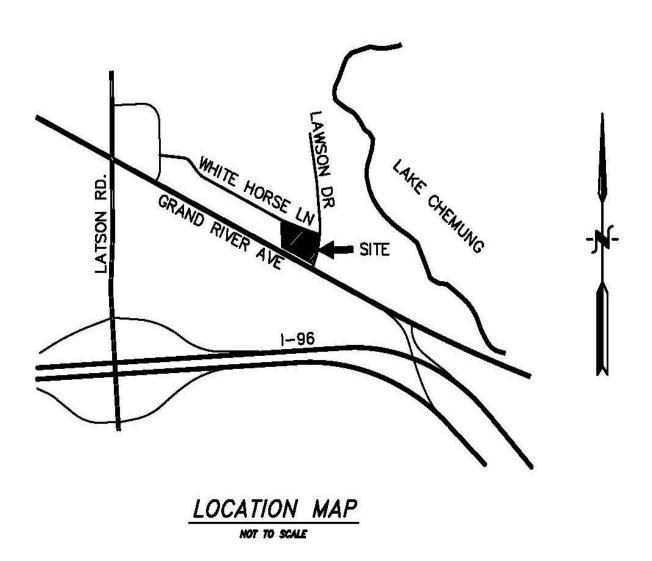


FIGURE 2

PHOTO DEPICTING SITE IMPROVEMENTS
NOT TO SCALE

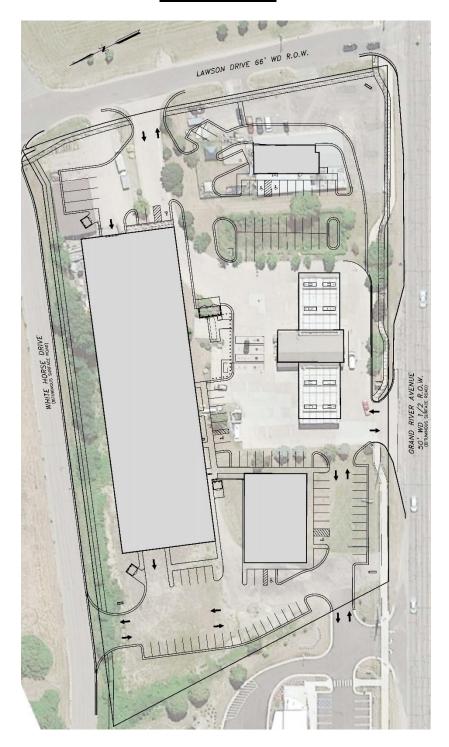


FIGURE 3

SOILS MAP (NOT TO SCALE)



SOILS MAP

Map Unit Symbol	Map Unit Name
BtB	Boyer-Oshtemo loamy sands, 2 to 6 percent slopes
BtC	Boyer-Oshtemo loamy sands, 6 to 12 percent slopes
BtE	Boyer-Oshtemo loamy sands, 18 to 25 percent slopes
BwA	Bronson loamy sand, 0 to 2 percent slopes
CvA	Conover loam, 0 to 2 percent slopes
FrB	Fox-Boyer complex, 2 to 6 percent slopes
FrC	Fox-Boyer complex, 6 to 12 percent slopes
FrD	Fox-Boyer complex, 12 to 18 percent slopes
FrE	-Fox-Boyer complex, 18 to 25 percent slopes
Но	Houghton muck
МоВ	Miami loam, 2 to 6 percent slopes
W Water	



Large Firm Resources. Personal Attention.

Attachments were removed to reduce packet size but are available upon request.

Memorandum

To: Mr. Daniel J. Boorstein

From: Michael J. Labadie, PE and Jill M. Bauer, PE, PTOE

Date: March 7, 2019

RE: Proposed Mixed-Use Development, Grand River Avenue, Genoa Township

ROWE Professional Services Company has completed our traffic impact assessment related to the proposed new mixed-use development including a coffee shop, retail, office, and a two-story self-storage building located on the north side of Grand River Avenue just west of Lawson Drive. The current site plan (Figure 1 included in the materials attached to this report), indicates a 7,000-square-foot retail building, 3,700-square-foot general office, 2,336-square-foot coffee shop with drive-thru, and a 42,740-square-foot net rentable self-storage building. Access to the site is proposed through one southern driveway onto Grand River Avenue, a northern drive onto White Horse Lane, an eastern driveway on to Lawson Drive, and a shared drive with the credit union located west of the site. This traffic impact assessment has been completed in accordance with the requirements specified by Genoa Township and the Livingston County Road Commission (LCRC).

Traffic Counts

Turning movement traffic counts were collected during the weekday AM (7 to 9 a.m.) and PM (4 to 6 p.m.) peak periods on October 25, 2018 at the following intersections:

- Grand River Avenue and Latson Road
- White Horse Lane and Lawson Drive
- Grand River Avenue and Lawson Drive
- Grand River Avenue and westbound I-96 off-ramp

The existing turning movement traffic counts are shown in Figure 2 attached to this memorandum.

Background Traffic Scenario

Historical traffic data from LCRC were referenced to determine the applicable growth rate for the existing traffic volumes to the project build-out year in 2019. Most recent traffic data from LCRC indicate that between 2009 and 2013, overall traffic volumes in the area have decreased or remained stagnant. Therefore, a 0.5 percent per year growth rate was applied to the project buildout year to calculate the background traffic without the proposed development. There are no other approved developments in the area to be included in the background traffic forecast.

The background traffic volumes are shown in Figure 3 attached to this memorandum.

Mr. Daniel J. Boorstein March 7, 2019 Page 2

Trip Generation

Using the information and methodologies specified in the latest version of *Trip Generation* (10th Edition) published by the Institute of Transportation Engineers (ITE), ROWE forecast the weekday AM and PM peak hour trips associated with the proposed mixed-use development. The results of the trip generation forecasts for the proposed site are provided in Table 1.

Table 1
ITE Trip Generation for Proposed Mixed-Use Development

	Land	o Generation		M Peak			M Peak I	Iour	
Land Use	Use Code	Size	In	Out	Total	In	Out	Total	Week Day
Coffee/Donut Shop with Drive-Thru Window	937	2,336 gsf	106	102	208	51	50	101	1,916
Shopping Center	820	7,000 gsf	4	3	7	36	40	76	986
General Office	710	3,700 gsf	3	1	4	1	4	5	43
Mini-Warehouse	151	42,740 net rentable sf	3	2	5	4	4	8	71
TOTAL TRIPS			116	108	224	92	98	190	3,016
Internal Capt	ure Trips:	11% (15%)	-13	-12	-25	-14	-15	-29	-332
TOTAL DRIVEWAY	TRIPS		103	96	199	78	83	161	2,684
Coffee/Donut Shop	Pass-by:	50% (50%)	-45	-45	-90	-18	-18	-36	-792
Shopping Center	r Pass-by:	34% (34%)	-1	-1	-2	-12	-12	-24	-335
TOTAL NEW TRIPS	3	·	57	50	107	48	53	101	1,557

Notes:

Trip generation based on trip rates or Fitted Curve Equations from the ITE Trip Generation Manual, 10th Edition AM (PM) Linked Trip % were based on the NCHRP 684 Internal Trip Capture Estimator Tool supported by ITE. Linked trips and pass-by trips based on information/procedures in the ITE Handbook, 3rd Edition. Pass-by trips were adjusted so that in/out trips would be equal.

Trip Distribution

The existing traffic patterns were used to develop a trip distribution model for the AM and PM peak hours for traffic generated by the proposed development. The existing traffic patterns indicate the following probable distribution for the proposed development:

AM Peak Hour

26% from and 7% to the north on Latson Road

16% from and 7% to the south on Latson Road

23% from and 19% to the west on Grand River Avenue

21% from and 41% to the east on Grand River Avenue

1% from and 0% to the west on White Horse Lane

1% from and 1% to the north on Lawson Drive

1% from and 1% to the south on Lawson Drive

11% from and 24% to I-94/Lake Chemung

Mr. Daniel J. Boorstein March 7, 2019 Page 3

PM Peak Hour

10% from and 21% to the north on Latson Road 9% from and 14% to the south on Latson Road 21% from and 29% to the west on Grand River Avenue 35% from and 23% to the east on Grand River Avenue 1% from and 1% to the west on White Horse Lane 0% from and 0% to the north on Lawson Drive 0% from and 0% to the south on Lawson Drive 24% from and 12% to I-94/Lake Chemung

The site plan indicates cross connections to the existing gas station and credit union. Traffic to and from the site would have the option to use driveways for these developments.

The proposed trip distribution for the site is shown in Figure 4 attached to this memorandum. The background traffic volumes were combined with the site generated traffic volumes to obtain the total future traffic volumes, which are shown in Figure 5 attached to this memorandum.

Level of Service Analysis

A level of service (LOS) analyses for existing, background (no build), and total future (build) conditions for the AM and PM peak hours was performed for the following intersections:

- Grand River Avenue and Latson Road
- White Horse Lane and Lawson Drive
- Grand River Avenue and Lawson Drive
- Grand River Avenue and westbound I-96 off-ramp

The existing site driveways onto Grand River Avenue were analyzed under total future traffic conditions for the AM and PM peak hours.

According to the most recent edition (6th Edition) of the Highway Capacity Manual, LOS is a qualitative measure describing operational conditions of a traffic stream or intersection. LOS ranges from A to F, with LOS A being the best. LOS D is generally considered to be acceptable. Tables 2 and 3 present the criteria for defining the various levels of service for unsignalized and signalized intersections, respectively.

Table 2
Level of Service Criteria (Unsignalized Intersection)

	A C4 170 1 757 1 1 1
Level of Service	Average Stopped Delay/Vehicle (seconds)
	(seconus)
A	≤10
В	$>10 \text{ and } \le 15$
С	>15 and ≤ 25
D	$>25 \text{ and } \le 35$
Е	>35 and ≤ 50
F	> 50

Note: LOS "D" is considered acceptable in urban/suburban areas.

Table 3
Level of Service Criteria (Signalized Intersection)

Level of Service	Average Stopped Delay/Vehicle (seconds)
A	≤10
В	$> 10 \text{ and } \le 20$
С	$> 20 \text{ and } \le 35$
D	$> 35 \text{ and } \le 55$
Е	$> 55 \text{ and } \le 80$
F	> 80

Note: LOS "D" is considered acceptable in urban/suburban areas.

The results of the level of service analyses for the aforementioned intersections listed are summarized in Tables 4 through 22.

Signalized Intersection of Grand River Avenue and Latson Road

The results of the level of service analysis for the signalized intersection of Grand River Avenue and Latson Road indicate that, under existing conditions, the eastbound and westbound approaches operate at an LOS C, the northbound approach operates at an LOS F, and the southbound approach operates at an LOS D during the AM peak hour. During the PM peak hour, the northbound, southbound, and eastbound approaches operate at an LOS D and the westbound approach operates at an LOS E. The overall intersection operates at an LOS D during the AM and PM peak hours.

With the addition of both background and site generated traffic, all approaches to the intersection would continue to operate at the same LOS during the AM and PM peak hours with the exception of the westbound approach during the AM peak hour which experiences an increase in delay of 1.2 seconds and decreases from an LOS C to an LOS D. The overall intersection would continue to operate at an LOS D during the AM peak hour and at an LOS E during the PM peak hour. Therefore, the traffic generated by the proposed development would have a minimal impact on the operation of this intersection.

The operational results for the intersection of Grand River Avenue and Latson Road are presented in Tables 4 and 5.

Table 4
AM Peak Hour
Level of Service Analysis for Grand River Avenue and Latson Road

Approach	Existing	Background	Future
Eastbound Grand River Avenue	C (33.3)	C (33.5)	C (34.3)
Westbound Grand River Avenue	C (34.2)	C (34.5)	D (35.7)
Northbound Latson Road	F (95.4)	F (96.5)	F (103.1)
Southbound Latson Road	D (35.5)	D (35.5)	D (35.6)

Table 5
PM Peak Hour
Level of Service Analysis for Grand River Avenue and Latson Road

Approach	Existing	Background	Future
Eastbound Grand River Avenue	D (38.7)	D (39.0)	D (39.6)
Westbound Grand River Avenue	E (71.3)	E (73.2)	E (79.3)
Northbound Latson Road	D (48.1)	D (48.6)	D (48.4)
Southbound Latson Road	D (45.3)	D (45.5)	D (46.5)

(XX.X) Average seconds of delay per vehicle

Signalized Intersection of Grand River Avenue and Lawson Drive

The results of the level of service analysis for the signalized intersection of Grand River Avenue and Lawson Drive indicate that, under existing conditions, the eastbound and westbound approaches operate at an LOS A, the northbound approach operates at an LOS D, and the southbound approach operates at an LOS E during the AM peak hour. During the PM peak hour, the eastbound and westbound approaches operate at an LOS A and the northbound and southbound approaches operate at an LOS D. The overall intersection operates at an LOS A during the AM peak hour and an LOS B during the PM peak hour.

Additional improvements to the intersection of Grand River Avenue and Lawson Drive were designed and approved in 2004 as a part of the amended and restated Summerfield Pointe planned unit development agreement. This approved design included the construction of a left turn lane on the southbound Lawson Drive approach. Although these improvements have not been constructed, they were included as a part of the background scenario.

With the addition of background improvements on Lawson Drive and both background and site generated traffic, the eastbound and westbound approaches would continue to operate at an LOS A during the AM and PM peak hours. The northbound approach would continue to operate at an LOS D during the AM peak hour but fall to an LOS E during the PM peak hour. The southbound approach would operate at an LOS E during the AM peak hour and an LOS D during the PM peak hour. The overall intersection would operate at an LOS A during the AM peak hour and an LOS B during the PM peak hour.

The operational results for the intersection of Grand River Avenue and Lawson Drive are presented in Tables 6 and 7.

Table 6
AM Peak Hour
Level of Service Analysis for Grand River Avenue and Lawson Drive

Approach	Existing	Background	Future
Eastbound Grand River Avenue	A (4.8)	A (4.8)	A (4.8)
Westbound Grand River Avenue	A (0.6)	A (0.6)	A (0.7)
Northbound Lawson Drive	D (39.0)	D (40.0)	D (40.8)
Southbound Lawson Drive	E (60.5)	D (42.8)	E (64.5)

Table 7
PM Peak Hour
Level of Service Analysis for Grand River Avenue and Lawson Drive

Approach	Existing	Background	Future
Eastbound Grand River Avenue	A (4.8)	A (4.8)	A (4.8)
Westbound Grand River Avenue	A (7.9)	A (7.9)	A (8.1)
Northbound Lawson Drive	D (48.3)	E (61.1)	E (68.4)
Southbound Lawson Drive	D (38.7)	D (37.9)	D (38.3)

(XX.X) Average seconds of delay per vehicle

To mitigate the delay, the signal timing for the intersection of Grand River Avenue and Lawson Drive was optimized, increasing the green time for the northbound and southbound phasing, adding 3 seconds during the AM peak hour and 2 seconds during the PM peak hour. The eastbound and westbound phasing was decreased respectively. The results of this analysis indicate that the eastbound approach would continue to operate at an LOS A during the AM and PM peak hours. The westbound approach would operate at an LOS A during the AM peak hour and an LOS B during the PM peak hour. The northbound and southbound approaches would operate at an LOS D during the AM and PM peak hours.

The results of this evaluation are presented in Tables 8 and 9.

Table 8
AM Peak Hour
Level of Service Analysis for Grand River Avenue and Lawson Drive

Approach	Existing	Background	Future (Mit)
Eastbound Grand River Avenue	A (4.8)	A (4.8)	A (6.1)
Westbound Grand River Avenue	A (0.6)	A (0.6)	A (0.8)
Northbound Lawson Drive	D (39.0)	D (40.0)	D (37.3)
Southbound Lawson Drive	E (60.5)	D (42.8)	D (46.0)

(XX.X) Average seconds of delay per vehicle

Table 9
PM Peak Hour
Level of Service Analysis for Grand River Avenue and Lawson Drive

Approach	Existing	Background	Future (Mit)
Eastbound Grand River Avenue	A (4.8)	A (4.8)	A (5.8)
Westbound Grand River Avenue	A (7.9)	A (7.9)	B (18.1)
Northbound Lawson Drive	D (48.3)	E (61.1)	D (51.1)
Southbound Lawson Drive	D (38.7)	D (37.9)	D (36.2)

(XX.X) Average seconds of delay per vehicle

Signalized Intersection of Grand River Avenue and I-96 Westbound Off-Ramp

The results of the level of service analysis for the signalized intersection of Grand River Avenue and the I-96 westbound off-ramp indicate that, under existing conditions, all approaches operate at an acceptable LOS D or above during the AM and PM peak hours. The overall intersection operates at an LOS A during the AM peak hour and an LOS C during the PM peak hour.

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With the addition of both background and site generated traffic, all approaches to the intersection would continue to operate at the same LOS during the AM and PM peak hours. The overall intersection would continue to operate at an LOS A during the AM peak hour and an LOS C during the PM peak hour.

The operational results for the intersection of Grand River Avenue and I-96 Westbound Off-Ramp are presented in Tables 10 and 11.

Table 10
AM Peak Hour
Level of Service Analysis for Grand River Avenue and I-96 Westbound Off-Ramp

Approach	Existing	Background	Future
Eastbound Grand River Avenue	A (0.4)	A (0.4)	A (0.4)
Westbound Grand River Avenue	A (4.5)	A (4.5)	A (4.7)
Northbound I-96 Off-Ramp	D (39.7)	D (39.7)	D (39.6)

(XX.X) Average seconds of delay per vehicle

Table 11
PM Peak Hour
Level of Service Analysis for Grand River Avenue and I-96 Westbound Off-Ramp

Approach	Existing	Background	Future
Eastbound Grand River Avenue	C (22.6)	C (22.7)	C (23.0)
Westbound Grand River Avenue	B (13.8)	B (13.9)	B (14.3)
Northbound I-96 Off-Ramp	C (32.5)	C (32.5)	C (32.4)

(XX.X) Average seconds of delay per vehicle

<u>Unsignalized Intersection of White Horse Lane and Lawson Drive</u>

The results of the level of service analysis for the unsignalized intersection of White Horse Lane and Lawson Drive indicate that, under existing conditions, all approaches operate at an LOS A during the AM and PM peak hours. The overall intersection operates at an LOS A during the AM and PM peak hours.

With the addition of both background and site generated traffic, all approaches to the intersection would continue to operate at an LOS A during the AM and PM peak hours. The overall intersection would continue to operate at an LOS A during the AM and PM peak hours.

The operational results for the intersection of Grand River Avenue and Lawson Drive are presented in Tables 12 and 13.

Table 12 AM Peak Hour Level of Service Analysis for White Horse Lane and Lawson Drive

Approach	Existing	Background	Future
Eastbound White Horse Lane	A (8.7)	A (8.7)	A (8.7)
Westbound Tractor Supply Driveway	A (9.3)	A (9.3)	A (9.3)
Northbound Lawson Drive	A (2.0)	A (2.0)	A (2.0)
Southbound Lawson Drive	A (0.0)	A (0.0)	A (0.0)

Table 13
PM Peak Hour
Level of Service Analysis for White Horse Lane and Lawson Drive

Approach	Existing	Background	Future
Eastbound White Horse Lane	A (8.7)	A (8.7)	A (8.7)
Westbound Tractor Supply Driveway	A (9.9)	A (9.9)	A (9.9)
Northbound Lawson Drive	A (3.6)	A (3.6)	A (3.6)
Southbound Lawson Drive	A (0.4)	A (0.4)	A (0.4)

(XX.X) Average seconds of delay per vehicle

<u>Unsignalized Intersections of Grand River Avenue and the Southeast and Southwest Site Driveways</u>

The site will have one access drive and a shared access drive with the existing credit union on the north side of Grand River Avenue just west of Lawson Drive. The results of the level of service analysis for these intersections indicate that, under future traffic conditions, the site driveway approach would operate at an acceptable LOS D or above during the AM and PM peak hours. The Grand River Avenue approach would operate at LOS A during both peak hours.

A left-turn lane evaluation was not performed since there is currently a two-way center left-turn lane on Grand River Avenue at the location of the site driveway. Additionally, a right turn lane exists for the site access driveway.

The operational results for the intersections of Grand River Avenue and the southeast and southwest driveways are presented in Tables 14 through 17.

Table 14
AM Peak Hour
Level of Service Analysis for Grand River Avenue and SW Access

Approach	Existing	Background	Future
Eastbound Grand River Avenue	A (0.10)	A (0.10)	A (0.10)
Westbound Grand River Avenue	A (0.0)	A (0.0)	A (0.0)
Southbound SW Access	C (16.0)	C (16.1)	C (17.3)

(XX.X) Average seconds of delay per vehicle

Table 15
PM Peak Hour
Level of Service Analysis for Grand River Avenue and SW Access

Approach	Existing	Background	Future
Eastbound Grand River Avenue	A (0.2)	A (0.2)	A (0.3)
Westbound Grand River Avenue	A (0.0)	A (0.0)	A (0.0)
Southbound SW Access	C (21.0)	C (21.0)	C (24.6)

Table 16
AM Peak Hour
Level of Service Analysis for Grand River Avenue and SE Access

Approach	Existing	Background	Future
Eastbound Grand River Avenue	A (0.4)	A (0.4)	A (0.8)
Westbound Grand River Avenue	A (0.0)	A (0.0)	A (0.0)
Southbound SE Access	C (19.1)	C (19.2)	D (32.7)

(XX.X) Average seconds of delay per vehicle

Table 17
PM Peak Hour
Level of Service Analysis for Grand River Avenue and SE Access

Approach	Existing	Background	Future
Eastbound Grand River Avenue	A (0.5)	A (0.5)	A (0.7)
Westbound Grand River Avenue	A (0.0)	A (0.0)	A (0.0)
Southbound SE Access	C (21.2)	C (21.3)	C (24.3)

(XX.X) Average seconds of delay per vehicle

Unsignalized Intersection of Lawson Drive and the Eastern Site Driveway

The site will have an access drive on Lawson Drive. The results of the level of service analysis for the intersection indicate that, under future traffic conditions, the site driveway approach would operate at an LOS A during the AM and PM peak hours. The Lawson Drive approaches would operate at LOS A during both peak hours.

The operational results for the intersections of Lawson Drive and the Eastern Site Driveway are presented in Tables 18 and 19.

Table 18
AM Peak Hour
Level of Service Analysis for Eastern Access and Lawson Drive

Approach	Existing	Background	Future
Eastbound E Access	A (8.8)	A (8.8)	A (9.0)
Northbound Lawson Drive	A (0.9)	A (0.9)	A (3.3)
Southbound Lawson Drive	A (0.0)	A (0.0)	A (0.0)

(XX.X) Average seconds of delay per vehicle

Table 19
PM Peak Hour

Level of Service Analysis for Eastern Access and Lawson Drive

Approach Existing Background Future

Approach	Existing	Background	Future
Eastbound E Access	A (8.7)	A (8.7)	A (8.8)
Northbound Lawson Drive	A (0.6)	A (0.6)	A (2.1)
Southbound Lawson Drive	A (0.0)	A (0.0)	A (0.0)

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Unsignalized Intersection of White Horse Lane and the Northern Site Driveway

The site will have an access drive on White Horse Lane. The results of the level of service analysis for the intersection indicate that, under future traffic conditions, the site driveway approach would operate at an LOS A during the AM and PM peak hours. The White Horse Lane approaches would operate at LOS A during both peak hours.

The operational results for the intersections of White Horse Lane and the Northern Site Driveway are presented in Tables 20 and 21.

Table 20 AM Peak Hour

Level of Service Analysis for White Horse Lane and Northern Access

Approach	Existing	Background	Future
Eastbound White Horse Lane	N/A	N/A	A (0.0)
Westbound White Horse Lane	N/A	N/A	A (0.0)
Northbound N Access	N/A	N/A	A (0.0)

(XX.X) Average seconds of delay per vehicle

Table 21 PM Peak Hour

Level of Service Analysis for White Horse Lane and Northern Access

Approach	Existing	Background	Future
Eastbound White Horse Lane	N/A	N/A	A (0.0)
Westbound White Horse Lane	N/A	N/A	A (0.0)
Northbound N Access	N/A	N/A	A (9.0)

(XX.X) Average seconds of delay per vehicle

Conclusions and Recommendations

The proposed mixed-use development consists of a 7,000-square-foot retail building, 3,700-square-foot general office, 2,336-square-foot coffee shop with drive-thru, and a 42,740-square-foot net rentable self-storage building. The proposed development will have access to Grand River Avenue through two site driveways along with a northern drive onto White Horse Lane and an eastern driveway to Lawson Drive.

The proposed development is forecast to generate 107 trips during the AM peak hour (57 inbound and 50 outbound from the site) and 101 trips during the PM peak hour (48 inbound and 53 outbound from the site).

An operational analysis was performed for existing, background, and total future conditions for the following intersections:

- Grand River Avenue and Latson Road
- White Horse Lane and Lawson Drive
- Grand River Avenue and Lawson Drive
- Grand River Avenue and westbound I-96 off-ramp

The operational reviews indicated that there was minimal impact to the intersections due to the proposed development except for the northbound approach to the Grand River Avenue and Lawson Drive intersection. Mitigation of this can be achieved through signal timing optimization and lengthening the

Mr. Daniel J. Boorstein March 7, 2019 Page 11

green time for the southbound and northbound phase during the AM and PM peak hour. This scenario includes proposed improvements to the intersection of Grand River Avenue and Lawson Drive as a part of the Summerfield development that have not yet been constructed. These proposed improvements include construction of a left turn lane on the southbound Lawson Drive approach.

An operational analysis of the proposed site driveways was performed for total future conditions. This operational review indicated that the site driveways would operate at acceptable levels during both the AM and PM peak hours.

Attachments

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BMH - PREMIER - P.U.D.

Howell, Michigan











AH-PREMIER P.U.D. HOWFLL MI

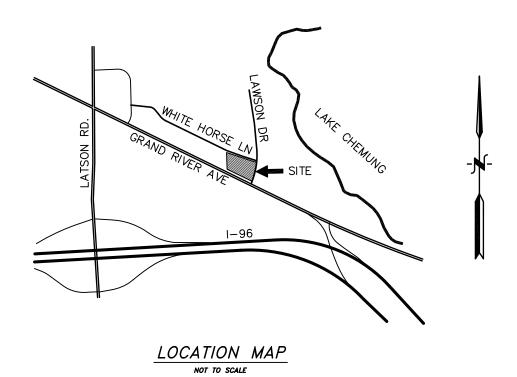
COVER SHEET

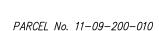
Scale N.T.S.

Date 2/19/20
Sht. No. Δ_Ω

CONCEPTUAL PLAN BMH - PREMIER P.U.D.

GENOA TOWNSHIP, MICHIGAN A PART OF THE NE 1/4 OF SECTION 9, T2N, R5E. GENOA TOWNSHIP, LIVINGSTON COUNTY, MICHIGAN





LEGAL DESCRIPTION

Michigan, and described as follows:

PARCEL No. 11-09-200-005

Reference: Certified Land Survey as recorded in

Document No. 2010S-0020, Livingston County Records.

Situated in the Township of Genoa, County of Livingston and State of

Commencing at the North 1/4 Corner of Section 9, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan; thence S01°30'47"E

(S02°24'00"W as recorded) 636.58 feet along the North-South 1/4 line of said Section 9 to the PLACE OF BEGINNING; thence S75°54'37"E 226.58 feet (S72°00'00"E 226.54 feet as recorded); thence S26°05'13"W 198.42 feet (S30°0'W 200.00 feet as recorded); thence N63°53'34"W 142.55 feet (N60°00'00"W 141.66 feet as recorded) along the Northerly

line of Grand River Avenue (50 foot wide 1/2 Right-of-Way); thence NO1°30'47"W 170.70 feet (NO2°24'00"E 172.53 feet as recorded) along said North-South 1/4 line of Section 9 to the Place of Beginning. Being a part of the Northeast 1/4 of Section 9, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan. Containing 0.75 acres of land, more or less. Subject to and together with all easements and

restrictions affecting title to the described above premises.

Commencing at the North 1/4 Corner of Section 9, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan; thence S01°30'47"E 807.28 feet (S02°24'00"W 809.11 feet as recorded) along the North-South 1/4 line of said Section 9; thence S63°53'34"E 142.55 feet (S60°00'00"E 141.66 feet as recorded) along the Northerly line of Grand River Avenue (50 foot wide 1/2 Right-of-Way) to the PLACE OF BEGINNING; thence N26°05'13"E 198.42 feet (N30°00'00"E 200.00 feet as recorded); thence S63°54'47"E (S60°00'00"E as recorded) 140.00 feet; thence S26°05'13"W 198.47 feet (S30°00'00"W 200.00 feet as recorded); thence N63°53'34"W (N60°00'00"W as recorded) 140.00 feet along said Northerly line of Grand River Avenue to the Place of Beginning Being a part of the Northeast 1/4 of Section 9, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan. Containing 0.64 acres of land, more or less. Subject to and together with all easements and restrictions affecting title to the described above premises.

PARCEL No. 11-09-200-014

Commencing at the North 1/4 Corner of Section 9, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan; thence S01°30'47"E 807.28 feet (S02°24'00"W 809.11 feet as recorded) along the North-South 1/4 line of said Section 9; thence S63°53'34"E 432.29 feet (S60°00'00"E 431.52 feet as recorded) along the Northerly line of Grand River Avenue (50 foot wide 1/2 Right-of-Way) to the PLACE OF BEGINNING; thence N21°37'33"E 216.19 feet (N25°32'20"E 217.67 feet as recorded); thence S68°22'27"E 106.63 feet (S64°27'40"E 121.69 feet as recorded); thence S01°17'00"W 23.43 feet (S05°09'00"W 23.38 feet as recorded): thence \$13.02.24.W 200.62 feet (\$16.19.00.W 210 feet as recorded); thence S15°21'15"W 7.30 feet; thence N63°53'34"W 145.97 feet (N60°00'00"W 148.90 feet as recorded) along said Northerly line of Grand River Avenue to the Place of Beginning. Being a part of the Northeast 1/4 of Section 9, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan. Containing 0.65 acres of land, more or less. Together with a private wide easement for ingress and egress as recorded in Liber 2219, Page 260, Livingston County Records, also subject to and together with all easements and restrictions affecting title to the described above premises.

PARCEL No. 11-09-200-016

Commencing at the North 1/4 Corner of Section 9, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan; thence S01°30'47"E 459.11 feet along the North-South 1/4 line of said Section 9 to the PLACE OF BEGINNING; thence S71°13'30"E 502.18 feet along the South line of White Horse Drive (Variable width Right-of-Way); thence S00°31'08"E 175.68 feet; thence N71°12'19"W 134.00 feet; thence N63°54'47"W 140.00 feet; thence N75°54'37"W 226.58 feet; thence NO1°30'47"W 177.47 feet to the Place of Beginning. Being a part of the Northeast 1/4 of Section 9, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan. Containing 1.83 acres of land, more or less. Subject to and together with all easements and restrictions affecting title to the described above premises.

PARCEL No. 11-09-200-017

Commencing at the North 1/4 Corner of Section 9, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan; thence S01°30'47" 459.11 feet along the North-South 1/4 line of said Section 9; thence S71°13'30"E 502.18 feet along the South line of White Horse Drive (Variable width Right-of-Way) to the PLACE OF BEGINNING; thence continuing S71°13'30"E 108.69 feet along said South line of White Horse Drive; thence N86°53'15"E 45.07 feet; thence Southerly 181.88 feet along the arc of a 993.63 foot radius curve to the right, through a central angle of 10°29'16" and having a long chord bearing S10°06'37"W 181.63 feet along the Westerly line of Lawson Drive (66 foot wide Right-of-Way); thence continuing S15°21'15"W 230.74 feet along said Westerly line of Lawson Drive; thence N13°02'24"E 200.62 feet; thence N01°17°00"E 23.43 feet; thence N68°22'27"W 106.63 feet; thence N00°31'08"W 175.68 feet to the Place of Beginning. Being a part of the Northeast 1/4 of Section 9, Town 2 North, Range 5 East, Genoa Township, Livingston County, Michigan. Containing 0.57 acres of land, more or less. Subject to and together with all easements and restrictions affecting title to the described



AERIAL PHOTOGRAPH SCALE: 1in. = 60ft

Google maps image and is orientated to the engineering line work within reasonable accuracy and precision, and may not accurately depict current site

SHEET INDEX

EXISTING CONDITIONS

PRELIMINARY PUD

PDP PARALLEL DEVELOPMENT PLAN

SITE PLAN

UTILITY PLAN

EVC EMERGENCY VEHICLE CIRCULATION PLAN

GR1 GRADING PLAN

GR2 RETENTION BASIN DETAIL AND DRAINAGE CALCULATION

DT1 NOTES AND DETAILS

SIGNAGE AND PAVEMENT MARKING NOTES AND DETAILS

PRELIMINARY LANDSCAPE PLAN

ENLARGEMENTS

ENLARGEMENTS AND LANDSCAPE DETAILS

SELF STORAGE BUILDING, FLOOR PLANS

SELF STORAGE BUILDING, ELEVATIONS

RETAIL BUILDING

FUEL STATION AND CONVENIENCE STORE

SECTIONS

OWNER BMH REALITY L.L.C. 775 N. SECOND STREET BRIGHTON, MICHIGAN 48116

DEVELOPER PREMIER GENOA LLC 775 N. SECOND STREET BRIGHTON, MICHIGAN 48116

ARCHITECT

DRN & ASSOCIATES, ARCHITECTS PC 50850 APPLEBROOKE DR. NORTHVILLE, MICHIGAN 48167 PHONE (248) 880-6523

ENGINEER/SURVEYOR

DESINE INC. 2183 PLESS DRIVE BRIGHTON, MICHIGAN 48114

LANDSCAPE ARCHITECT

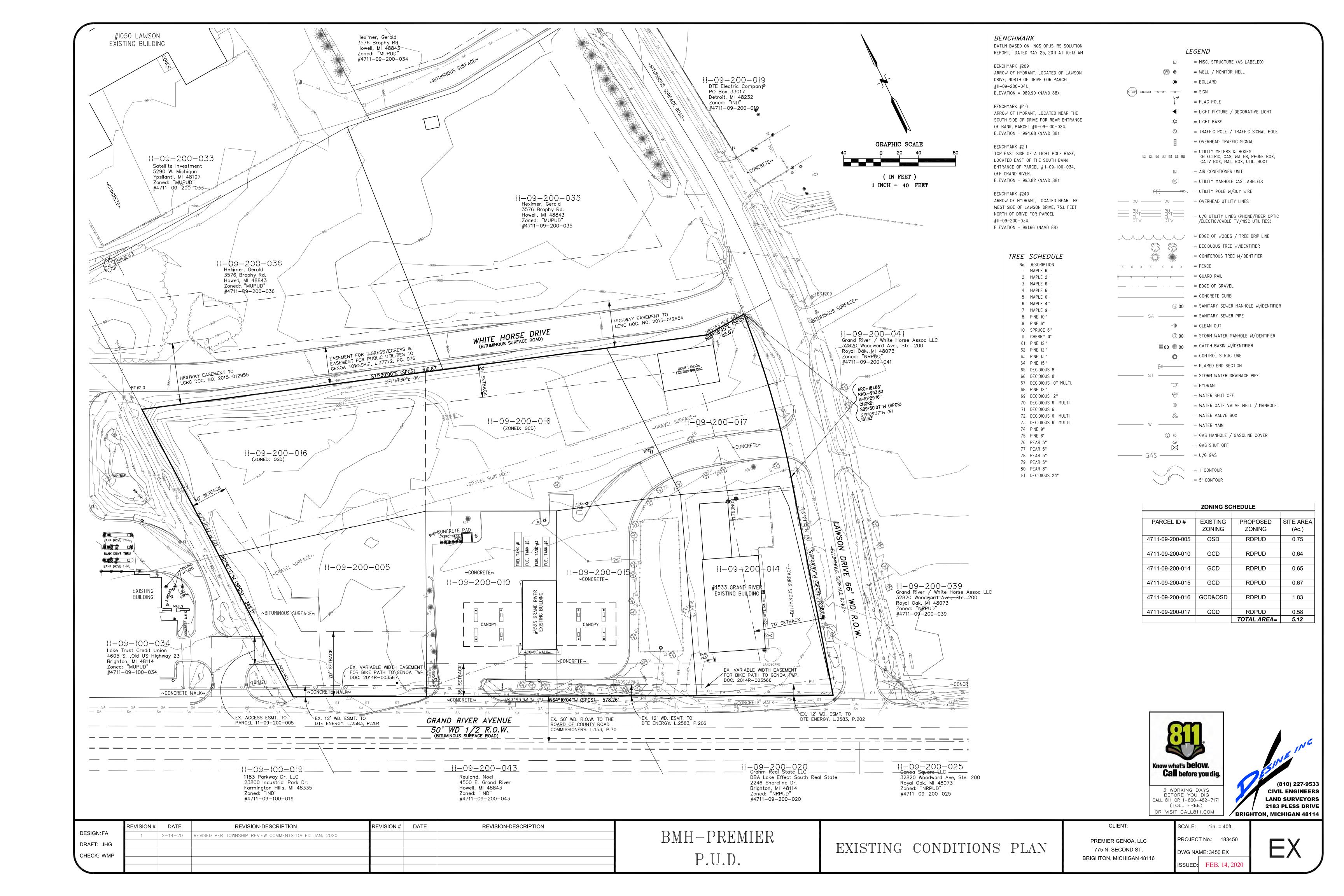
VERT VERDE LANDSAPE ARCHITECT PLYMOUTH, MICHIGAN

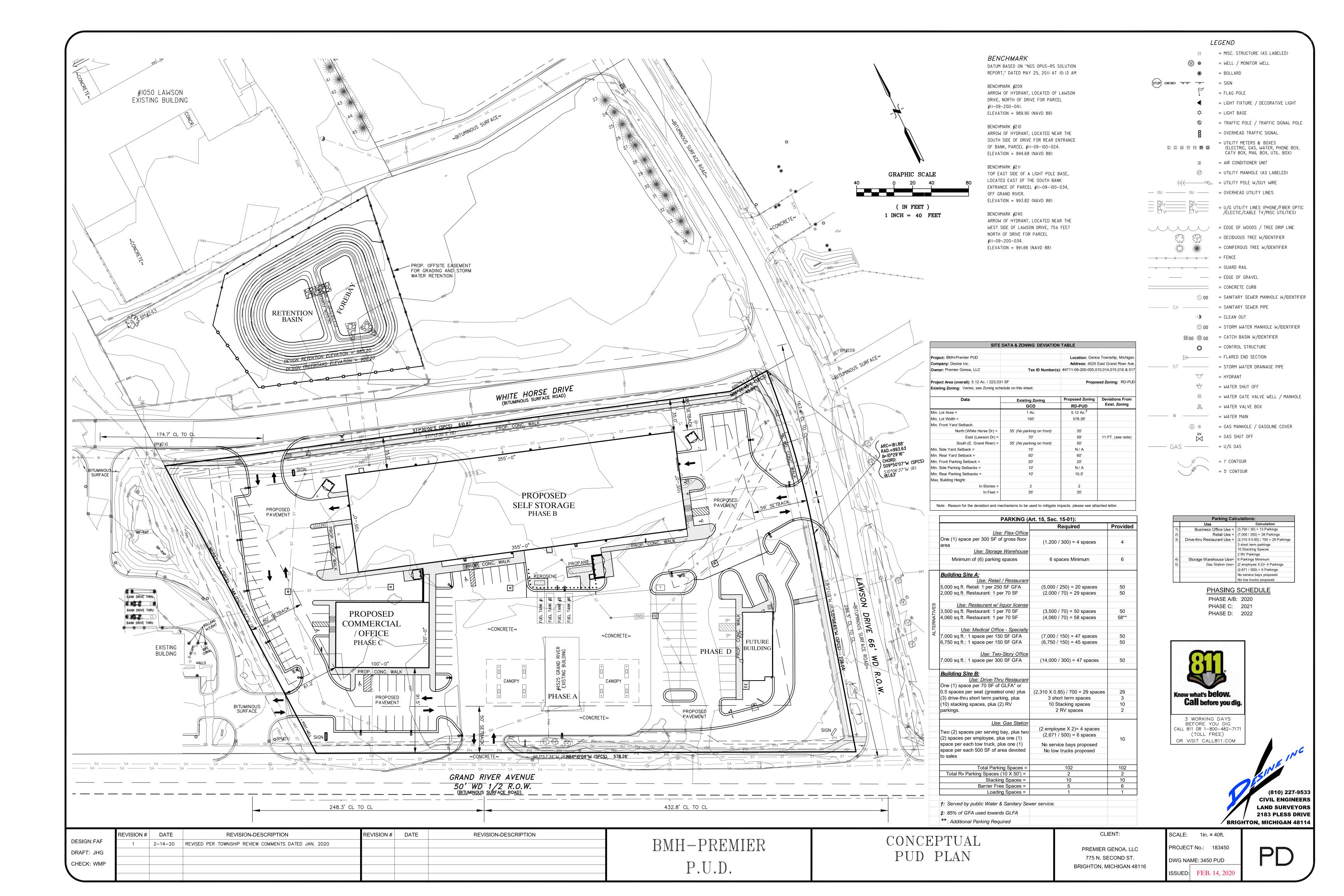


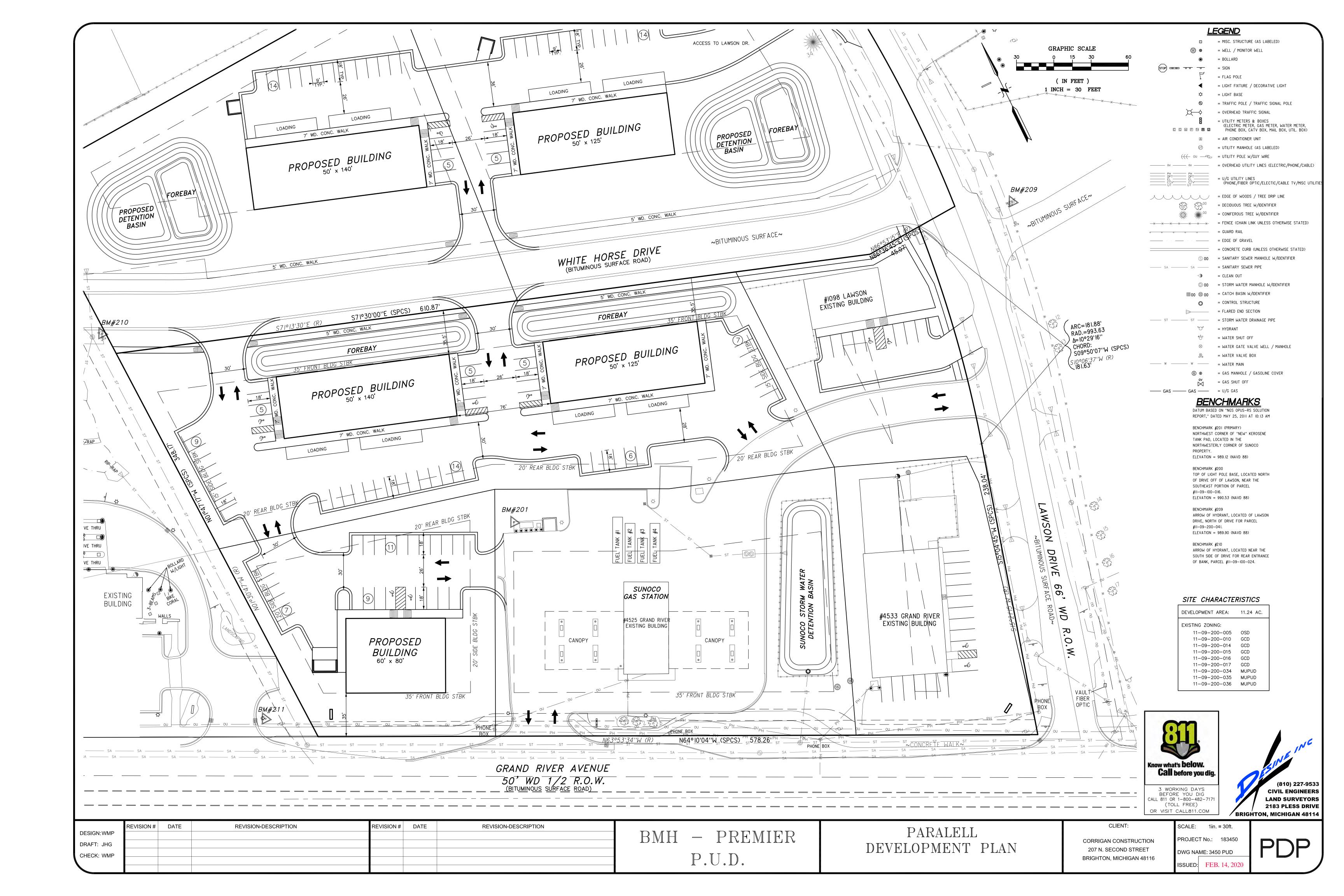


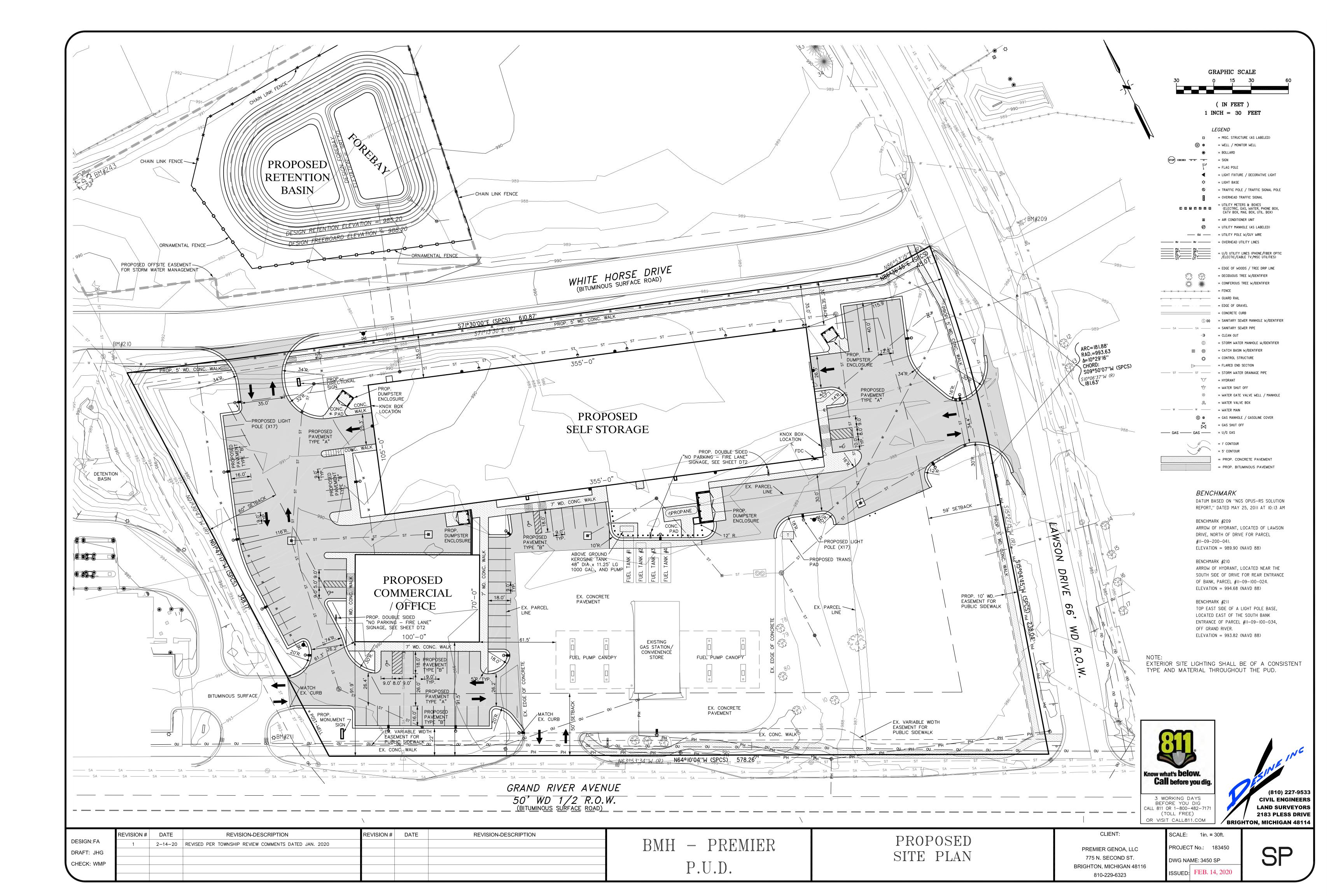
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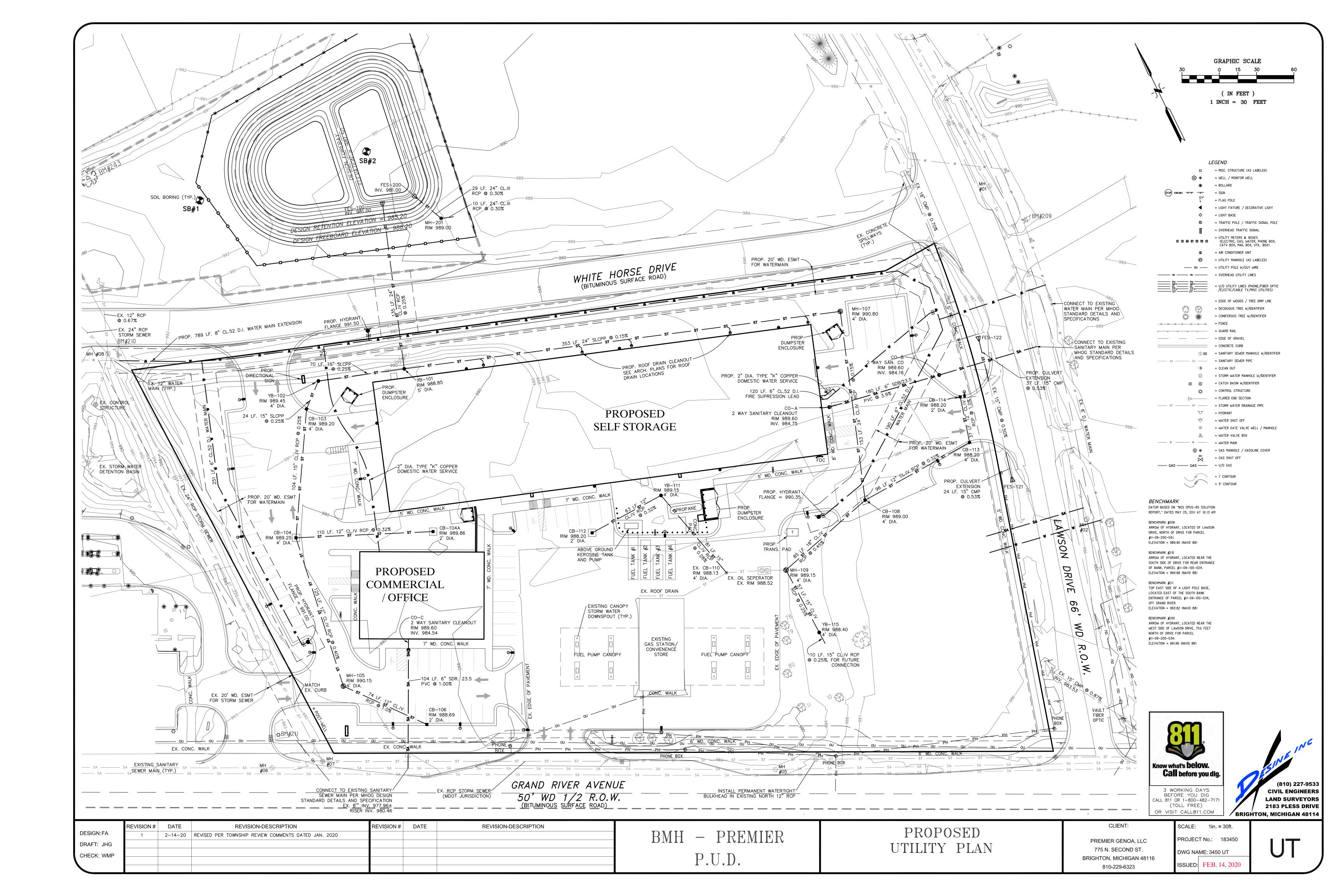
PHONE: (810) 227-9533

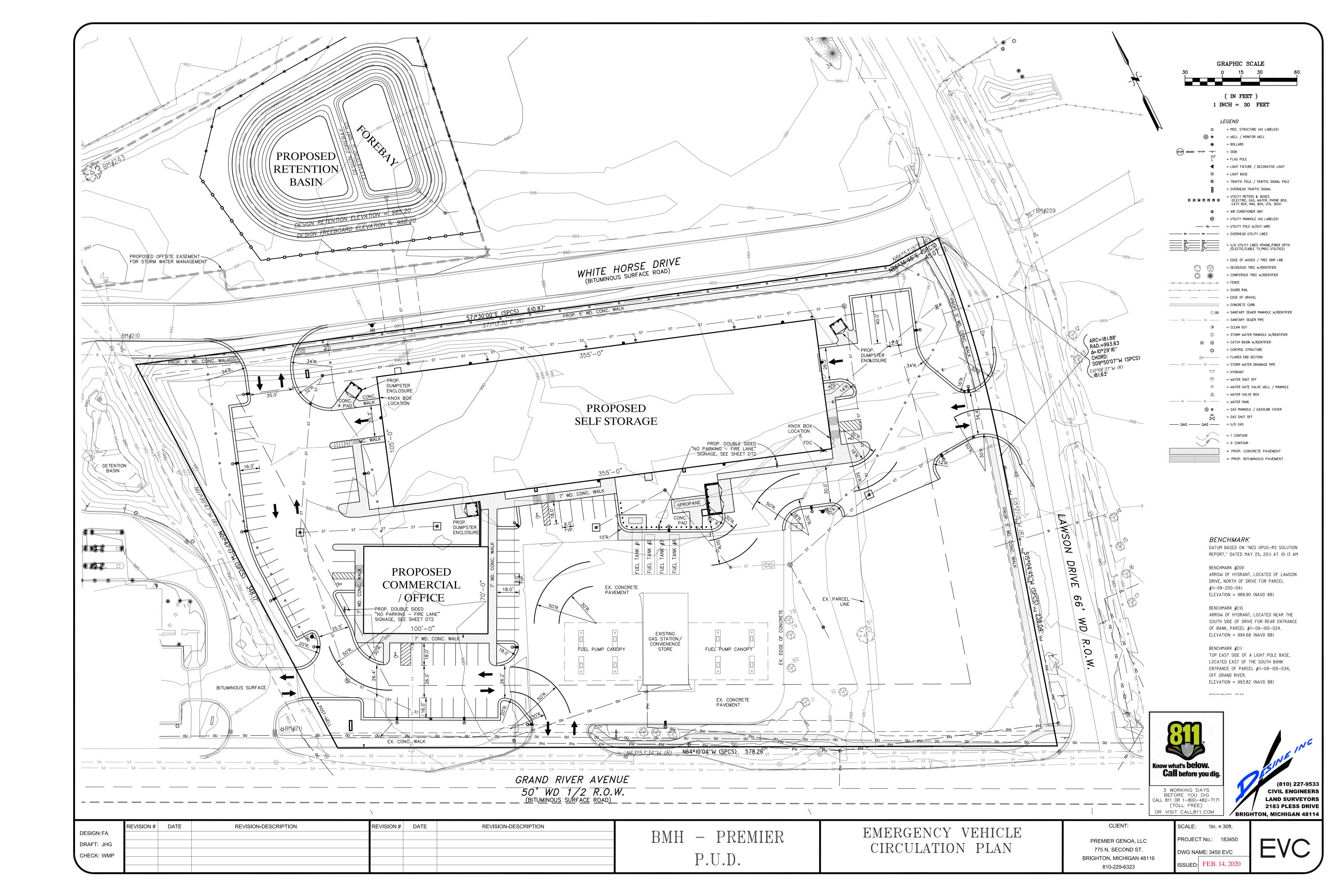


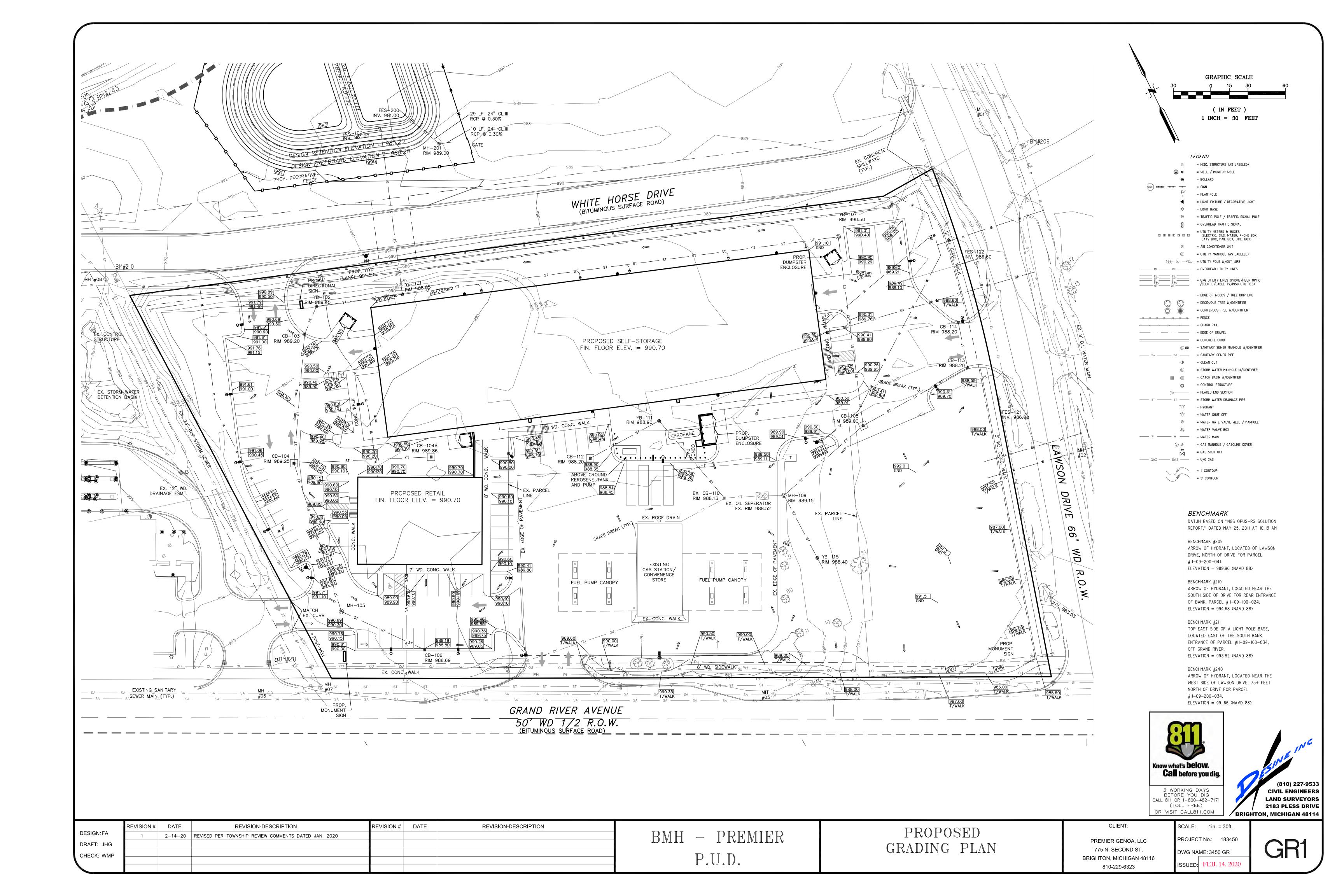


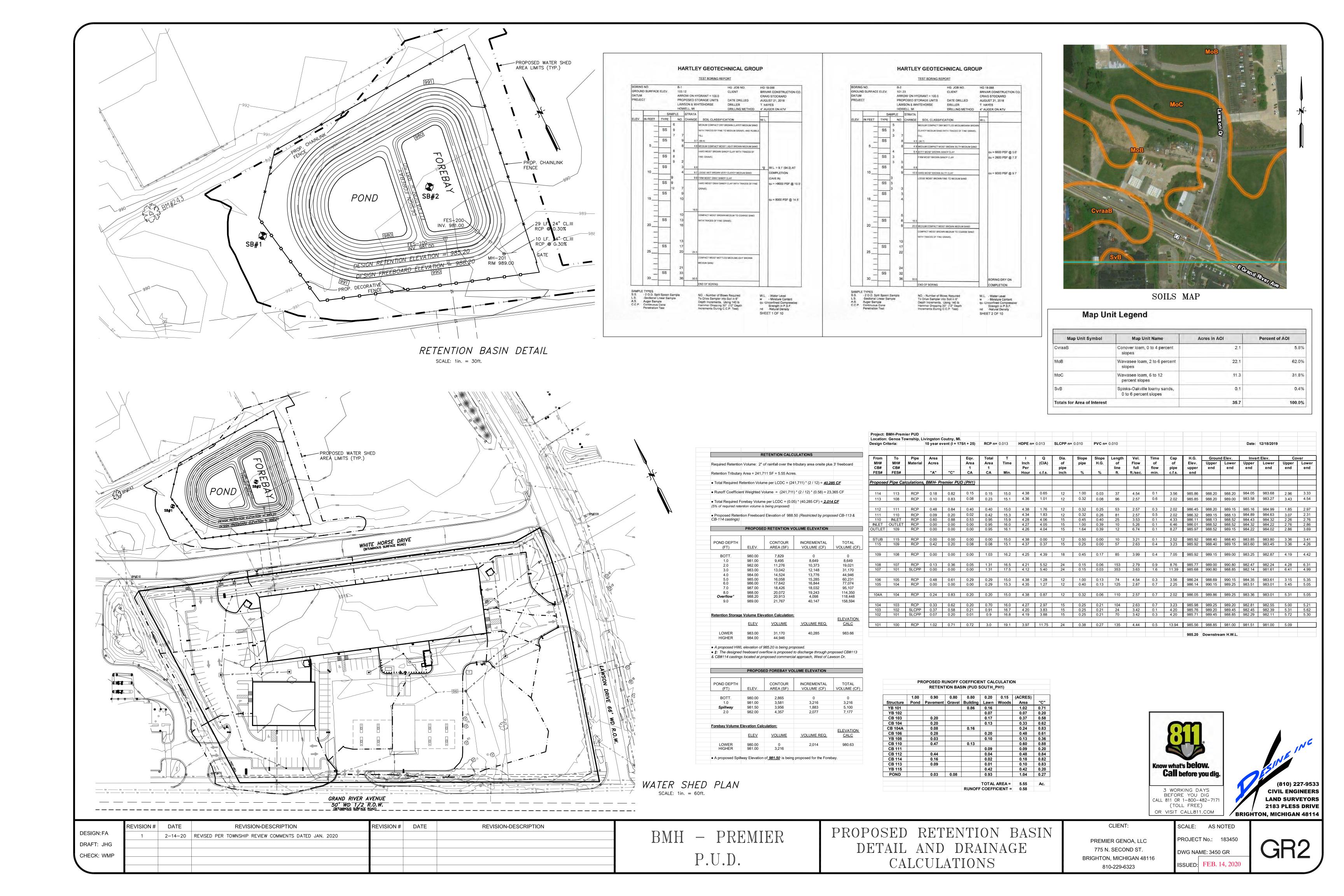


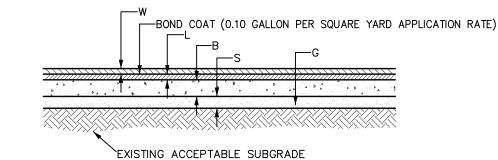












BITUMINOUS PAVEMENT CROSS SECTION

NOT TO SCALE

TYPE "A" CROSS SECTION

KEY	DESCRIPTION	MATERIAL SPECIFICATION	MINIMUM COMPACTED THICKNESS
W	WEARING COURSE	MDOT 36A	1.5"
L	LEVELING COURSE	MDOT 13A	2.5"
В	AGGREGATE BASE	MDOT 22A	8"
S	GRANULAR SUBBASE	MDOT CLASS II	7"
G	GEOGRID	N/A	N/A

TYPE "B" CROSS SECTION

KEY	DESCRIPTION	MATERIAL SPECIFICATION	MINIMUM COMPACTED THICKNESS
W	WEARING COURSE	MDOT 36A	1.5"
L	LEVELING COURSE	MDOT 13A	1.5"
В	AGGREGATE BASE	MDOT 22A	8"
S	GRANULAR SUBBASE	MDOT CLASS II	7"
G	GEOGRID	N/A	N/A

MINIMUM TOWNSHIP CROSS SECTION

KEY	DESCRIPTION	MATERIAL SPECIFICATION	MINIMUM COMPACTED THICKNESS
W	WEARING COURSE	MDOT 36A	1.5"
L	LEVELING COURSE	MDOT 13A	1.5"
В	AGGREGATE BASE	MDOT 22A	6"
S	GRANULAR SUBBASE	MDOT CLASS II	6"
G	GEOGRID	N/A	N/A

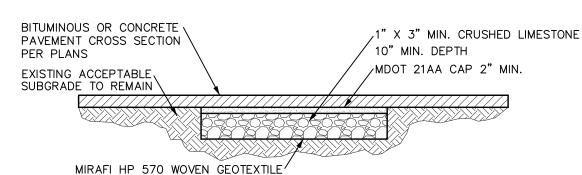
BITUMINOUS PAVEMENT NOTES:

1. The construction specifications of the appropriate Local Municipality are a part of this work. Refer to the General Notes, Road and/or Driveway and Parking Lot Construction Notes and Typical Road and/or Pavement Cross Section details on the project plans for additional requirements.

2. Unsuitable soils found within the 1 on 1 influence zone of the roadway, such as muck, peat, topsoil, marl, silt or other unstable materials shall be excavated and replaced up to the proposed subgrade elevation with MDOT Class III granular material compacted to 95% maximum unit weight, modified proctor.

3. Contractor shall proof roll prepared subgrade as directed by Engineer. Unacceptable areas of subgrade shall be undercut and replaced as directed by Engineer

4. Owner/Developer may delay placement of the bituminous wearing course outside of the public road right of way. Repair of the bituminous leveling course may be necessary due to any delay in placement of the bituminous wearing course. Substantial repair to the bituminous leveling course may be necessary if placement of the bituminous wearing course is delayed for more than 12 months after placement of the bituminous leveling course. The bituminous leveling course shall be repaired as directed by Engineer prior to placement of the bituminous wearing course.



SUBGRADE UNDERCUT AND REPLACEMENT CROSS-SECTION

PAVEMENT SUBGRADE UNDERCUT NOTES:

1. Areas of pavement subgrade that do not pass a proof roll inspection shall be undercut when directed by the Material Testing Engineer and/or Project Engineer. All undercut work shall be witnessed and field measured by the Material Testing Engineer and/or Project Engineer.

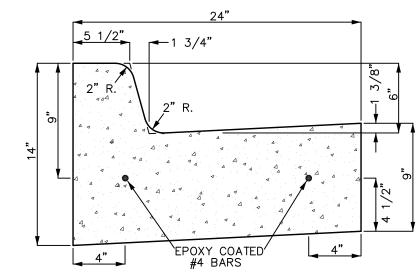
2. Undercut areas shall be excavated to a depth of 12" below the proposed subgrade elevation using an Excavator or Backhoe with a Smooth Edged Ditching Bucket so as not to scarify the underlying soils. Undercut areas shall remain free of all construction traffic and equipment to avoid rutting and/or tracking of the underlying soils.

3. Mirafi HP 570 Woven Geotextile Fabric (or approved equal) shall be placed over all undercut areas per the Manufacturer's specifications. Overlap all seams a minimum of 12" unless specified otherwise by the Manufacturer.

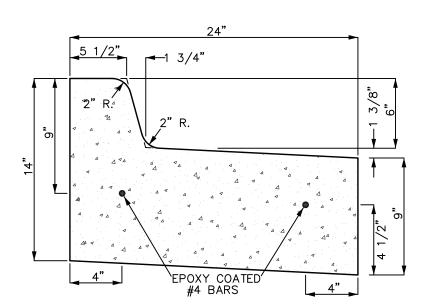
4. Backfill the undercut areas with 1" x 3" minimum size crushed angular limestone up to the proposed subgrade elevation. Crushed concrete material shall NOT be substituted for crushed limestone material. The backfill material shall be spread with a Wide Track Dozer to minimize loading on the underlying soils. Static roll the backfill material with a large smooth drum roller.

5. Construct the appropriate Bituminous or Concrete Pavement Cross Section over the undercut areas per the Project Plans.

6. The General Contractor and/or Earthwork Subcontractor shall provide ALDI Inc with unit pricing to perform subgrade undercut work per square yard (SY) of undercut area. Undercut Unit Pricing SHALL include excavation, loading, hauling and offsite disposal of excess spoils, placement of geotextile fabric and backfill including all labor, equipment and materials necessary to complete pavement subgrade undercut work as specified on the Project Plans.



MDOT TYPE F4 CURB



MDOT TYPE F4 CURB REVERSE PITCH

NOT TO SCALE

CONCRETE CURB NOTES

1. Refer to the project plans for the proposed locations of the specific curb

2. The construction specifications of the appropriate Local Municipality are a part of this work. Refer to the General Notes and Curb Cross Section Details on the project plans for additional requirements.

3. Extend the base and/or subbase material of the appropriate adjacent pavement cross-section horizontally to 1 foot behind the back of curb. Concrete curb shall be constructed on no less than 6" of combined depth of compacted base/subbase material.

4. Concrete material shall meet or exceed the specification requirements of the appropriate Local Municipality. Unless specified otherwise by the Local Municipality, concrete material shall be air-entrained and shall have a minimum 28-day class design strength of 3500 psi. Contractor shall submit concrete mix design and aggregate mechanical analysis report to the Local Municipality and Engineer for review and approval prior to use.

5. Install transverse contraction control joints in concrete curb with 1" minimum depth at 10' on center. Tool joints in fresh concrete or saw cut within 8 hours.

6. Install transverse expansion control joints in concrete curb as follows: 400' maximum on center, at spring points of intersecting streets and within 10' on each side of catch basins. Transverse expansion control joints shall be 1" thick asphalt fiber joint filler matching entire curb cross section.

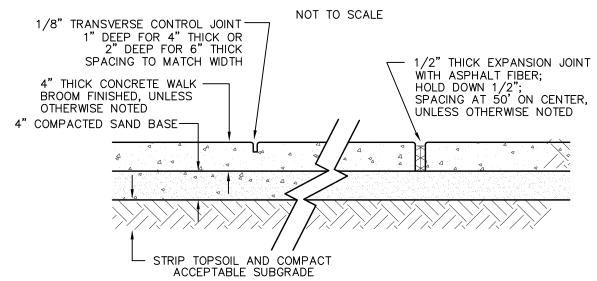
7. Provide 0.5" asphalt fiber control joint between back of curb and all other concrete structures, such as concrete sidewalks and concrete driveways.

8. Curb Contractor shall provide final adjustment of catch basin castings in curb line. Castings shall be tuck pointed to structure water tight with concrete or mortar inside and outside of casting.

9. Install curb cuts for all existing and proposed sidewalks and pedestrian ramps in accordance with the American Disabilities Act and the Barrier Free Design requirements of the appropriate Local, County and/or State Agency. Refer to MDOT Standard Plan R-28, latest revision. Install curb cuts for all existing and proposed vehicular ramps and drives as noted on the project plans.

CONC. SIDEWALK SLOPE WALK AWAY FROM BUILDING AT 2.0% (MAX.) 6" MIN 1" ASPHALT FIBER ISOLATION JOINT 6" (TYP.) (HEIGHT MAY VARY, SEE GRADING PLAN) COMPACTED SAND BASE COMPACTED SUBGRADE

SIDEWALK WITH INTERGAL CURB & ISOLATION JOINT DETAIL



SIDEWALK CROSS SECTION NOT TO SCALE

SIDEWALK CROSS SECTION NOTES:

1. The construction specifications of the Local Municipality are a part of this work. Refer to the General Notes and the Sidewalk Cross Section Details on the Project Plans for additional requirements.

2. Sidewalk widths may vary. See the Project Plans for the proposed sidewalk width at each location. Increase sidewalks to 6" minimum thickness at driveways and other areas exposed to vehicular traffic.

3. The existing subgrade soils shall be prepared prior to placement of the granular subbase. Unsuitable soils found within the 1 on 1 influence zone of the proposed sidewalk areas, such as muck, peat, topsoil, marl, silt or other unstable materials shall be excavated and replaced with structural fill. Structural fill shall be MDOT Class II granular material placed in accordance with the General Notes on the Project Plans.

4. The sidewalk compacted subbase material shall be MDOT CL II sand. No subbase material substitutions shall be permitted without prior written approval of the Project Engineer and receipt of the Owner's Authorization. The subbase shall be compacted to a minimum of 95% of the maximum unit weight, modified

5. Concrete material shall meet or exceed the specification requirements of the appropriate Local Municipality. Unless specified otherwise by the Local Municipality, concrete material shall be air-entrained and shall have a minimum 28-day class design strength of 3500 psi. Contractor shall submit concrete mix design and aggregate mechanical analysis report to the Local Municipality and Engineer for review and approval prior to use.

6. Install transverse contraction control joints in accordance with the Sidewalk Cross Section Detail. Space contraction control joints to match sidewalk width, but no greater than 10' on center. Tool joints in fresh concrete or saw cut within 8 hours.

7. Install transverse expansion control joints in accordance with the Sidewalk Cross Section Detail. Space expansion control joints at 50 feet on center maximum. Transverse expansion control joints shall be 1/2" thick asphalt fiber joint filler matching entire sidewalk cross section.

8. Provide 0.5" asphalt fiber control joint between concrete sidewalks and all other concrete structures, such as concrete building foundations, concrete curb and concrete driveways.

9. Construct all Barrier Free Sidewalk Ramps in accordance with the American Disabilities Act and the Barrier Free Design Requirements of the appropriate Local, County or State Agency with jurisdiction over the project. Refer to MDOT Standard Plan R-28, latest revision.

10. The Concrete Pavement shall not be exposed to vehicular traffic until the concrete has reached at least 75 % of the design flexural strength.

GENERAL NOTES:

1. Contractor shall perform the work in accordance with the requirements of the appropriate Local, County and State Agencies and all other Government and Regulatory Agencies with jurisdiction over the project. Contractor shall notify the appropriate Agencies in advance of each stage of work in accordance with each Agency's requirements.

2. Contractor shall comply with all permit, insurance, licensing and inspection requirements associated with the work. Prior to construction, Contractor and Owner/Developer shall determine who is responsible for obtaining each required permit. Contractor shall verify that the each required permit has been obtained prior to commencement of the stage of work associated with the required permit(s).

3. Contractor shall furnish liability insurance and property damage insurance to save harmless the Owner, Developer, Architect, Engineer, Surveyor and Government Agencies for any accident occurring during the construction period. Refer to the appropriate Local, County and State Agencies for additional requirements. Copies of insurance certifications shall be made available to the Owner/Developer.

4. Contractor shall conduct and perform work in a safe and competent manner. Contractor shall perform all necessary measures to provide for traffic and pedestrian safety from the start of work and through substantial completion. Contractor shall determine procedures and provide safety equipment such as traffic controls, warning devices, temporary pavement markings and signs as needed. Contractor shall comply with the safety standards of the State Department of Labor, the occupational health standards of the State Department of Health and safety regulations of the appropriate Local, County, State and Federal Agencies. Refer to the safety specifications of the appropriate Regulatory Agencies. The Contractor shall designate a qualified employee with complete job site authority over the work and safety precautions; said designated employee shall be on site at all times during the work.

5. Contractor shall coordinate scheduling of all work in the proper sequence, including work by Subcontractors. Additional costs due to improper planning by Contractor or work done out of sequence as determined by standard acceptable construction practices, shall be Contractor's responsibility.

6. Contractor shall contact the 811 Public Underground Utility Locating System or other appropriate local underground utility locating Agency, a minimum of three (3) working days prior to construction. Existing utility information on the project plans may be from information disclosed to this firm by the Utility Companies, Local, County or State Agencies, and/or various other sources. No guarantee is given as to the completeness or accuracy thereof. Prior to construction, locations and depths of all existing utilities (in possible conflict with the proposed improvements) shall be verified in the field.

7. Contractor shall coordinate scheduling a Pre-Construction Meeting with Engineer prior to commencement of work.

8. The Local Municipality, County and/or State in which the project is located may require an Engineer's Certification of construction of the proposed site improvements. Contractor shall verify the certification requirements with Engineer prior to commencement of work. Contractor shall coordinate construction staking, testing, documentation submittal and observation with the appropriate Agency, Surveyor and/or Engineer as required for Engineer's Certification and Government Agency Acceptance. All materials used and work done shall meet or exceed the requirements of certification and acceptance, the contract documents and the material specifications noted on the project plans. Any materials used or work done that does not meet said requirements, contract documents and/or specifications shall be replaced and/or redone at Contractor's expense. The Owner/Developer may wait for test results, certifications and/or Agency reviews prior to accepting work.

9. Engineer may provide subsurface soil evaluation results, if available, to Contractor upon request. Subsurface soil evaluation results, soils maps and/or any other documentation does NOT guarantee existing soil conditions or that sufficient, acceptable on-site granular material is available for use as structural fill, pipe bedding, pipe backfill, road subbase or use as any other granular material specified on the project plans. On-site granular material that meets or exceeds the material specifications noted on the project plans may be used as structural fill, pipe bedding, pipe backfill and/or road subbase material. On-site granular material shall be stockpiled and tested as acceptable to the appropriate Agency and/or Engineer prior to use.

10. During the performance of their work, Contractor shall be solely responsible for determining soil conditions and appropriate construction methods based on the actual field conditions. Contractor shall furnish, install and maintain sheeting, shoring, bracing and/or other tools and equipment and/or construction techniques as needed for the safety and protection of the workers, pedestrians and vehicular traffic and for protection of adjacent structures and site improvements.

11. Contractor shall install temporary and permanent soil erosion and sedimentation control devices at the appropriate stages of construction in accordance with the appropriate regulatory Agencies.

12. Structural fill shall be placed as specified on the project plans and within the 1 on 1 influence zone of all structures, paved areas and other areas subject to vehicular traffic. Structural fill shall be placed using the controlled density method (12" maximum lifts, compacted to 95% maximum unit weight, modified proctor). Fill material shall meet or exceed the specifications noted on the project plans or as directed by Engineer when not specified on the project plans.

13. All existing monuments, property corners, ground control and benchmarks shall be protected and preserved; and if disturbed by Contractor, shall be restored at Contractor's expense. Contractor shall notify Surveyor of any conflicts between existing monuments, property corners, ground control and/or benchmarks and the proposed site improvements.

14. Contractor shall notify Owner/Developer and Engineer immediately upon encountering any field conditions, which are inconsistent with the project plans and/or specifications.

15. When noted on the project plans for demolition and/or removal, Contractor shall remove existing structures, building and debris and recycle and/or dispose of in accordance with Local, County, State and Federal regulations.

16. Contractor shall remove excess construction materials and debris from site and perform restoration in accordance with the project plans and specifications. Disposing of excess materials and debris shall be performed in accordance with Local, County, State and Federal regulations.

17. Construction access to the site shall be located as acceptable to the Owner/Developer and to the appropriate Local, County and/or State Agency with jurisdiction over the road(s) providing access to the site. Construction access shall be maintained and cleaned in accordance with the appropriate Local, County and/or State Agencies and as directed by Owner/Developer and/or Engineer.

18. Contractor shall take necessary precautions to protect all site improvements from heavy equipment and construction procedures. Damage resulting from Contractor actions shall be repaired at Contractor's expense.



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CIVIL ENGINEERS
LAND SURVEYORS
2183 PLESS DRIVE
BRIGHTON, MICHIGAN 48114

DESIGN: FAF

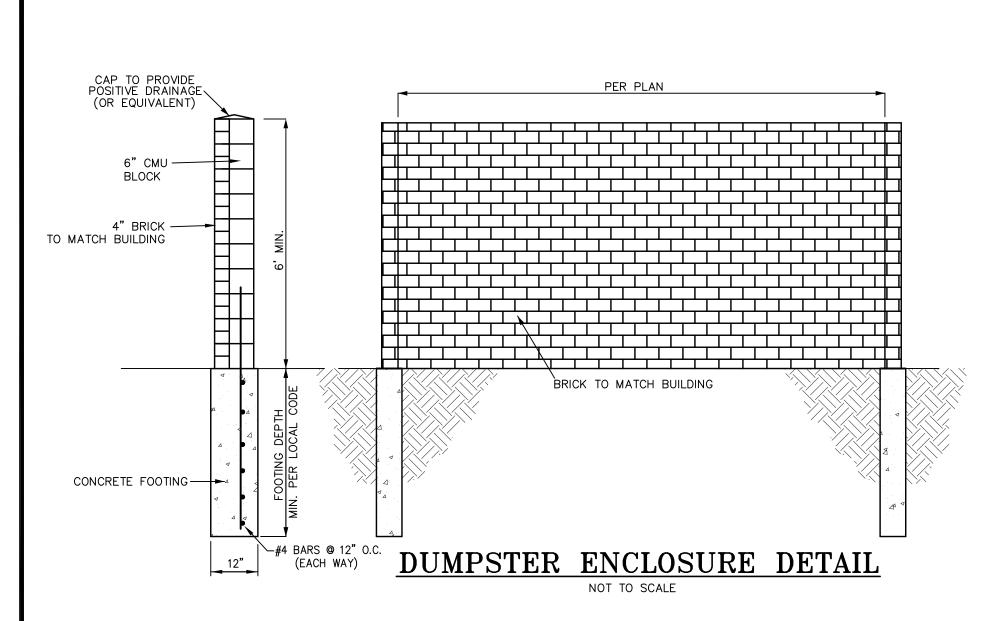
DRAFT: JHG
CHECK: WMP

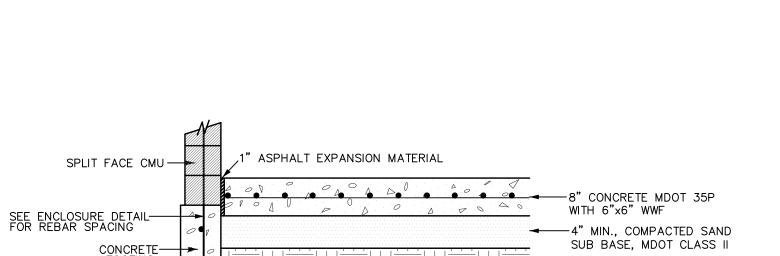
BMH - PREMIER
P.U.D.

SITE PAVEMENT NOTES & DETAILS CLIENT:
PREMIER GENOA LLC
775 N. SECOND STREET
BRIGHTON, MICHIGAN 48116

SCALE: NO SCALE
PROJECT No.: 183450
DWG NAME: 3450 DT

DT1



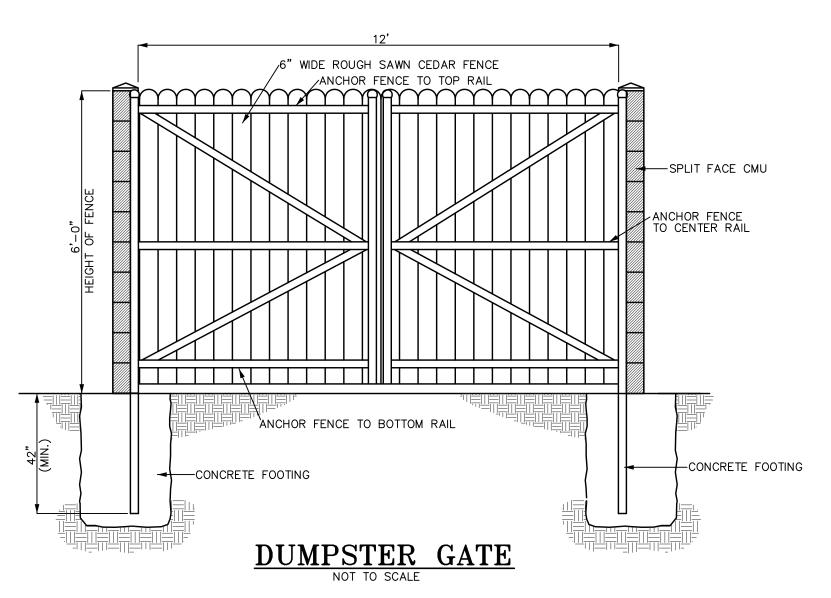


DUMPSTER PAD SLAB CROSS-SECTION

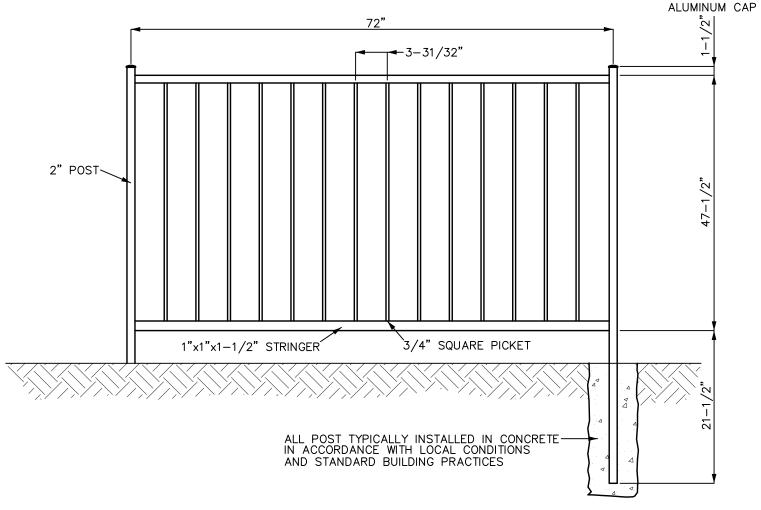
PAVEMENT NOTES:

- 1. UNSUITABLE SOILS, SUCH AS MUCK, PEAT, TOPSOIL, MARL, SILT OR OTHER UNSTABLE MATERIALS, SHALL BE UNDERCUT AND REPLACED
- OR OTHER UNSTABLE MATERIALS, SHALL BE UNDERCUT AND REPLACE WITH COMPACTED SAND SUBGRADE FILL. WHERE INCIDENTAL
- TO ROUGH GRADING.

 2. AREAS OF SUBGRADE FILL SHALL BE CONSTRUCTED USING 12" THICK LIFTS OF COMPACTED SAND, MDOT CLASS III OR EQUIVALENT ON—SITE MATERIAL; WHEN INSIDE ROAD INFLUENCE ZONE.

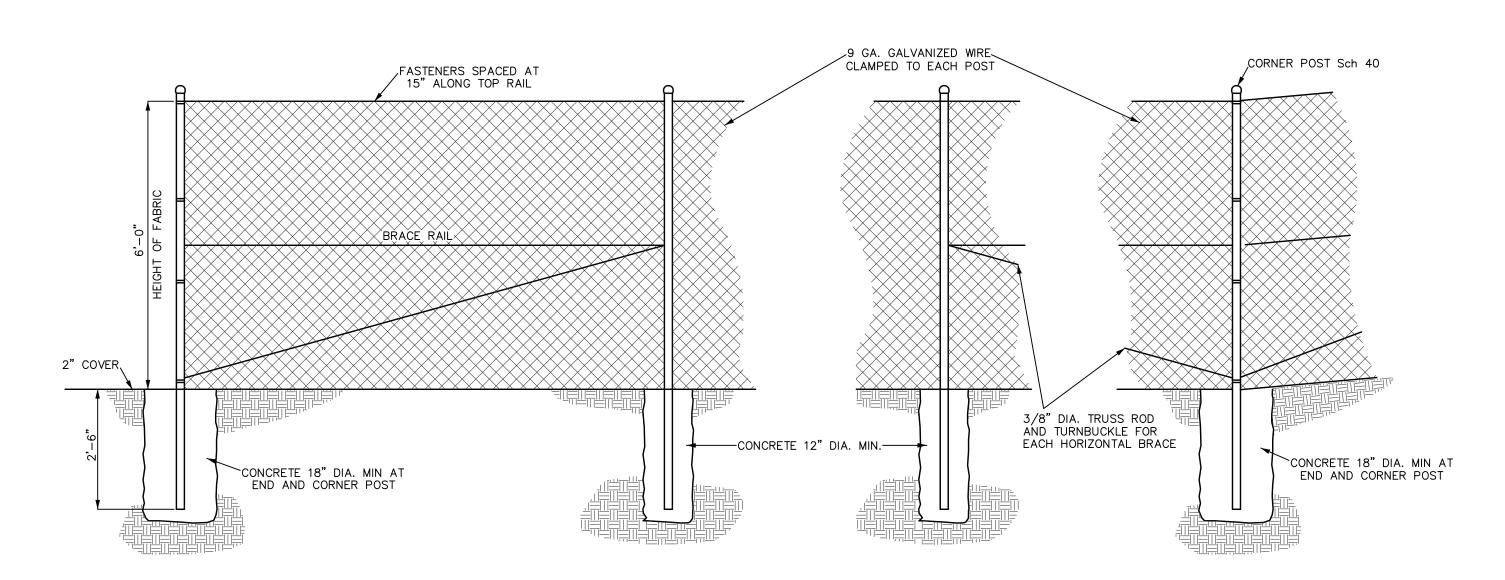


NOTE: ALL POLES, RAILS AND HARDWARE SHALL BE GALVANIZED STEEL.



ORNAMENTAL FENCE DETAIL

NOTE: MATERIAL SHALL BE ALUMINUM WTIH BACK FINISH.



→ EXISTING ACCEPTABLE SUBGRADE

CHAIN LINK FENCE DETAIL

NOT TO SCALE

NOTE: ALL FENCING AND HARDWARE SHALL BE

REVISION # DATE REVISION-DESCRIPTION REVISION # DATE REVISION-DESCRIPTION

DESIGN: FAF

DRAFT: JHG

CHECK: WMP

GALVANIZED STEEL

BMH - PREMIER
P.U.D.

SIGNAGE &
PAVEMENT MARKING
NOTES & DETAILS

CLIENT:

PREMIER GENOA LLC

775 N. SECOND STREET

WEBBERVILLE, MICHIGAN 48892

(517) 521-3907

SCALE: AS NOTED
PROJECT No.: 9183450
DWG NAME: 3450 DT

ISSUED: FEB. 14, 2020

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bollards and embed the bollard into the concrete footing a minimum of 3 feet below proposed finish grade. 6. Signs shall not be mounted on posts until after concrete has cured for a minimum of seven days or 3/4 strength is achieved.

cutting, drilling and/or other pole modifications shall be painted with galvanizing paint. All mounting hardware shall be stainless steel.

8. All Fire Lane signs shall have a Red Border and Red Letters on White Background. All Fire Lane signs shall be Reflective. Fire lane striping and signage shall meet the requirements of the Local Building Inspector and Fire Department.

1. Pavement markings shall be in accordance with the type, color, size and locations shown on the plans. If the information on the plans is not complete and the authority having jurisdiction does not have specific requirements, then use the following: Paint shall be supplied in accordance with AASHTO: M 248 latest addition.

Colors shall be as follows: (YELLOW- parking stalls, loading zones, parking islands, no parking zones and fire lanes) (WHITE - stop bars, pedestrian crossings, lane

demarcations, directional arrows and lettering) (BLUE - handicap parking stalls and symbols). Stripe widths shall be as follows: (4" - parking spaces, driveway lanes,

2. The pavement shall be clean and free of dirt, dust, moisture, oils and other foreign materials at time of marking application. Any old pavement markings shall be removed unless paints are compatible and overlay identically. The surface of the pavement prior to application shall be a minimum of 45 degrees F and rising unless

3. The signage shall be in accordance with the type, color, size and locations shown on the plans in accordance with AASHTO M268. The signage shall be provided

4. Posts, brackets and frames shall be steel per ASTM A-36, A-242, A-441, A-572, A588, Grade 50 and hot dip galvanized in accordance with ASTM A123. All

5. Sign post footings shall be a minimum of 3'-6" deep and 8" in diameter unless poor soils or frost conditions require greater depth and/or diameter. Sign posts shall be kept plumb, 6 inches off the bottom of footing excavation and centered as 3000-psi concrete is placed under and around the sign post. The overall sign and post system should be able to withstand 33 pounds per square foot. All signs located in paved areas or with less than 3 feet of clearance between the centerline of sign post and the back of curb and/or edge of pavement shall be installed in a pipe bollard. Pipe bollards shall be 6" diameter schedule 40 steel pipe. Pipe bollards shall be filled with concrete that is rounded at the top of the bollard. Bollards shall be painted traffic yellow. Increase the sign post footing diameter to 14" minimum for pipe

9. "Mounting Height" shall be the minimum height of the bottom of the sign above finish grade. When signs are located downhill from the roadway, driveway and/or parking area, then the "Mounting Height" shall be the height of the bottom of the sign above the top of pavement finish grade at the nearest edge of pavement adjacent to the sign.

10. All Traffic Control and Fire Lane signs shall be installed at 3 feet behind the back of curb (and/or edge of pavement) to the centerline of the sign post unless noted otherwise on the project plans.

11. The Contractor(s) and/or Subcontractor(s) responsible for installation of the sign posts shall contact the 811 Public Underground Utility Locating System a minimum of three (3) working days prior to installation of the signposts. Install the sign posts in the locations specified on the project plans. When underground utilities conflict with the proposed sign post locations, field adjust the sign locations the minimum amount necessary to safely clear the underground utilities. Maintain a minimum of 2 feet of clearance between the edge of sign and the back of curb and/or edge of sidewalk.



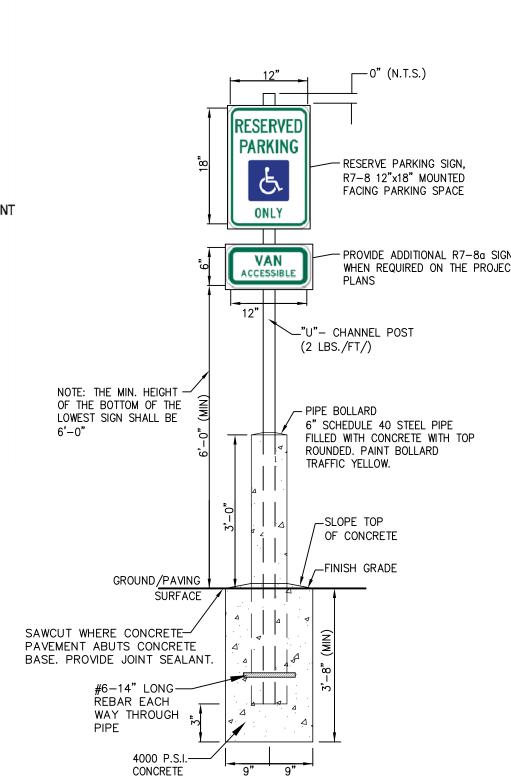
SIGNAGE AND PAVEMENT MARKING NOTES:

the Manufacturer's recommendations are greater.

barrier free loading zones and no parking zones) (12" - crosswalks) (24" - stop bars).

in accordance with the Local Municipality and the Michigan Manual of Uniform Traffic Devices latest edition.

7. All barrier free striping and signage shall meet the Americans with Disabilities Act (ADA) requirements.

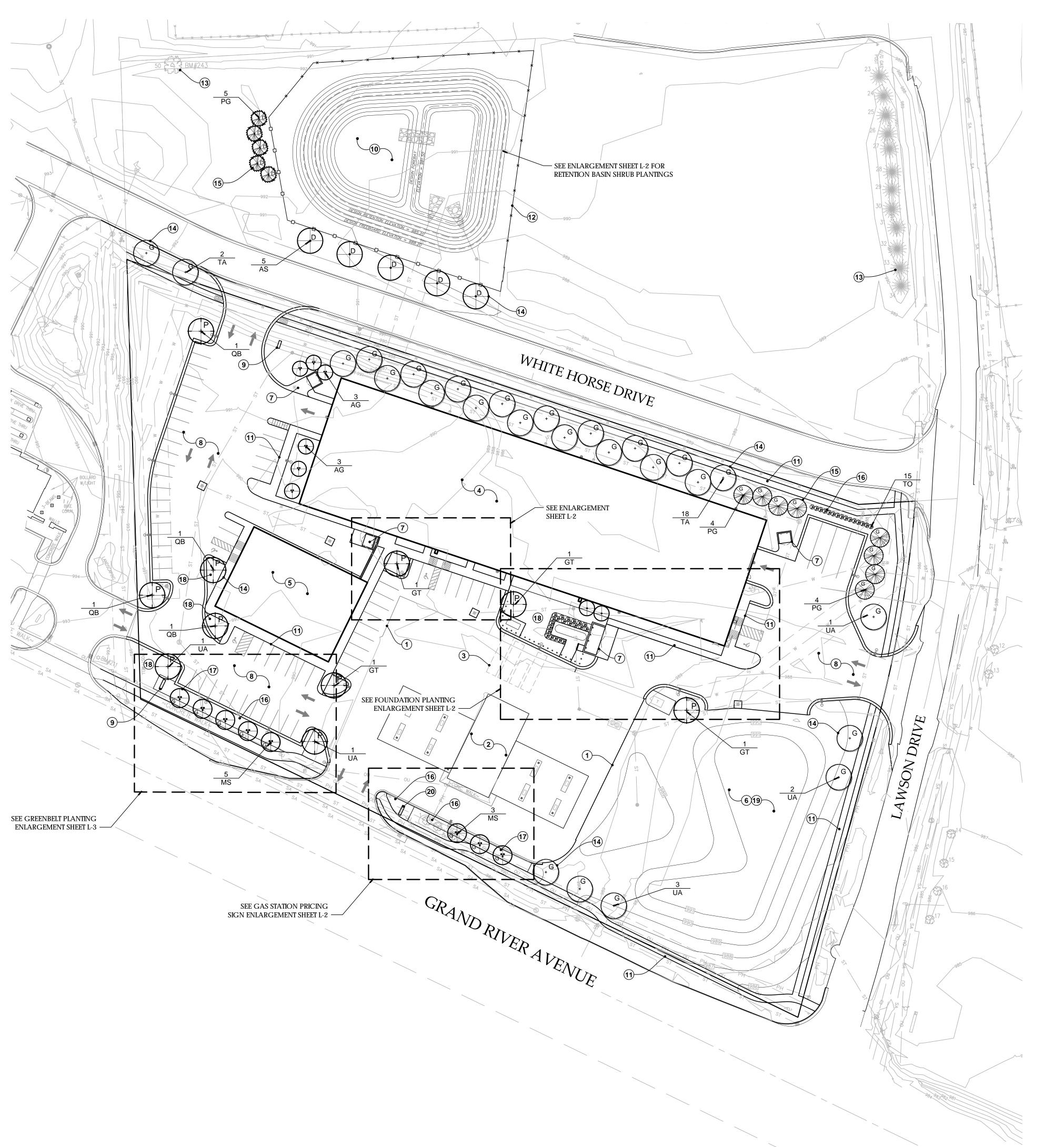


BARRIER FREE PARKING

SIGN DETAIL

SIGN SCHEDULE

	SIGIT OF ILEGAL										
SIGN	KEY	SIZE (W x H)	TYPE OR MOUNT	MOUNTING HEIGHT	QUANTITY						
RESERVED PARKING	R7-8	12" x 18"	POST MOUNTED	7'-6"	5						
ONLY	n/-0	12 X 10	BUILDING MOUNTED	7'-6"	0						
VAN	R7-8a	12" x 6"	POST MOUNTED	7'-0"	4						
NO PARKING FIRE LANE	FIRE LANE	12" x 18"	POST MOUNTED	6'-0"	6						





Project: BMH-Premier PUD Company: Vert Verde Landscape Architecture, LLC

Owner: Premier Genoa, LLC

Location: Genoa Township, Michigan Address: 4525 East Grand River Ave.

Existing Zoning: Varies

Proposed Zoning: RD-PUD

DATA	EXISTING ZONING	PROPOSED ZONING		
DATA	GCD	RD-PUD		
PARKING LOT LANDSCAPE				
	Required	Provided	Deviation	
1 Deciduous Canopy Tree and				
100 sf. of landscape area / 10	8 Trees and 740 sf.	9 Trees and 2,935 sf.	Providing 1 additional tree	
spaces required - 74 spaces	landscape area	landscape area	Froviding Ladditional tree	
GREENBELT LANDSCAPE				
1 Deciduous Canopy Tree / 40			Proposing (3) canopy and (8	
If. of lot frontage			ornamental trees	
Grand River Ave - 578 lf.	15	11	ornamental trees	
			Proposing (3) canopy and (4	
Lawson Drive - 420 If	11	7	evergreen trees	
			Providing 8 additional trees	
White Horse Drive - 610 If	16	24	From the continue trees	
RETENTION BASIN LANDSCAPE				
1 Tree and 10 shrubs / 50 lf. of	10 Trees and 98	10 Trees and 98		
retention basin - 491 lf.	shrubs	shrubs		
retention basin - 491 II.	Siliuos	SHIUDS		
OPEN SPACE REQUIREMENTS				

55,758 sf.

59,077 sf. (26.5%)

TREE PLANTING KEY:

D - REQUIRED DETENTION BASIN TREE

P - REQUIRED PARKING LOT TREE G - REQUIRED GREENBELT TREE

Minimum of 25% of the site

Site Area: 223,031 sf. (5.12 ac)

shall be open space

Note Key:

1 LIMITS OF EXISTING PARKING LOT

2 EXISTING GAS STATION

(3) EXISTING UNDERGROUND TANKS (4) PROPOSED STORAGE FACILITY, SEE ARCHITECTURE

5 PROPOSED OFFICE / RETAIL SPACE, SEE ARCHITECTURE

6 FUTURE BUILD-OUT

7) PROPOSED DUMPSTER ENCLOSURE

8 PROPOSED PARKING LOT, SEE CIVIL ENGINEERING DRAWINGS

9 PROPOSED MONUMENT SIGN

PROPOSED STORM WATER BASIN, SEE CIVIL ENGINEERING DRAWINGS

(11) PROPOSED CONCRETE SIDEWALK, TYPICAL

(12) PROPOSED FENCE WITH GATE

(13) EXISTING TREES, TO REMAIN

DECIDUOUS CANOPY TREE, SEE SHEET L-3 FOR PLANT SCHEDULE AND TYPICAL DETAIL

(15) EVERGREEN TREE, SEE SHEET L-3 FOR PLANT SCHEDULE AND TYPICAL DETAIL

SHRUB AND PERENNIAL PLANTING, SEE SHEET L-3 FOR PLANT SCHEDULE AND TYPICAL DETAIL

ORNAMENTAL FLOWERING TREE, SEE SHEET L-3 FOR PLANT SCHEDULE AND TYPICAL DETAIL

18) SODDED LAWN

19 SEEDED LAWN

20) EXISTING GAS STATION PRICING SIGN

Maintenance Notes

LANDSCAPING REQUIRED BY THIS ORDINANCE SHALL BE MAINTAINED IN A HEALTHY, NEAT AND ORDERLY APPEARANCE, FREE FROM REFUSE AND DEBRIS. ALL UNHEALTHY AND DEAD PLANT MATERIAL SHALL BE REPLACED IMMEDIATELY, UNLESS THE SEASON IS NOT APPROPRIATE FOR PLANTING, IN WHICH CASE SUCH PLANT MATERIAL SHALL BE REPLACED AT THE BEGINNING OF THE NEXT PLANTING SEASON. THE OWNER SHALL INSURE PERPETUAL AND MANDATORY MAINTENANCE AND/OR REPLACEMENT OF VEGETATIVE PLANTINGS PURSUANT TO THE APPROVED LANDSCAPE PLAN.

ALL LANDSCAPED AREAS WILL BE IRRIGATED WITH AN AUTOMATIC IRRIGATION SYSTEM.

Scale:

Project Number: 18.033

Sheet Number:

landscape

architecture 734.249.3568 Plymouth, MI james@vertverde.com

Owner Review

Revision

Pre-submittal Review

12.27.2019 Conceptual PUD Submittal

BMH - Premier PUD

4525 East Grand River Ave.

Genoa Township, MI

Premier Genoa, LLC

775 N. Second Street

Brighton, MI 48116

Project Sponsor:

Sheet Name:

Preliminary Landscape Plan

12.20.2018

01.03.2019 01.08.2019

02.14.2020

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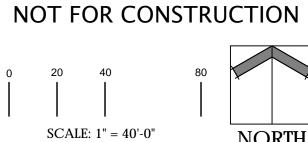
JG JG

12.10.2018 1" = 40'-0"

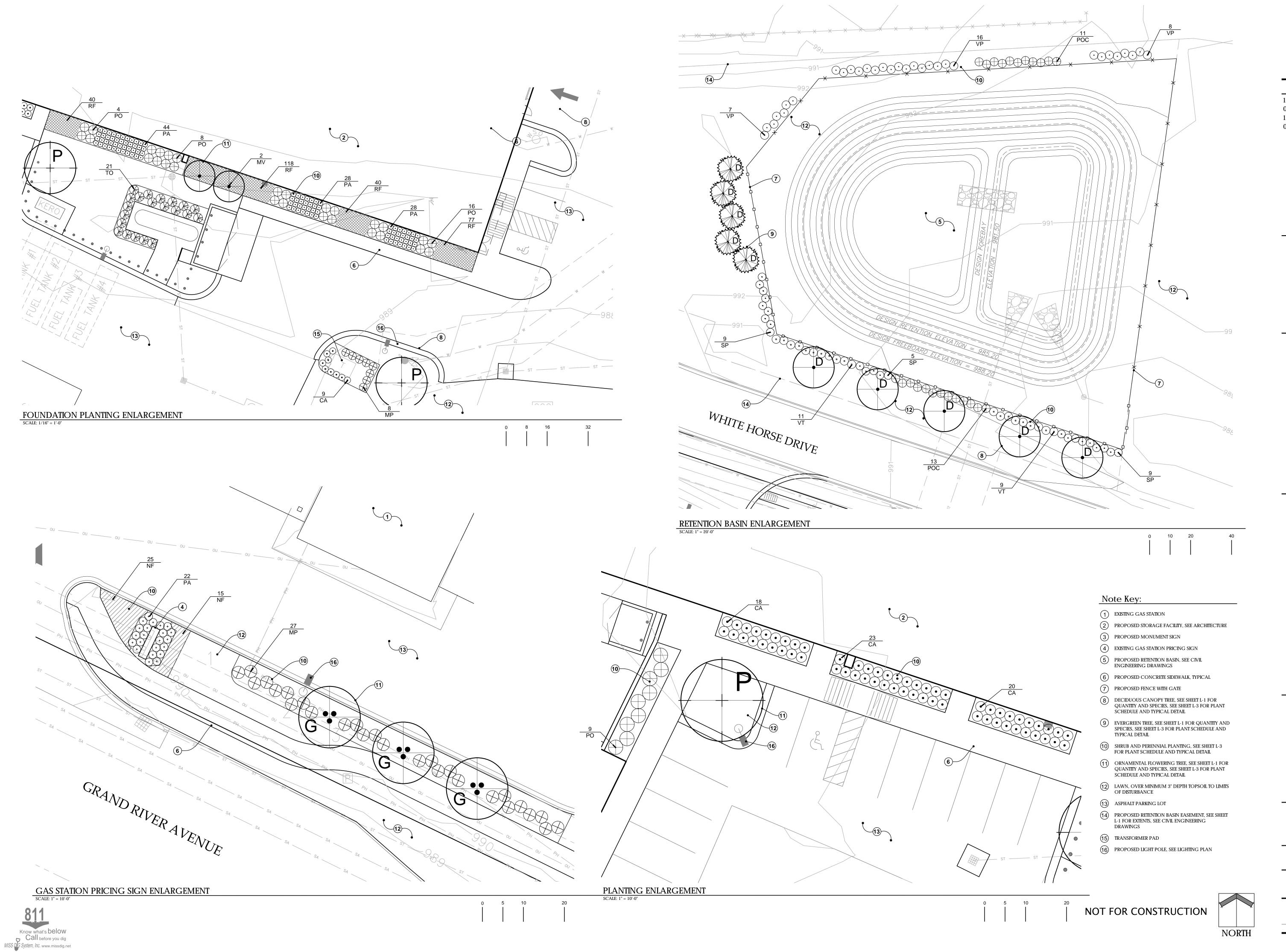
SURVEY PROVIDIED BY:

DESINE, Inc. 2183 Pless Drive Brighton, MI 48114 810.227.9533

DESINE JOB NUMBER: 183450 DATED: FEBRUARY 27, 2019









12.20.2018 Owner Review
01.08.2019 Pre-submittal Review
12.27.2019 Conceptual PUD Submittal
02.14.2020 Revision

Proje

BMH – Premier PUD 4525 East Grand River Ave. Genoa Township, MI

Project Sponsor:

Premier Genoa, LLC 775 N. Second Street Brighton, MI 48116

Sheet Name:

Enlargements

Drawn: JG

Scale: AS NOTED

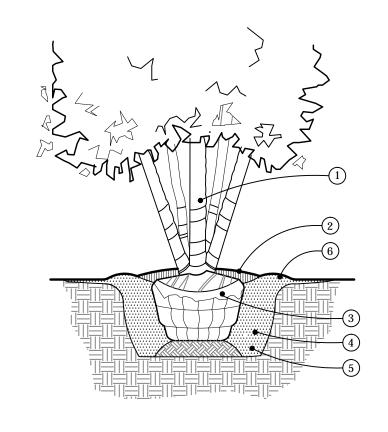
Project Number:

12.10.2018

18.033
Sheet Number:

L-2

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MULTISTEM TREE PLANTING

4 \ EVERGREEN TREE PLANTING

NOT TO SCALE

NOT TO SCALE

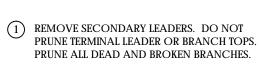
1. CONTRACTOR TO VERIFY PERCOLATION OF 2. SET TOP OF ROOTBALL 3" ABOVE FINISH GRADE

- REMOVE ALL TAGS, STRINGS, PLASTICS, AND ANY OTHER MATERIALS WHICH ARE UNSIGHTLY OR COULD CAUSE GIRDLING
- (2) COVER PLANTING W/ 3" DOUBLE SHREDDED HARDWOOD BARK MULCH. MINIMUM 6' DIA. LEAVE 3" CIRCLE OF BARE SOIL AROUND BASE OF THE STEMS.
- (3) REMOVE ALL NON-BIODEGRADABLE MATERIALS FROM THE ROOTBALL. FOLD DOWN ALL BURLAP AND REMOVE WIRE BASKET FROM THE TOP 1/3 OF THE
- 4 PLANT MIX. SEE GENERAL LANDSCAPE NOTES SHEET L-3 AND SPECIFICATIONS
- (5) TREE PIT TO BE THREE TIMES WIDTH OF
- (6) 4" TOPSOIL SAUCER

- 1) REMOVE ALL TAGS, STRINGS, PLASTICS, AND ANY OTHER MATERIALS WHICH ARE UNSIGHTLY OR COULD CAUSE GIRDLING. 2 STAKE TREES WITH 2-3" WIDE BELT-LIKE, FABRIC STRAPS ONLY, ARBOR TIE OR APPROVED EQUAL, (CONNECT FROM TREE TO STAKE OPPOSITE FROM EACH OTHER, AND ALLOW FOR SOME "FLEXING")
 - DO NOT USE WIRE OR ROPE THROUGH A HOSE. REMOVE AFTER ONE YEAR. (3) 2"X2" HARDWOOD STAKES OR EQUIVALENT DRIVEN 6-8" OUTSIDE OF ROOTBALL. REMOVE AFTER ONE YEAR.
 - (4) COVER PLANTING W/ 3" SHREDDED HARDWOOD BARK MULCH. MINIMUM 6' DIAMETER, CONNECT EVERGREEN
 - (5) REMOVE ALL NON-BIODEGRADABLE MATERIALS FROM THE ROOTBALL. FOLD DOWN ALL BURLAP AND REMOVE WIRE BASKET FROM THE TOP 1/3 OF THE ROOTBALL.
 - (6) PLANT MIX. SEE GENERAL LANDSCAPE NOTES SHEET L-3 AND SPECIFICATIONS
 - (7) TREE PIT TO BE 3 TIMES WIDTH OF
 - 8 4" TOPSOIL SAUCER

1. STAKE EVERGREENS UNDER 12' HEIGHT 2. GUY EVERGREENS 12' HEIGHT AND

- 3. NEVER CUT OR PRUNE CENTRAL LEADER 4. SET STAKES VERTICAL AND EVENLY SPACED
- 5. PRUNE ONLY TO REMOVE DEAD OR BROKEN BRANCHES



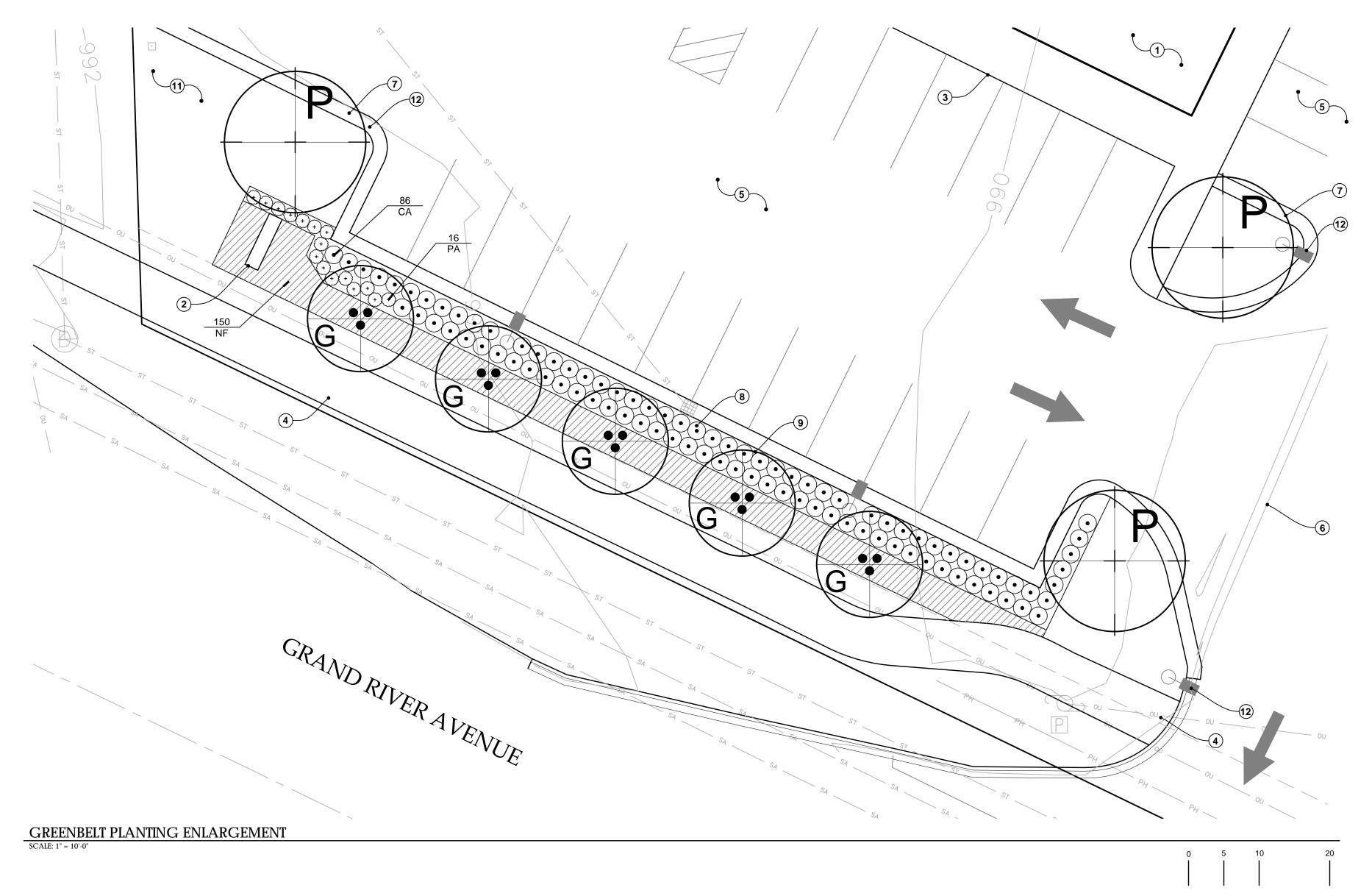
- (2) REMOVE ALL TAGS, STRINGS, PLASTICS, AND ANY OTHER MATERIALS WHICH ARE UNSIGHTLY OR COULD CAUSE GIRDLING.
- (3) STAKE TREES JUST BELOW FIRST BRANCH W/ 2-3" WIDE BELT-LIKE, FABRIC STRAPS ONLY, ARBOR TIE OR APPROVED EQUAL, (CONNECT FROM TREE TO STAKE OPPOSITE FROM EACH OTHER, AND ALLOW FOR SOME "FLEXING") DO NOT USE WIRE OR ROPE THROUGH A HOSE. REMOVE AFTER ONE YEAR.
- (4) (2) 2"X2" HARDWOOD STAKES OR EQUIVALENT DRIVEN 6"-8" OUTSIDE OF ROOTBALL. REMOVE AFTER ONE YEAR.
- (5) COVER PLANTING W/ 3" DOUBLE SHREDDED HARDWOOD BARK MULCH. MINIMUM 6' DIA. LEAVE 3" CIRCLE OF BARE SOIL AROUND THE BASE OF THE TRUNK.
- (6) REMOVE ALL NON-BIODEGRADABLE MATERIALS FROM THE ROOTBALL. FOLD DOWN ALL BURLAP AND REMOVE WIRE BASKET FROM THE TOP 1/3 OF THE ROOTBALL.
- 7) PLANT MIX. SEE GENERAL LANDSCAPE NOTES SHEET L-3 AND SPECIFICATIONS
- TREE PIT TO BE 3 TIMES WIDTH OF ROOTBALL 9) 4" TOPSOIL SAUCER
- TREE SHALL BEAR SAME RELATION TO FINISH GRADE AS IT BORE ORIGINALLY OR SLIGHTLY HIGHER THAN FINISH GRADE UP TO 4" ABOVE GRADE, IF DIRECTED BY LANDSCAPE ARCHITECT FOR HEAVY CLAY SOIL AREAS.

1. STAKE TREES 3" - 5" CALIPER ONLY 2. GUY TREES 6" CALIPER AND OVER 3. CONTRACTOR TO VERIFY PERCOLATION OF

PLANTING PIT PRIOR TO INSTALLATION

4. SET STAKES VERTICAL AND EVENLY SPACED

5. STAYS OR GUYS TO BE SET ABOVE FIRST BRANCH



NOTES:
1. CONTRACTOR TO VERIFY PERCOLATION OF PLANTING PIT PRIOR TO INSTALLATION 2. SET TOP OF ROOTBALL 2" ABOVE FINISH GRADE

- 1 SEE PLAN FOR SPACING
- 2 SHRUBS, SEE PLANT SCHEDULE
- 3 3" DEPTH DOUBLE SHREDDED HARDWOOD MULCH, TYPICAL
- 4 REMOVE ALL NON-BIODEGRADALBE TWINE FROM ENTIRE ROOTBALL. REMOVE BURLAP FROM TOP 1/2 OF ROOTBALL.
- 5) SHOVEL CUT OR METAL EDGE, SEE PLAN (6) EXCAVATE EXISTING SOIL TO 12" DEPTH, REPLACE WITH PLANT MIX. SEE GENERAL
- SPECIFICATIONS (7) SCARIFY TO 4" DEPTH AND RECOMPACT

LANDSCAPE NOTES SHEET L-3 AND

- UNDISTURBED SUBGRADE
- 9 MINIMUM 8" BETWEEN ROOTBALL AND EDGE OF PLANTING PIT

SHRUB PLANTING NOT TO SCALE

Note Key:

- PROPOSED RETAIL SPACE, SEE SHEET L-1, SEE ARCHITECTURE
- (2) PROPOSED MONUMENT SIGN (3) PROPOSED CONCRETE SIDEWALK
- (4) EXISTING SIDEWALK TO REMAIN
- (6) LIMITS OF EXISTING ASPHALT PARKING LOT
- (5) PROPOSED ASPHALT PARKING LOT

(8) SHRUB AND PERENNIAL PLANTING

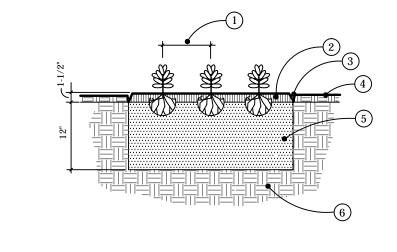
9 ORNAMENTAL FLOWERING TREE, SEE SHEET L-1 FOR QUANTITY AND SPECIES

7 DECIDUOUS CANOPY TREE, SEE SHEET L-1 FOR QUANTITY AND SPECIES

- (10) RETAIL SPACE FOUNDATION PLANTING, SEE SHEET
- L-1 FOR QUANTITY AND SPECIES
- (11) SODDED LAWN, OVER MINIMUM 3" DEPTH TOPSOIL
- (12) PROPOSED LIGHT POLE, SEE LIGHTING PLAN

PLANT SCHEDULE

TREES							
QTY	SYM	BOTANICAL NAME	COMMON NAME	SIZE	SPACING	ROOT	COMMENTS
5	AS	Acer s. 'Green Mountain'	Green Mountain Sugar Maple	2.5" cal.	as shown	B&B	Single straight trunk
6	AG	Amalanchier x g. 'Autumn Brilliance'	Autumn Brilliance Serviceberry	6'-7' ht.	as shown	B&B	Minimum 5 stems
4	GT	Gleditsia t. 'Skyline'	Skyline Honeylocust	2.5" cal.	as shown	B&B	Single straight trunk
13	PG	Picea glauca	White Spruce	6'-7' ht.	as shown	B&B	Unsheared, branched to gorur
4	QB	Quercus bicolor	Swamp White Oak	2.5" cal.	as shown	B&B	Single straight trunk
8	UA	Ulmus americana 'Valley Forge'	American Elm 'Valley Forge'	2.5" cal.	as shown	B&B	Single straight trunk
20	TA	Tilia americana 'Boulevard'	Boulevard American Basswood	2.5" cal.	as shown	B&B	Single straight trunk
8	MS	Malus 'Spring Snow'	Spring Snow Crabapple	6'-7' ht.	as shown	B&B	Minimum 5 stems
2	MV	Malus 'Velvet Pillar'	Velvet Pillar Crabapple	2.0" cal.	as shown	B&B	Single straight trunk
SHRUI	BS						
23	SP	Syringa p. 'Miss Kim'	Miss Kim Dwarf Korean Lilac	30" ht.	as shown	B&B	
31	VP	Viburnum p.t. 'Newport'	Newport American Snowball Bush	30" ht.	as shown	B&B	
36	TO	Thuja occidentalis 'Nigra'	Dark Green Arborvitae	6' ht.	as shown	B&B	Trim to hedge
37	PO	Physocarpus o. 'Summer Wine'	Summer Wine Ninebark	30" ht.	as shown	cont.	Well rooted
24	POC	Physocarpus o. 'Coppertina'	Coppertina Ninebark	30" ht.	as shown	cont.	Well rooted
20	VT	Viburnum trilobum 'Compactum'	Compact American Cranberry Bush	30" ht.	as shown	cont.	Well rooted
35	MP	Myrica pennsylvanica	Northern Bayberry	30" ht.	as shown	cont.	Well rooted
PEREN	INIALS						
156	CA	Calamagrostis a. Karl Foerster'	Karl Foerster Feather Reed Grass	#2	as shown	cont.	Well rooted
138	PA	Pennisetum a. 'Karley Rose'	Karley Rose Dwarf Fountain Grass	#2	as shown	cont.	Well rooted
275	RF	Rudbeckia f. 'Goldstrum'	Black-eyed Susan	#1	18" o.c.	cont.	Well rooted
190	NF	Nepeta x f. 'Walkers Low'	Walkers Low Nepeta	#1	24" o.c.	cont.	Well rooted



1. CONTRACTOR TO VERIFY PERCOLATION OF PLANTING PIT PRIOR TO INSTALLATION

SEE PLANT SCHEDULE FOR SPACING (2) 2" DEPTH CANADIAN PEAT TOP DRESSING

OR 2" DEPTH DOUBLE SHREDDED

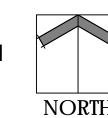
HARDWOOD MULCH

- (3) SHOVEL CUT OR METAL EDGE, SEE PLAN
- (4) FINISH GRADE (5) EXCAVATE EXISTING SOIL TO 12" DEPTH, REPLACE WITH PLANT MIX. SEE GENERAL LANDSCAPE NOTES SHEET L-3 AND **SPECIFICATIONS**
- (6) UNDISTURBED SUBGRADE

PERENNIAL / GROUNDCOVER PLANTING



NOT FOR CONSTRUCTION



AS NOTED Scale: Project Number: 18.033

12.10.2018

JG

JG

Sheet Number:

landscape architectüre 734.249.3568 Plymouth, MI

james@vertverde.com

Owner Review

Revision

Pre-submittal Review

Conceptual PUD Submittal

BMH - Premier PUD

4525 East Grand River Ave.

Genoa Township, MI

Premier Genoa, LLC

775 N. Second Street

Brighton, MI 48116

Project Sponsor:

Sheet Name:

Details

Enlargements

and Landscape

12.20.2018

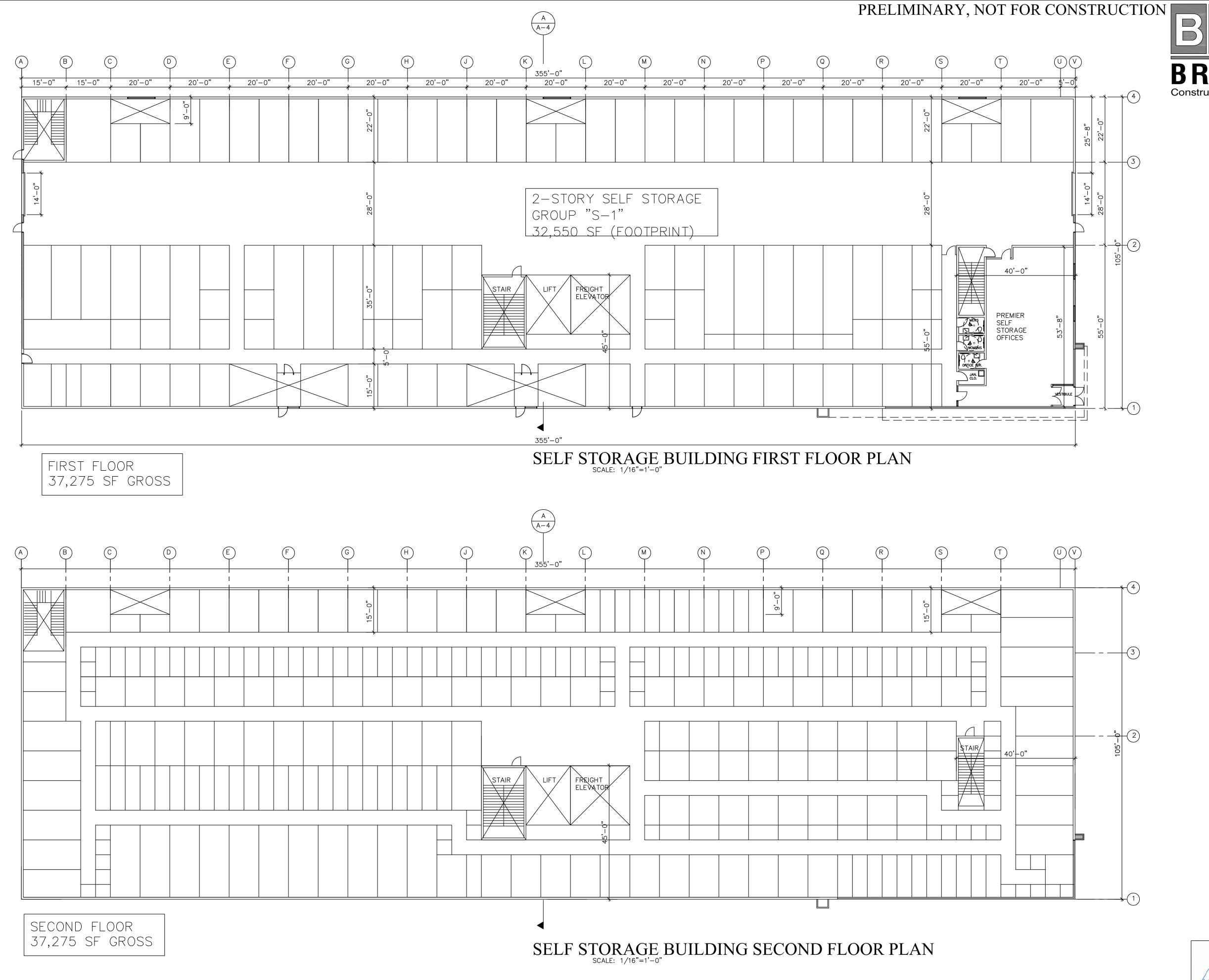
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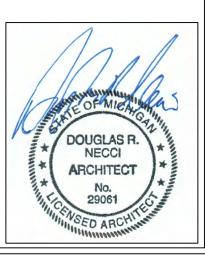
02.14.2020

12.27.2019

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DECIDUOUS TREE PLANTING





Scale
1/16"=1'-0"

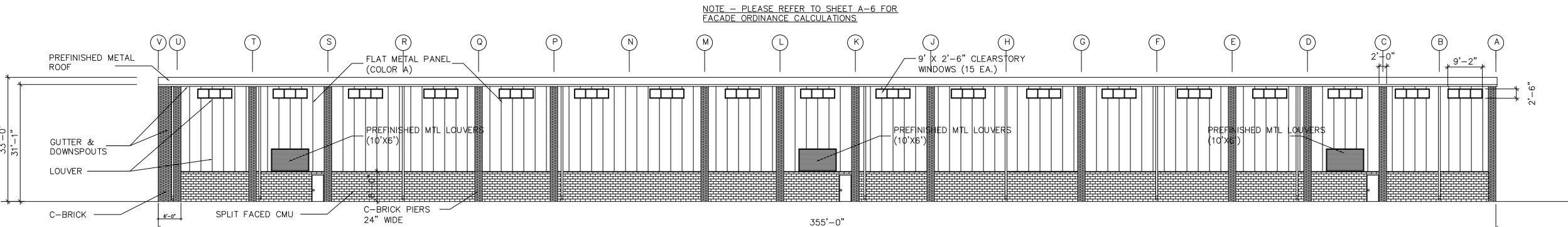
Date
2/19/20

Sht. No.

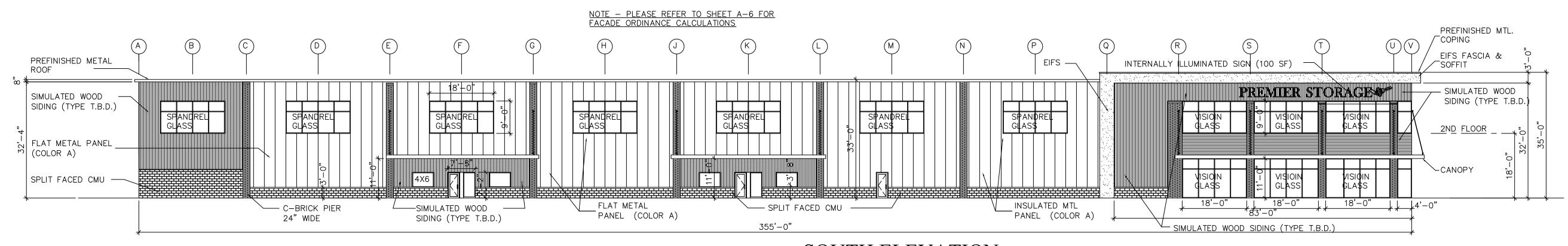
BMH-PREMIER P.U.D. HOWELL, MI



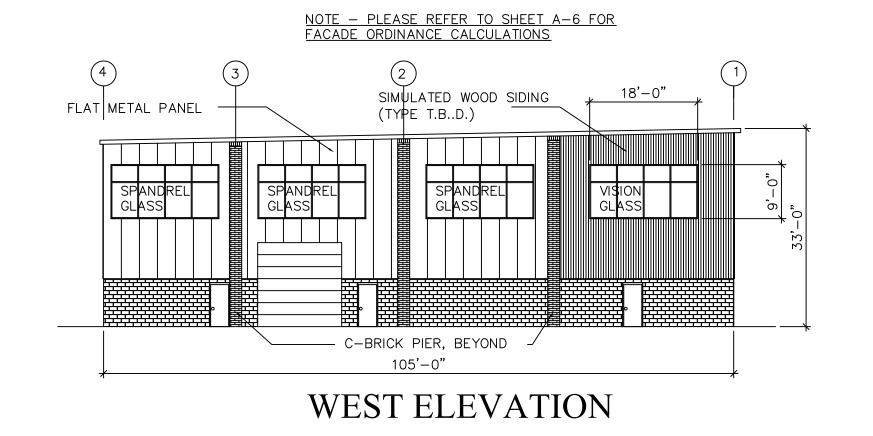


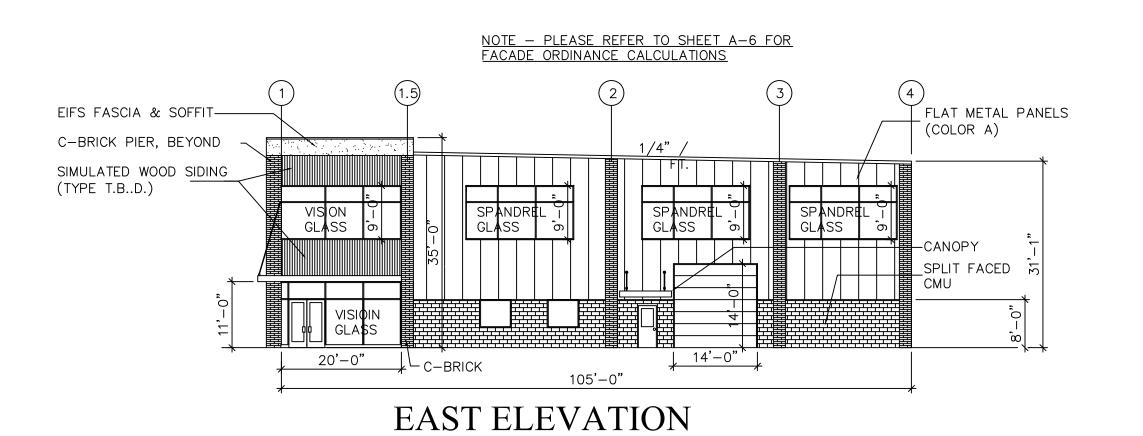


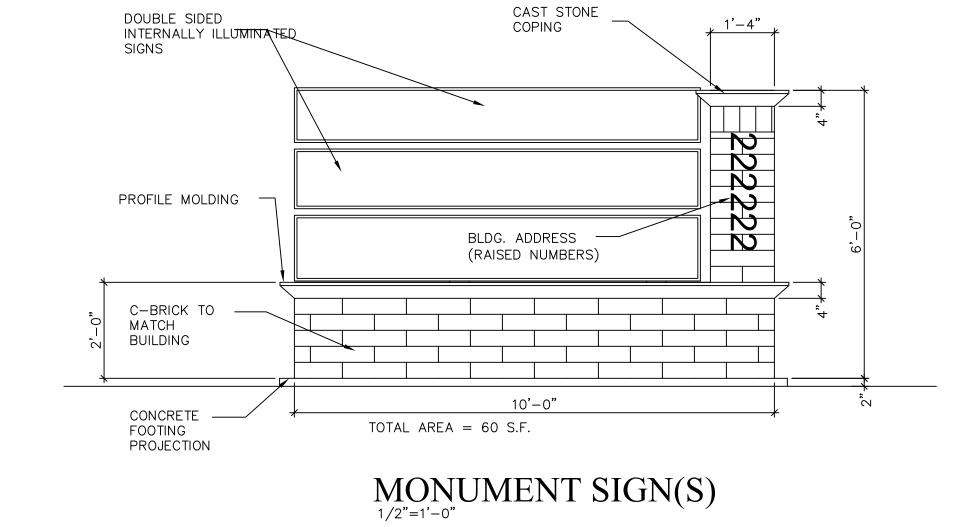
NORTH ELEVATION

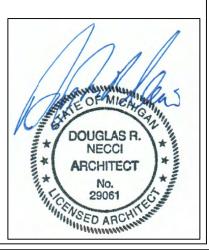








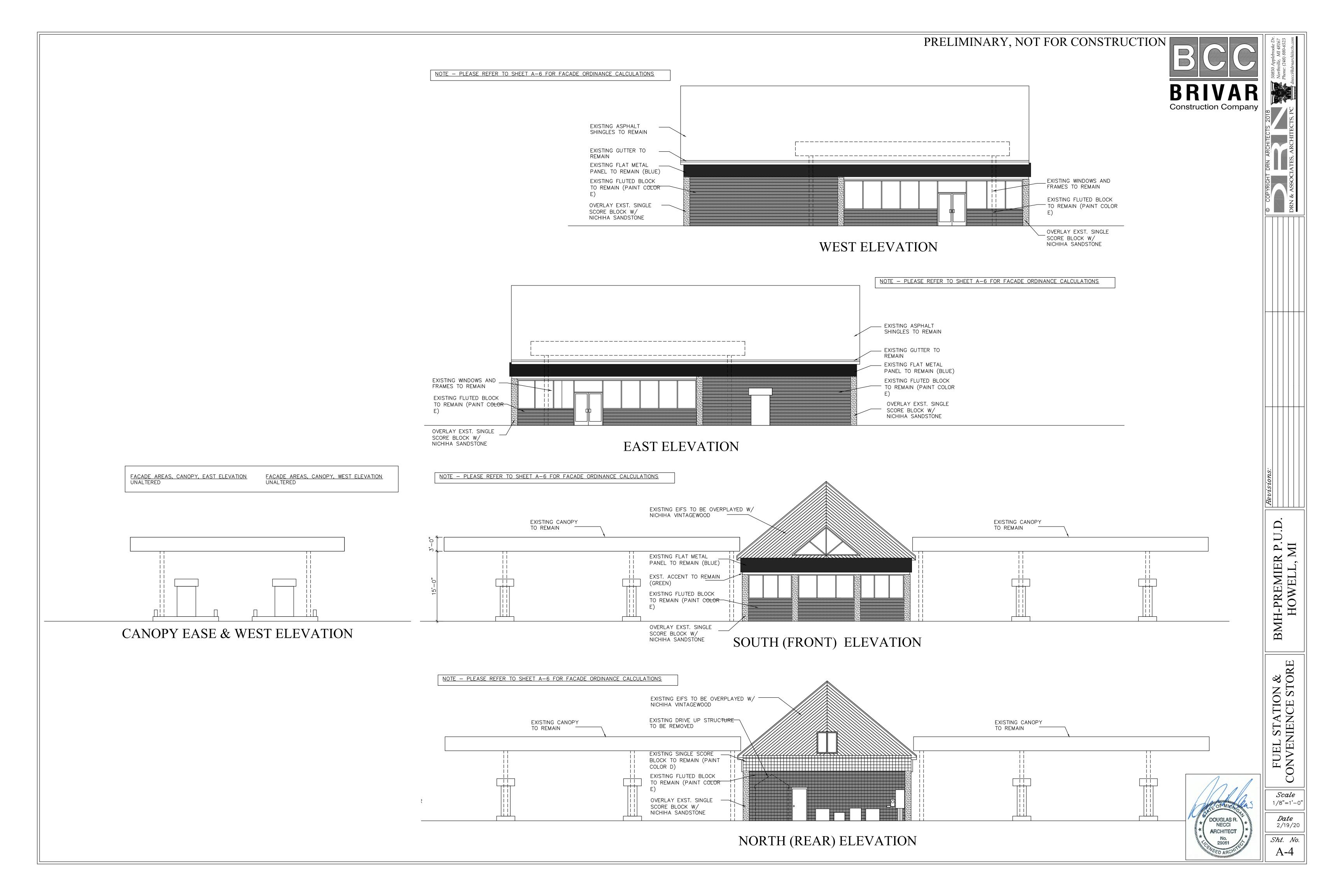




SELF STORAGE BLDG. ELEVATIONS Scale1/16"=1'-0' $\it Date$ 2/19/20 Sht. No.

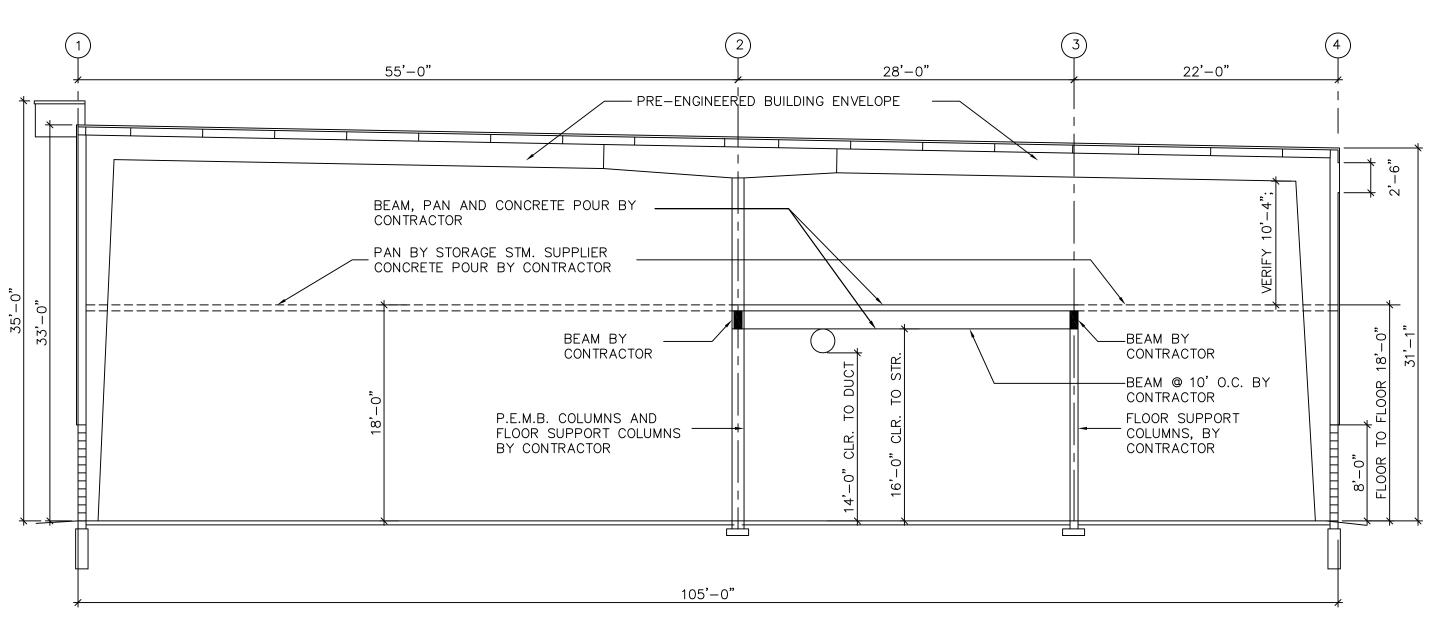
BMH-PREMIER I HOWELL, M



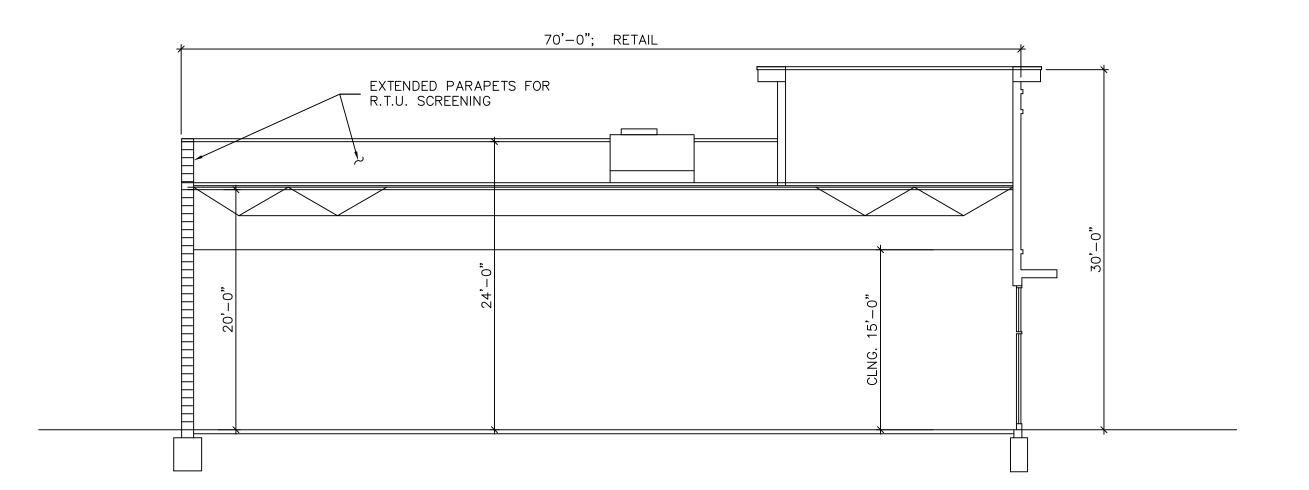








TYPICAL STORAGE BUILDING SECTION 1/8"=1'-0"



TYPICAL RETAIL & OFFICE BUILDING SECTION 1/8"=1'-0"





SELF STORAGE BUILDING

2/19/2020							
		Α	В	С	Comp	liance	Comments
		% as	Maximu	Minimum	If Λ_R – ±% F	xceeds Maximum	
Material	Area (SF)	% as designed	m Allowed	 Required		Jnder Minimum	
C-Brick (Note 2)	652	7%		75%	-68.2%	Deviation	See attached letter
Simulated Wood	2,158	23%	25%		-2.4%	Complies	
Flat Metal Panels	5,532	58%	25%		33.0%	Deviation	See attached letter
Split faced CMU	820	9%	25%		-16.4%	Complies	
EIFS	374	4%	25%		-21.1%	Complies	
TL	9,536	100%					
Windows & Spandrel Glass (Note 1)	2,525	20.9%		25%	-4.1%	Deviation	See attached letter
TL	12,061						
Date 2/12/2020	12,001			NORTH			
		Α	В	С	Comp	liance	Comments
Material	Area (SF)	% as	Maximu	Minimum	If A-B = $+\%$, Ex	xceeds Maximum	
iviaceriai	AIEd (SF)	designed	m	Required		Jnder Minimum	
C-Brick (Note 2)	981	9.7%		75%	-65.3%	Deviation	See attached letter
Flat Metal Panels	6,658	66.1%	25%		41.1%	Deviation	See attached letter
Split faced CMU	2,430	24.1%	25%		-0.9%	Complies	
TL	10,069	100.0%					
Windows, Spandrel & Louvers (Note 1)	774	7.1%		25%	-17.9%	Deviation	See attached letter
TL	10.942						
IL	10,843			EAST			
	A B C Compliance					Comments	
Material	Area (SF)	% as	Maximu	Minimum	If $A-B = +\%$, Ex	xceeds Maximum	
iviateriai	Alea (Si)	designed	m	Required	If A-C = - %,	Jnder Minimum	
C-Brick (Note 2)	326	14.1%		75%	-60.9%	Deviation	See attached letter
Simulated Wood	220	9.5%	25%		-15.5%	Complies	
Flat Metal Panels	1,270	55.0%	25%		30.0%	Deviation	See attached letter
Split faced CMU	421	18.2%	25%		-6.8%	Complies	
EIFS	70	18.2%	25%		-6.8%	Complies	
TL	2,307						
Windows & Spandrel (Note 1)	938	27.1%		25%	2.1%	Complies	
Doors	220						
TL	3,465						
		A	ВС		Compliance		Comments
		% as	Maximu m			xceeds Maximum Jnder Minimum	
Material	Area (SF)	designed		,quircu		Deviation	See attached letter
	Area (SF) 183	designed 8%	- 111	75%	-07.2/01	Deviation	occ attachica ictici
C-Brick (Note 2)			25%		-07.2%	Complies	see attached letter
Material C-Brick (Note 2) Simulated Wood Flat Metal Panels	183	8%					See attached letter
C-Brick (Note 2) Simulated Wood	183 535	8% 23%	25%		-2.3%	Complies	
C-Brick (Note 2) Simulated Wood Flat Metal Panels Split faced CMU	183 535 1,047	8% 23% 44%	25% 25%		-2.3% 19.4%	Complies Deviation	
C-Brick (Note 2) Simulated Wood Flat Metal Panels Split faced CMU TL	183 535 1,047 592 2,357	8% 23% 44% 25%	25% 25%		-2.3% 19.4% 0.1%	Complies Deviation Complies	See attached letter
C-Brick (Note 2) Simulated Wood Flat Metal Panels Split faced CMU	183 535 1,047 592	8% 23% 44%	25% 25%		-2.3% 19.4% 0.1%	Complies Deviation	

- 1. 25% Minimum Glass or Spandrel Glass required on indoor commercial storage building elevations
- visible from a public or private road or parking lot.
- 2. 75% Minimum brick or stone required on wall exposed to public view from a street or residential area.

GAS STATION / CONVENIENCE STORE

GAS STATION / CONVENIENCE S	TORE						
2/19/2020			S	OUTH			
		A B		С	Compliance		Comments
Material	Area (SF)	Percentage	Maximum	Minimum	If A-B = + %, Exceeds Maximum		
		as designed	Allowed	Required			
Fluted CMU (existing, to be painted)	143	24%	25%		-0.6%	Complies	
Simulated Wood (Nichiaha) (New)	280	48%	25%		22.8%	Deviation	See attached letter
Sandstone Panels (Nichiha) (New) (Note	54	9%	25%		-15.8%	Complies	
Flat Metal (trim and coping)	109	19%	25%		-6.4%	Complies	
IL	586	100%					
Windows (Note 1)	203	25.7%	N/A		25.7%	Complies	
TL	789						
	ſ	NORTH (Not vis	ible from publ	ic view from	street or residen	tial)	
		A	В	С	Compli	ance	Comments
Matavial	Λ ma a /CΓ\	Percentage	Maximum	Minimum	If A-B = + %, Exce	eds Maximum	
Material	Area (SF)	as designed	Allowed	Required	If A-C = -%, Unc	der Minimum	
Fluted CMU (existing, to be painted)	356	44%	25%		19.2%	Deviation	See attached letter
Simulated Wood (Nichiaha) (new)	303	38%	25%		12.6%	Deviation	See attached letter
Sandstone Panels (Nichiha) (New) (Note	27	3%	25%		-21.7%	Complies	
Single Score CMU, painted (existing)	120	15%	25%		-10.1%	Complies	
TL	806	100%					
)	24	2.50/	21/2				
Windows (Note 1)	21	2.5%	N/A			Complies	
TL	827						
112	027						
		Δ		AST	Compli	ance	Comments
		A Percentage	В	С	Compli If A-B = +%. Exce		Comments
Material	Area (SF)	Percentage	B Maximum	C Minimum	If A-B = + %, Exce	eds Maximum	
Material Fluted CMU (existing, to be painted)	Area (SF) 442		В	C Minimum		eds Maximum	
		Percentage as designed	B Maximum Allowed	C Minimum	If A-B = +%, Exce	eds Maximum der Minimum	
Fluted CMU (existing, to be painted)		Percentage as designed 65%	B Maximum Allowed 25%	C Minimum	If A-B = +%, Exce If A-C = -%, Und 40.4%	eds Maximum der Minimum Deviation	
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new)	442	Percentage as designed 65% 0%	B Maximum Allowed 25% 0%	C Minimum	If A-B = +%, Excelling If A-C = -%, Under 40.4%	eds Maximum der Minimum Deviation N/A	
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note	442 0 41	Percentage as designed 65% 0% 6% 29%	B Maximum Allowed 25% 0% 25%	C Minimum	If A-B = +%, Excelling If A-C = -%, Und 40.4% 0.0% -18.9%	eds Maximum der Minimum Deviation N/A Complies	See attached letter
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping)	442 0 41	Percentage as designed 65% 0% 6%	B Maximum Allowed 25% 0% 25%	C Minimum	If A-B = +%, Excelling If A-C = -%, Und 40.4% 0.0% -18.9%	eds Maximum der Minimum Deviation N/A Complies	See attached letter
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3) TL	442 0 41 193 676	Percentage as designed 65% 0% 6% 29%	B Maximum Allowed 25% 0% 25% 25%	C Minimum	If A-B = +%, Excelling If A-C = -%, Und 40.4% 0.0% -18.9% 3.6%	eds Maximum der Minimum Deviation N/A Complies Deviation	See attached letter
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3)	442 0 41 193	Percentage as designed 65% 0% 6% 29%	B Maximum Allowed 25% 0% 25%	C Minimum	If A-B = +%, Excelling If A-C = -%, Und 40.4% 0.0% -18.9%	eds Maximum der Minimum Deviation N/A Complies	See attached letter
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3) TL Windows (Note 1)	442 0 41 193 676	Percentage as designed 65% 0% 6% 29%	B Maximum Allowed 25% 0% 25% 25%	C Minimum	If A-B = +%, Excelling If A-C = -%, Und 40.4% 0.0% -18.9% 3.6%	eds Maximum der Minimum Deviation N/A Complies Deviation	See attached letter
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3) TL	442 0 41 193 676 274	Percentage as designed 65% 0% 6% 29% 100%	B Maximum Allowed 25% 0% 25% 25% N/A	C Minimum Required	If A-B = +%, Excellent If A-C = -%, Under 40.4% 0.0% -18.9% 3.6%	eds Maximum der Minimum Deviation N/A Complies Deviation Complies	See attached letter
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3) TL Windows (Note 1)	442 0 41 193 676 274	Percentage as designed 65% 0% 6% 29% 100% 28.8%	B Maximum Allowed 25% 0% 25% 25% N/A	C Minimum Required	If A-B = +%, Excellent If A-C = -%, Under 40.4% 0.0% -18.9% 3.6% 28.8%	eds Maximum der Minimum Deviation N/A Complies Deviation Complies	See attached letter See attached letter
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3) TL Windows (Note 1) TL	442 0 41 193 676 274	Percentage as designed 65% 0% 6% 29% 100% 28.8%	B Maximum Allowed 25% 0% 25% 25% N/A N/A	C Minimum Required c view from s	If A-B = +%, Excellent If A-C = -%, Und 40.4% 0.0% -18.9% 3.6% 3.6% 28.8% Careet or resident Compli	eds Maximum der Minimum Deviation N/A Complies Deviation Complies tial) ance	See attached letter
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3) TL Windows (Note 1)	442 0 41 193 676 274	Percentage as designed 65% 0% 6% 29% 100% 28.8% WEST (Not vis A Percentage	B Maximum Allowed 25% 0% 25% 25% N/A N/A ible from publi B Maximum	C Minimum Required c view from s C Minimum	If A-B = +%, Excelling If A-C = -%, Und 40.4% 0.0% -18.9% 3.6% 3.6% 28.8% ctreet or resident Compliation If A-B = +%, Excelling If A-B =	eds Maximum der Minimum Deviation N/A Complies Deviation Complies tial) ance	See attached letter See attached letter
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3) TL Windows (Note 1) TL	442 0 41 193 676 274	Percentage as designed 65% 0% 6% 29% 100% 28.8%	B Maximum Allowed 25% 0% 25% 25% N/A N/A	C Minimum Required c view from s C Minimum	If A-B = +%, Excellent If A-C = -%, Und 40.4% 0.0% -18.9% 3.6% 3.6% 28.8% Careet or resident Compli	eds Maximum der Minimum Deviation N/A Complies Deviation Complies tial) ance	See attached letter See attached letter
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3) TL Windows (Note 1) TL Material	442 0 41 193 676 274 950 Area (SF)	Percentage as designed 65% 0% 6% 29% 100% 28.8% WEST (Not vis A Percentage as designed	B Maximum Allowed 25% 0% 25% 25% N/A N/A Sible from publi B Maximum Allowed	C Minimum Required c view from s C Minimum	If A-B = +%, Excellent A-C = -%, Und 40.4% 0.0% -18.9% 3.6% 28.8% street or resident Compli If A-B = +%, Excellent A-C = -%, Und	eds Maximum Deviation N/A Complies Deviation Complies tial) ance eds Maximum der Minimum	See attached letter See attached letter Comments
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3) TL Windows (Note 1) TL Material Fluted CMU (existing, to be painted)	442 0 41 193 676 274 950 Area (SF)	Percentage as designed 65% 0% 6% 29% 100% 28.8% WEST (Not vis A Percentage as designed 65%	B Maximum Allowed 25% 0% 25% 25% N/A N/A Sible from publi B Maximum Allowed 25%	C Minimum Required c view from s C Minimum	If A-B = +%, Excellent A-C = -%, Under 40.4% 0.0% -18.9% 3.6% 28.8% ctreet or resident Complication Complication A-C = -%, Under 40.4%	eds Maximum Deviation N/A Complies Deviation Complies tial) ance eds Maximum der Minimum Deviation	See attached letter See attached letter Comments
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3) TL Windows (Note 1) TL Material Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (New)	442 0 41 193 676 274 950 Area (SF) 442 0	Percentage as designed 65% 0% 6% 29% 100% 28.8% WEST (Not vis A Percentage as designed 65% 0%	B Maximum Allowed 25% 0% 25% 25% N/A N/A Sible from publi B Maximum Allowed 25% 0%	C Minimum Required c view from s C Minimum	If A-B = +%, Excellent A-C = -%, Und 40.4% 0.0% -18.9% 3.6% 28.8% Street or resident Complication Complication A-C = -%, Und 40.4% 0.0%	eds Maximum Deviation N/A Complies Deviation Complies Complies tial) ance eds Maximum der Minimum Deviation N/A	See attached letter See attached letter Comments
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3) TL Windows (Note 1) TL Material Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (New) Sandstone Panels (Nichiha) (New) (Note	442 0 41 193 676 274 950 Area (SF) 442 0 41	Percentage as designed 65% 0% 6% 29% 100% 28.8% WEST (Not vis A Percentage as designed 65% 0% 6%	B Maximum Allowed 25% 0% 25% 25% N/A N/A Sible from publi B Maximum Allowed 25% 0% 25%	C Minimum Required c view from s C Minimum	If A-B = +%, Excellent A-C = -%, Under 40.4% 0.0% -18.9% 3.6% 28.8% ctreet or resident Compliation A-B = +%, Excellent A-C = -%, Under 40.4% 0.0% -18.9%	eds Maximum Deviation N/A Complies Deviation Complies tial) ance eds Maximum der Minimum Deviation N/A Complies	See attached letter See attached letter Comments See attached letter
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3) TL Windows (Note 1) TL Material Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (New) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping)	442 0 41 193 676 274 950 Area (SF) 442 0 41	Percentage as designed 65% 0% 6% 29% 100% 28.8% WEST (Not vis A Percentage as designed 65% 0% 6%	B Maximum Allowed 25% 0% 25% 25% N/A N/A Sible from publi B Maximum Allowed 25% 0% 25%	C Minimum Required c view from s C Minimum	If A-B = +%, Excellent A-C = -%, Under 40.4% 0.0% -18.9% 3.6% 28.8% ctreet or resident Compliation A-B = +%, Excellent A-C = -%, Under 40.4% 0.0% -18.9%	eds Maximum Deviation N/A Complies Deviation Complies tial) ance eds Maximum der Minimum Deviation N/A Complies	See attached letter See attached letter Comments See attached letter
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3) TL Windows (Note 1) TL Material Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (New) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3) TL	442 0 41 193 676 274 950 Area (SF) 442 0 41 193	Percentage as designed 65% 0% 6% 29% 100% 28.8% WEST (Not vis A Percentage as designed 65% 0% 6% 29% 100%	B Maximum Allowed 25% 0% 25% 25% N/A N/A sible from publi B Maximum Allowed 25% 0% 25% 25%	C Minimum Required c view from s C Minimum	If A-B = +%, Excelling A-C = -%, Under 40.4% 0.0% -18.9% 3.6% 28.8% Street or resident Compliation	eds Maximum Deviation N/A Complies Deviation Complies tial) ance eds Maximum der Minimum Deviation N/A Complies Deviation	See attached letter See attached letter Comments See attached letter
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3) TL Windows (Note 1) TL Material Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (New) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3)	442 0 41 193 676 274 950 Area (SF) 442 0 41 193	Percentage as designed 65% 0% 6% 29% 100% 28.8% WEST (Not vis A Percentage as designed 65% 0% 6% 29%	B Maximum Allowed 25% 0% 25% 25% N/A N/A Sible from publi B Maximum Allowed 25% 0% 25%	C Minimum Required c view from s C Minimum	If A-B = +%, Excellent A-C = -%, Under 40.4% 0.0% -18.9% 3.6% 28.8% ctreet or resident Compliation A-B = +%, Excellent A-C = -%, Under 40.4% 0.0% -18.9%	eds Maximum Deviation N/A Complies Deviation Complies tial) ance eds Maximum der Minimum Deviation N/A Complies	See attached letter See attached letter Comments See attached letter
Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (new) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3) TL Windows (Note 1) TL Material Fluted CMU (existing, to be painted) Simulated Wood (Nichiaha) (New) Sandstone Panels (Nichiha) (New) (Note Flat Metal (trim and coping) Asphalt Shingles (Note 3) TL	442 0 41 193 676 274 950 Area (SF) 442 0 41 193	Percentage as designed 65% 0% 6% 29% 100% 28.8% WEST (Not vis A Percentage as designed 65% 0% 6% 29% 100%	B Maximum Allowed 25% 0% 25% 25% N/A N/A sible from publi B Maximum Allowed 25% 0% 25% 25%	C Minimum Required c view from s C Minimum	If A-B = +%, Excelling A-C = -%, Under 40.4% 0.0% -18.9% 3.6% 28.8% Street or resident Compliation	eds Maximum Deviation N/A Complies Deviation Complies tial) ance eds Maximum der Minimum Deviation N/A Complies Deviation	See attached letter See attached letter Comments See attached letter

- 1. 25% Minimum windows does not apply to gas station building
- 2. Asphalt shingles are not listed on the "Schedule of Exterior Wall Materials", therefore not counted in percentages.
- 3. Sand Stone Panels considered to be "Finshes" as defined as footnote .(e) in 12.01.03

RETAIL / SERVICE BUILDING

2/19/2020		SOUTH								
		Α	В	С	Comp	iance	Comments			
Material	Area (SF)	Percentage	Maximum	Minimum	If $A-B = +\%$, Ex	ceeds Maximum				
iviateriai	Alea (Sir)	as designed	Allowed	Required	If A-C = - %, U	nder Minimum				
C-Brick (Note 2)	1,387	81%		75%		Complies				
EIFS	330	19%	25%		-5.8%	Complies				
TL	1,717	100%								
Windows (Note 1)	839	32.8%		N/A	32.8%	Complies				
TL	2,556									
	N	ORTH (Not visil	ole from public	view from str	reet or residen	tial)				
		Α	В	С	Comp	iance	Comments			
NA	4 (65)	Percentage	Maximum	Minimum		ceeds Maximum				
Material	Area (SF)	as designed	Allowed	Required	If A-C = - %, U	nder Minimum				
C-Brick (Note 2)	133	5.8%		N/A	5.8%	Complies				
Split faced CMU	2,175	94.2%	25%		69.2%	Deviation	See attached letter			
Doors (not included)										
TL	2,308	100.0%								
Windows (Note 1)	0	0.0%		N/A	0.0%	Complies				
TL	2,308									
		•	E <i>A</i>	AST						
		A B C Compliance								
Material	Area (SF)	Percentage	Maximum			ceeds Maximum				
C Duist (Note 2)		as designed	Allowed	•		nder Minimum				
C-Brick (Note 2)	1,035	79%	250/	75%	4.3%	Complies				
EIFS	270	21%	25%		-4.3%	Complies				
TL	1,305	100%								
Windows (Note 1)	450	25.6%		N/A	25.6%	Complies				
TL	1,755									
		WEST								
		Α	В	С	Comp		Comments			
Material	Area (SF)	Percentage	Maximum			ceeds Maximum				
C-Brick (Note 2)	1,035	as designed 79%	Allowed	Required 75%	79.3%	nder Minimum Complies				
EIFS	270	21%	25%	, 5/0	-4.3%	Complies				
TL	1,305	100%	23/0		7.3/0	Compiles				
Windows (Note 1)	450	25.6%		N/A	25.6%	Complies				
TL	1,755									
16	1,733						1			

1. 25% Minimum glass requirement only applies to self storage building; no minimum for other buildings. 2. 75% Minimum brick or stone required on wall exposed to public view from a street or residential area.

DOUGLAS R. NECCI ARCHITECT No. 29061

RETAIL/SERVICES BUILDING										
2/19/2020	SOUTH									
		Α	В	С	Compl	iance	Comments			
Natarial	A === (CE)	Percentage as	Maximum	Minimum	If A-B = +%, Ex	ceeds Maximum				
Material	Area (SF)	designed	Allowed	Required	If A-C = - %, Under Minimum					
C-Brick (Note 2)	1,387	81%		75%	5.8%	Complies				
EIFS	330	19%	25%		-5.8%	Complies				
TL	1,717	100%								
Windows (Note 1)	839	32.8%		N/A	32.8%	Complies				
Τι	2,556									
TL	2,550	NORTH (Not vis	ible from public	view from stre	et or residentia	1)				
		A	B	C	Compl	•	Comments			
		Percentage as	Maximum	Minimum	·					
Material	Area (SF)	designed	Allowed	Required	-	nder Minimum				
C-Brick (Note 2)	133	5.8%	7111011101	N/A	5.8%	Complies				
Split faced CMU	2,175	94.2%	25%	,	69.2%	Deviation	See attached letter			
Doors (not included)	, -									
TL	2,308	100.0%								
Windows (Note 1)	0	0.0%		N/A	0.0%	Complies				
TL	2,308									
	2,300		EAS	ST						
						iance	Comments			
Material	Area (SF)	Percentage as	Maximum	Minimum	If A-B = +%, Ex	ceeds Maximum				
Material	Area (SF)	designed	Allowed	Required	If A-C = - %, U	nder Minimum				
C-Brick (Note 2)	1,035	79%		75%	4.3%	Complies				
EIFS	270	21%	25%		-4.3%	Complies				
TL	1,305	100%								
Windows (Note 1)	450	25 69/		N/A	25 69/	Complies				
Willdows (Note 1)	450	25.6%		IN/A	25.6%	Complies				
TL	1,755									
		Α	В	Compl	iance	Comments				
Material	Area (SF)		B C Maximum Minimum		If A-B = +%, Ex	ceeds Maximum				
iviateriai	Alea (SI)	designed	Allowed	Required	If A-C = - %, Under Minimum					
C-Brick (Note 2)	1,035	79%		75%	79.3%	Complies				
EIFS	270	21%	25%		-4.3%	Complies				
TL	1,305	100%								
Windows (Note 1)	450	25.6%		N/A	25.6%	Complies				
23113 (11010 1)	30	23.070		14/ ^	25.070	complics				
TL	1,755									

Notes:

- 1. 25% Minimum glass requirement only applies to self storage building; no minimum for other buildings.
- 2. 75% Minimum brick or stone required on wall exposed to public view from a street or residential area.

GENOA CHARTER TOWNSHIP PLANNING COMMISSION PUBLIC HEARING FEBRUARY 10, 2020 6:30 P.M. MINUTES

<u>CALL TO ORDER:</u> The meeting of the Genoa Charter Township Planning Commission was called to order at 6:30 p.m. Present were Marianne McCreary, Chris Grajek, Eric Rauch, and Jeff Dhaenens. Absent were Jim Mortensen, Glynis McBain, and Jill Rickard. Also present was Kelly VanMarter, Community Development Director/Assistant Township Manager, and Brian Borden of Safebuilt Studio. There were no audience members present.

<u>PLEDGE OF ALLEGIANCE</u>: The pledge of allegiance was recited.

APPROVAL OF AGENDA:

Moved by Commissioner McCreary, seconded by Commissioner Grajek, to approve the agenda as presented. **The motion carried unanimously.**

ELECTION OF OFFICERS:

Ms. Van Marter noted that officers are required to be elected at the first meeting of the month. The Chairperson, Vice-Chairperson and Secretary need to be selected.

Moved by Commissioner Rauch, seconded by Commissioner Grajeck, to elect Ms. McCreary as Secretary, Mr. Grajak as Chairman, and himself as Vice-Chairman. **The motion carried unanimously**.

CALL TO THE PUBLIC: The call to the public was made at 6:35 pm with no response.

OLD BUSINESS:

OPEN PUBLIC HEARING #1... Review of a special use, site plan and environmental impact assessment for outdoor sales, storage and display for Home Depot. The property in question is located at 3330 E. Grand River, Howell. The request is petitioned by Scott A. Mommer. A. Recommendation of Special Use Application. B. Recommendation of Environmental Impact Assessment. (1-20-20) C. Recommendation of Site Plan. (1-13-20)

Mr. Brent LaVanway of Boss Engineering was present. He reviewed the changes since the last meeting. They have removed a row of parking and added a landscape island on the south side of the parking lot. This location will include the seasonal items, shed displays and equipment rental.

They are suggesting deciduous trees for the landscape island. They would be six-feet high at the time of planting. The opaque wood fence that is along the southern border of the parking lot

will be eight-feet high and will sit on top of a three-foot high retaining wall, so the total height of the screening will be 11 feet.

Mr. Borden reviewed his letter dated February 5, 2020.

He noted that the applicant made the changes to the plan based on the discussion from the last meeting.

- 1. The Commission may require additional shrub plantings, or allow excess existing trees to count as the required buffer zones.
- 2. At the time of planting, the height of items stored in the parking lot will exceed the height of screening. The Commission may require taller evergreen trees at the time of planting and/or an increase in the number of evergreen trees.
- 3. The plantings depicted between the evergreen trees must be identified on the landscape plan.
- 4. A detail of the fencing around the seasonal sales area must be provided. Mr. LaVanway believes the description of the fence on the plan was a carry-over from a previous plan. There will not be a fenced-in area in the parking lot. This is being replaced with the new area discussed this evening. They will make this change on the plan.

Commissioner Rauch questioned the seasonal displays that are up near the building. What is the timing of the "season"? Mr. LaVanway stated the items being displayed depend on the time of year. There would be lawn mowers, tractors, etc. in the spring and summer, and then snow plows, etc. in the winter. Commissioner McCreary stated there is no time frame. It was agreed to remove the word "seasonal" from the plans. Commissioner Dhaenens wanted to ensure that if the word "seasonal" is removed, they cannot then move sheds to that area. Mr. LaVanway stated the location of the shed display is noted specifically on the plans so that is the only location where they will be placed.

Ms. VanMarter requested that if the Commission sends an approval to the Township Board, to stated that the Special Use Permit be valid for one year, and renewals will be approved by Township staff.

The call to the public was made at 6:58 pm with no response.

Moved by Commissioner Rauch, seconded by Commission McCreary, to recommend to the Township Board approval of the Special Land Use Application for Home Depot being that the Commission finds this item to meet the requirements of Section 7.02.02(d) of the Township Ordinance. The recommendation is conditioned upon the following:

• It is valid for one year and can be renewed annually by Township staff.

The motion carried unanimously.

Moved by Commissioner Rauch, seconded by Commission Dhaenens, to recommend to the Township Board approval of the Environmental Impact Assessment dated 1/20/20 for Home

Depot, with the condition that Paragraph I is amended to state that no fence is allowed in the middle of the parking lot for seasonal sales. **The motion carried unanimously**.

Moved by Commissioner Rauch, seconded by Commission Dhaenens, to recommend to the Township Board approval of the Site Plan dated 1/13/20 for Home Depot, with the following conditions:

- The fence on the south lot line is clarified as being eight-feet tall
- The word "seasonal" shall be removed from the tractor display area noted in the front of the building
- The use of shrubs between the evergreens proposed in the landscape planter is approved.

The motion carried unanimously.

ADMINISTRATIVE BUSINESS

Staff Report

Ms. VanMarter stated the March 9, 2020 meeting is very heavy. There are five items scheduled. She reviewed the proposals.

She noted that there may be a special meeting in March if some of the applications are not ready to be heard at the first one.

Approval of the December 9, 2019 Planning Commission meeting minutes

Moved by Commissioner McCreary, seconded by Commissioner Dhaenens, to approve the minutes of the December 9, 2019 Planning Commission Meeting as presented. **The motion carried unanimously.**

Member Discussion

Chairman Grajek asked the Commissioners to think of items that they would like to change with the way the meetings are handled. He suggested that Commissioners do not interrupt each other or the petitioner when they are speaking.

Commissioner Rauch suggested letting the petitioner present their entire proposal without the Commission asking questions during the presentation. Questions should be held until they are finished.

Adjournment

Moved by Commissioner Dhaenans, seconded by Commissioner Rauch, to adjourn the meeting at 7:33 pm. **The motion carried unanimously.**

Respectfully Submitted.

Patty Thomas, Recording Secretary

